



Oregon

Theodore R. Kulongoski, Governor

Department of Land Conservation and Development

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NOTICE OF ADOPTED AMENDMENT

07/01/2009

TO: Subscribers to Notice of Adopted Plan
or Land Use Regulation Amendments

FROM: Plan Amendment Program Specialist

SUBJECT: City of Gresham Plan Amendment
DLCD File Number 003-09

The Department of Land Conservation and Development (DLCD) received the attached notice of adoption. Due to the size of amended material submitted, a complete copy has not been attached. A Copy of the adopted plan amendment is available for review at the DLCD office in Salem and the local government office.

Appeal Procedures*

DLCD ACKNOWLEDGMENT or DEADLINE TO APPEAL: Tuesday, July 14, 2009

This amendment was submitted to DLCD for review prior to adoption. Pursuant to ORS 197.830(2)(b) only persons who participated in the local government proceedings leading to adoption of the amendment are eligible to appeal this decision to the Land Use Board of Appeals (LUBA).

If you wish to appeal, you must file a notice of intent to appeal with the Land Use Board of Appeals (LUBA) no later than 21 days from the date the decision was mailed to you by the local government. If you have questions, check with the local government to determine the appeal deadline. Copies of the notice of intent to appeal must be served upon the local government and others who received written notice of the final decision from the local government. The notice of intent to appeal must be served and filed in the form and manner prescribed by LUBA, (OAR Chapter 661, Division 10). Please call LUBA at 503-373-1265, if you have questions about appeal procedures.

***NOTE:** THE APPEAL DEADLINE IS BASED UPON THE DATE THE DECISION WAS MAILED BY LOCAL GOVERNMENT. A DECISION MAY HAVE BEEN MAILED TO YOU ON A DIFFERENT DATE THAT IT WAS MAILED TO DLCD. AS A RESULT, YOUR APPEAL DEADLINE MAY BE EARLIER THAN THE ABOVE DATE SPECIFIED.

Cc: Lauren McGuire, City of Gresham
Gloria Gardiner, DLCD Urban Planning Specialist
Jennifer Donnelly, DLCD Regional Representative
Bill Holmstrom, DLCD Transportation Planner

<paa> YA

Notice of Adoption

THIS FORM **MUST BE MAILED** TO DLCD
WITHIN 5 WORKING DAYS AFTER THE FINAL DECISION
PER ORS 197.610, OAR CHAPTER 660 - DIVISION 18

In person electronic mailed

DATE STAMP

DEPT OF

JUN 24 2009

LAND CONSERVATION AND DEVELOPMENT

For DLCD Use Only

Jurisdiction: **City of Gresham**

Local file number: **CPA 09-017**

Date of Adoption: **6/16/2009**

Date Mailed: **6-23-09**

Was a Notice of Proposed Amendment (Form 1) mailed to DLCD? **Yes** Date: **2/27/2009**

- | | |
|---|--|
| <input checked="" type="checkbox"/> Comprehensive Plan Text Amendment | <input checked="" type="checkbox"/> Comprehensive Plan Map Amendment |
| <input checked="" type="checkbox"/> Land Use Regulation Amendment | <input type="checkbox"/> Zoning Map Amendment |
| <input type="checkbox"/> New Land Use Regulation | <input type="checkbox"/> Other: |

Summarize the adopted amendment. Do not use technical terms. Do not write "See Attached".

This proposal amends the City's 1995 Downtown Plan by adopting a new Downtown Plan District Design Manual and other Code changes that include new land use districts and code amendments to provide both clear & objective and discretionary development review processes. The Code provisions also include permitted land uses and intensities; development design principles, guidelines & standards; street design standards; and parking requirements to regulate the built environment Downtown.

Does the Adoption differ from proposal? **Yes**, Please explain below:

Some Development Plan Map and Development Code changes were made, including:

1. Revised Floor Area Ratio.
2. Added setback requirements.
3. Adjusted permitted uses and densities, especially in DRL-1 and DRL-2 districts.
4. Expanded and revised guidelines and standards, including specifying which guidelines and standards apply to which land uses.

The approved amendment is substantially similar to the proposal sent to DLCD on Feb. 27, 2009.

Plan Map Changed from: **CUC,DT,DR-30,DC-1,DC-2,DR-12, CC, CMU, SC, TLDR, TR** to: **DCC, DCL, DEM, DMU, DRL-1, DRL-2, DTM**

Zone Map Changed from: _____ to: _____

Location: Area generally bounded by Powell, Burnside, Hogan, Eastman Parkway

Acres Involved: **550**

Specify Density: Previous: _____ New: _____

Applicable statewide planning goals:

- | | | | | | | | | | | | | | | | | | | |
|-------------------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|-------------------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|
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Was an Exception Adopted? YES NO

Did DLCD receive a Notice of Proposed Amendment...

45-days prior to first evidentiary hearing? Yes No

If no, do the statewide planning goals apply? Yes No

If no, did Emergency Circumstances require immediate adoption?

Yes No

DLCD file No. 003-09 (17391) [15586]

Please list all affected State or Federal Agencies, Local Governments or Special Districts:

Metro

Local Contact: **Brian Martin**

Phone: (503) 618-2266 Extension:

Address: 1333 NW Eastman Parkway

Fax Number: - -

City: Gresham

Zip: 97030

E-mail Address:

brian.martin@ci.gresham.or.us

ADOPTION SUBMITTAL REQUIREMENTS

This form **must be mailed** to DLCD **within 5 working days after the final decision**
per ORS 197.610, OAR Chapter 660 - Division 18.

1. Send this Form and TWO Complete Copies (documents and maps) of the Adopted Amendment to:

**ATTENTION: PLAN AMENDMENT SPECIALIST
DEPARTMENT OF LAND CONSERVATION AND DEVELOPMENT
635 CAPITOL STREET NE, SUITE 150
SALEM, OREGON 97301-2540**

2. Electronic Submittals: At least **one** hard copy must be sent by mail or in person, or by emailing **larry.french@state.or.us**.
3. Please Note: Adopted materials must be sent to DLCD not later than **FIVE (5) working days** following the date of the final decision on the amendment.
4. Submittal of this Notice of Adoption must include the text of the amendment plus adopted findings and supplementary information.
5. The deadline to appeal will not be extended if you submit this notice of adoption within five working days of the final decision. Appeals to LUBA may be filed within **twenty-one (21) days** of the date, the Notice of Adoption is sent to DLCD.
6. In addition to sending the Notice of Adoption to DLCD, you must notify persons who participated in the local hearing and requested notice of the final decision.
7. **Need More Copies?** You can now access these forms online at **<http://www.lcd.state.or.us/>**. Please print on **8-1/2x11 green paper only**. You may also call the DLCD Office at (503) 373-0050; or Fax your request to: (503) 378-5518; or Email your request to **larry.french@state.or.us** - **Attention: Plan Amendment Specialist**.

BEFORE THE CITY COUNCIL OF THE
CITY OF GRESHAM

IN THE MATTER OF AMENDMENTS TO) Order No. 615
VOLUME 1, FINDINGS; VOLUME 2, POLICIES;)
VOLUME 3, CODE; OF THE GRESHAM) CPA 09-017
COMMUNITY DEVELOPMENT PLAN REGARDING)
THE DOWNTOWN PLAN AND DESIGN MANUAL)

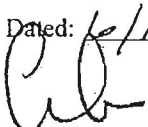
On June 2, 2009, the City Council held a public hearing to take testimony on amendments to Volume 1, Findings; Volume 2, Policies, Volume 3, Code; of the Gresham Community Development Plan relating to the Downtown Plan and Design Manual.

The hearing was conducted under Type IV procedures. Mayor Shane T. Bemis presided at the hearing.

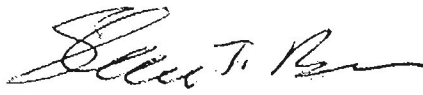
The Council closed the public hearing and approved the proposed amendments at the June 2, 2009 meeting, and a decision was made at the June 16, 2009 meeting.

A permanent record of this proceeding is to be kept on file in the Gresham City Hall, along with the original of the Order.

The Council orders that these amendments are approved, and adopts the findings, conclusions, and recommendations as stated in the attached Planning Commission Recommendation Order and staff reports.

Dated: 6/16/09


City Manager



Mayor

**BEFORE THE PLANNING COMMISSION OF THE
CITY OF GRESHAM**

TYPE IV RECOMMENDATION ORDER

CPA 09-017

A public hearing was held on May 11, 2009, upon an application to consider proposed amendments to Volumes 1, 2 and 3 of the Gresham Community Development Plan, and the Community Development Plan Map regarding the **Downtown Plan District Design Manual**.

The Commission closed the public hearing at the May 11, 2009 meeting, and a final recommendation to Council was made at the May 11, 2009 meeting.

Richard Anderson, Chairperson, presided at the hearing.

A permanent record of this proceeding is to be kept on file in the Gresham City Hall, along with the original of this Type IV Recommendation Order.

The Planning Commission recommends **ADOPTION** of the proposed Gresham Community Development Plan and Plan Map amendments to the City Council, and adopts the findings, conclusions and recommendations contained in the attached April 30, 2009 staff report and the May 8 Addendum staff report, with the following exceptions, additions and/or changes:

NONE


Chairperson

MAY 11, 2009

Date



MEMORANDUM

URBAN DESIGN & PLANNING

STAFF REPORT TYPE IV HEARING—COMPREHENSIVE PLAN AMENDMENT DOWNTOWN PLAN/DESIGN STANDARDS PROJECT

To: Gresham Planning Commission

From: Mike Abbaté, Urban Design & Planning Director
Jonathan Harker, AICP, Comprehensive Planning Manager
Lauren McGuire, Senior Planner
Brian Martin, AICP, Associate Planner

Hearing Date: May 11, 2009
Report Date: April 30, 2009
File: CPA 09-017

Proposal: To adopt comprehensive plan amendments to Volumes 1, 2 and 3 of the Community Development Plan relating to the Downtown Plan District with:

- 1) Text amendments to the Downtown Plan District findings report to add properties generally north of NW Division Street and fronting the north side of NW Burnside Road between NW Eastman Parkway and NW Hogan Road (Volume 1, Appendix 37)
- 2) Text amendments making minor updates to Downtown Plan District Goals sections (Volume 2, 10.314)
- 3) Text amendments establishing a Downtown Plan District Design Manual including Design Principles, seven new Downtown Sub-Districts, new Downtown Street Type standards, and new Downtown Design Guidelines and Standards. The Downtown Plan District Design Manual replaces the existing Downtown Plan District found in Section 4.1100. (Volume 3, Section 4.1100 and Appendix 2.000)
- 4) Text amendments related to the Downtown Plan District including new definitions, the two-track design review process, parking requirements and references, and other minor changes such as codifying current practices for handling scrivener's errors in the Development Code (Volume 3)
- 5) Map amendments replacing the existing Downtown Plan District Sub-Districts (CUC, DT, DR-30, DC-1, DC-2, DR-12) with new Downtown Plan District Sub-Districts (DCC, DMU, DTM, DEM, DRL-1, DRL-2, DCL) and replacing SC designated parcels with DCC, DMU and DTM, TLDR designated parcels with DRL-1, DRL-2 and DTM, CC designated parcels with DCL and DTM and TR designated parcels with DCL. (Volume 2, Appendix C)

Exhibits: 'A' – Text Amendments to Volume 1, 2 and 3, Community Development Plan
'B' – Map Amendments, Community Development Plan
'C' – Downtown Plan District Design Manual (Volume 3, Appendix 3.000)
'D' – Pre-hearing testimony

Recommendation: **Staff recommends adoption of the proposed comprehensive plan amendments.**

SECTION I

EXECUTIVE SUMMARY

Background

The Downtown Plan / Downtown Design Standards project, which is on the 2009 City Council Work Plan, is intended to establish a new vision for a vibrant Downtown Gresham that reflects the current community aspirations and needs; sets the stage for the redevelopment of Downtown; and promotes greater physical and visual connectivity with the Civic Neighborhood. Together, Downtown and Civic Neighborhood are a Regional Center. The 2009 Downtown Plan process continued work begun in 2007 to replace the 1995 Downtown Plan.

The Downtown Plan adopted in 1995 was intended to promote mixed-use development throughout the Downtown and to encourage some of the highest development intensities in Gresham with a wide range of commercial, office and residential uses, along with appropriate Community Services. The plan included coordinating street and parking management along with enhancing public transit and pedestrian circulation. The 1995 plan also envisioned enhancement of the Downtown's urban design including the creation of an architectural review body.

In 2007 the City began a process to update the 1995 plan. The 2007 process worked closely with a stakeholders committee that included Downtown residents, business owners and the Gresham Downtown Development Association as well as effected agencies such as Tri-met, the Planning Commission and Metro. The 2007 work included a first update of the original 1995 vision. Aspirational statements about that addressed future land use/development, access/mobility, housing, design and special attractors were developed. The major themes of the overall vision were that:

- Downtown serve as the mixed-use, pedestrian-oriented center of "focus" of the community;
- Downtown be strongly connected to Civic Neighborhood;
- The current public-private sector partnership be continued and strengthened to ensure future investment and realization of the vision.

The 2007 work also included extensive research on existing conditions; housing and commercial market analysis; opportunity sites; transportation system; and assets, barriers and opportunities. Recommendations were made regarding amending the Comprehensive plan map and text; adopting mandatory architectural design standards; improving the transportation system, developing strategies to fund CIP projects and strategies to attract desired development types and market Downtown.

Work on the Downtown Plan update continued in 2008 with a focus on refining the work of 2007, securing greater community feedback on the Downtown Plan and implementing the recommendations from the 2007 vision as well as refining the Downtown vision. The process included developing and evaluating land use alternatives. This step included an extensive public outreach process that developed and then evaluated three different land-use scenarios for the Downtown. Out of this process, a single Land Use Framework Map was created that provided a land use vision for the future of Downtown that illustrated land use sub-areas showing characteristics, improved transportation connections and potential park/plaza locations.

Also begun in 2008 and finished in 2009 (effective date is May 7, 2009) was the legislative process to adopt a new Downtown Plan District findings report (Volume 1) and new set of Goals, Policies and Action Measures (Volume 2). The amendments to the Community Development Plan were based on the work done in 2007 and 2008, including the public outreach process. The Goals, Policies and Action Measures were grouped under six thematic categories: Vision, Land Use, Urban Design, Transportation & Connections, Parks & People Places and Economic Development. The goals for these six categories are:

- **Vision:** Downtown will be the recognized center of Gresham, and will include most significant civic and governmental functions, including public parks and the Center for the Arts. It will include

large numbers of professional sector jobs, medium and high density residential development and a thriving and unique entertainment, nightlife and shopping district.

- **Land Use:** Make Downtown a thriving, mixed-use, active part of the Regional Center and the focus of the community.
- **Urban Design:** Make Downtown a special place that is visually interesting and that has buildings and streetscapes of high design quality.
- **Transportation & Connections:** Develop a transportation system that supports the vision of a vibrant Downtown and provides for the safe and efficient movement of pedestrians, automobiles, bicycles, transit and emergency vehicles.
- **Parks & People Places:** Create a cohesive and linked public and private system of parks, plazas, courtyards, gardens, and major pedestrian streets/paths, etc. that will help make Downtown a great place to live, work and visit.
- **Economic Development:** Use development tools and incentives to encourage redevelopment of Downtown and the creation of more businesses and housing.

Goals, Policies and Action Measures provide the direction and the actions that the City can take to implement the Downtown Vision and are the basis for the proposed amendments.

Also in 2008, the City Council established a Design Commission consisting of seven members with professional design experience in architecture, urban design, and landscape architecture and planning, as well as members with practical construction experience. Responsibilities of the Design Commission include advising Council, Planning Commission and staff on design issues and reviewing new construction as Design Districts are established.

The next step following the Land Use Framework and the adoption of Goals, Policies and Action Measures was to create a Downtown Plan District Design Manual. In addition to continuing to have extensive public outreach work on the Design Manual was reviewed with the Design Commission as well as the Planning Commission. The Design Manual establishes:

- A two-alternative Design Review Process with a discretionary process and a clear and objective process.
- Design Principles that are the general, over-arching statements and considerations that guide the design of the built environment.
- Downtown Sub-Districts and Development Standards that prescribe the basic building envelope, permitted uses, building heights, floor area ratios, densities, setbacks and other regulations.
- Street Type Standards that provide direction concerning building locations and relationships to adjacent streets, multi-modal circulation, and provision of public spaces and pedestrian amenities.
- Guidelines and standards. Development guidelines and standards provide the means for a development to show consistency with the Design Principles. Guidelines are the basis for the discretionary process and standards for the clear and objective process.

The Downtown Plan District Design Manual was designed to provide a regulatory framework to ensure that the City's physical environment – uses, buildings, streetscapes, pedestrian environments and open spaces – matches the City's vision for Downtown. Downtown is envisioned as the vital, active, social center for the community where people live, work, shop and play in an exceptional, sustainable environment. The City has an aspiration that Downtown add approximately 3,300 housing units and 6,000 jobs through a mix of commercial, office, mixed-use and residential uses that provide unique opportunities for high-quality development while allowing the flexibility to respond to market conditions.

Proposed Comprehensive Plan Amendment Overview

Volume 1 (Findings)

Text amendments to the Downtown Plan District findings report describe adding the Downtown Plan District area properties generally north of NW Division Street and fronting the north side of NW Burnside Road between NW Eastman Parkway and NW Hogan Road. The properties currently have corridor, station center or residential designations: CC, CMU, SC, TLDR and TR. See Table 1.

The areas to be added have special characteristics that are not common to the rest of the City. The areas are inside the Regional Center area indicated on the Metro 2040 Growth Concept Map. Metro Regional Centers are "centers of commerce and local government services serving a market area of hundreds of thousands of people, regional centers become the focus of transit and highway improvements. They are characterized by two- to four- story compact employment and housing development served by high-quality transit." The areas also are near MAX light-rail stops and several bus lines that converge on the Gresham Transit Center.

While the areas are in or near the Regional Center, the Downtown core and high-capacity transit, much of the area has developed as low-density, auto-oriented development. Many parcels are underutilized considering their proximity to the center of the City.

The location of the added area north of Division Street also provides a special characteristic because of its location along Burnside. This highly traveled east-west route through Gresham provides an economic opportunity to announce to passers-by that they are in the center of Gresham. Incorporating this area into the Downtown Plan District will allow the City to encourage a more urban character that better relates to the Downtown core and presents the center of the City as a place that is more than somewhere to drive through.

Volume 2 (Goals, Policies and Action Measure)

Text amendments making minor updates to Downtown Plan District Goals sections are proposed generally to add language in the background and summary sections that are changed due to adding the additional acreage to the Downtown Plan District area and to reflect recent work.

Volume 3 (Code)

- Revise **Article 3 Definitions** to add or modify definitions.
- Delete the current **Section 4.1100 Downtown Plan District** and replace it with a new set of development rules for Downtown. The new set of regulations has been referred to as the "Design Manual" throughout the project. This includes:
 - Creating seven new Downtown Sub-Districts to replace existing districts. Table 1 shows the number of lots that changed from current sub-districts to new sub-districts.

Table 1: Downtown Plan District properties: land-use sub-district changes

Current sub-districts	Proposed sub-districts							Total
	DCC	DCL	DEM	DMU	DRL-1	DRL-2	DTM	
CC	0	64	0	0	0	0	4	68
CMU	0	12	0	0	0	0	0	12
CUC	297	0	0	5	0	0	2	304
DC-1	0	0	7	4	0	0	0	11
DC-2	23	64	14	0	0	0	0	101
DR-12	2	0	0	2	11	0	0	15
DR-30	31	0	0	27	0	61	28	147
DT	23	9	13	0	0	129	150	324
SC	5	0	0	5	0	0	35	45
TLDR	0	0	0	0	68	14	5	87
TR	0	1	0	0	0	0	0	1
Total	381	150	34	43	79	204	224	1,115

- o Expanding the Downtown Plan District to include an area roughly bounded by Eastman Parkway to the west, Division to the south and Burnside to the north (including properties on both sides of Burnside).
- o Providing, in general, more flexibility in permitted land uses, including allowing small-scale manufacturing (as an accessory to other commercial uses), information services, and live-work. Some areas also were changed from primarily commercial districts to allowing a range of office, retail, service, and housing. The changes provide additional limits on auto-dependent uses, drive-throughs, and outdoor storage.
- o Providing a new approach for areas where commercial uses are required on the ground floor. The current approach is to require first-floor commercial in the existing Central Urban Core, Downtown Commercial-1 and Downtown Commercial-2 Sub-Districts. The proposed approach limits the commercial requirement to certain key shopping streets: Main, Stanley (a new street envisioned as Town Fair redevelops), Third and part of Powell. All-residential buildings (condominiums and apartments with no first-floor commercial) will be allowed in those districts, except on those key shopping streets.
- o Providing two different Sub-Districts with residential emphases that reflect the existing and desired character for different areas Downtown.
 - DRL-1: Applies to Ava Avenue and the area west of Gresham High School. Allows small-scale commercial uses up to 5,000 square feet only along Main Ave. and Ava Ave. DRL-1 also allows single-family detached and duplexes but no single-family attached or apartments/condominiums. The building height limit is 35-feet.
 - DRL-2: Applies to an area generally bounded by Elliott, Sixth, Second, and Liberty and a second area west of Main Ave. and north of 14th Street. DRL-2 allows small-scale commercial up to 7,500 square feet, single-family detached, single-family attached, and apartments/condos with a 50-foot height limit.
- o Allowing more development potential in many areas, such as more height in most of Downtown. Heights limits are proposed to be lower in some areas along Burnside.
- o Providing Design Guidelines and Standards that will ensure higher-quality site and building design.
- **Revise Article VII Site Design Review Standards and Criteria** to reflect the Design Commission review processes for Downtown and clarify that the Downtown Plan District takes precedence over most provisions.

- Modify **Section 7.0300 Innovative Housing Demonstration Projects** to allow demonstration projects in DRL-1 and DRL-2.
- Revise **Section 9.0800 Parking** to update parking regulations for Downtown, including putting a 40,000-square-foot cap on how large a surface parking lot can be. This is an interim measure until the City can finalize new parking ratios and strategies as part of the Centers Parking Strategy project and Downtown Plan implementation action plan. This work is anticipated to occur in the summer and fall of 2009.
- Update **Article 11 Gresham Procedural Code** to modify the Design Review process for Downtown to finalize the Design Commission's role and clarify the application, review and appeal processes. The Design Commission will review most projects Downtown, whether using discretionary Guidelines or the clear and objective Standards. Appeals will be heard by the City Council.
- Other minor text amendments including adding language in **Article 2** that codifies current practices for handling scrivener's errors in the Development Code and adding/correcting references.
- Add the Design Manual as **Appendix 2.000**. The Design Manual is Section 4.1100 with the addition of an attractive layout and numerous additional pictures and graphics to explain the standards and guidelines.

4.1100 Downtown Plan District Design Manual (Volume 3:-- Appendix 2.000)

The Downtown Plan District includes the following elements:

- An introduction, including an explanation of the two-track process. Under this process, an applicant will be able to choose between going through a discretionary process governed by Guidelines and following clear and objective Standards. Both the Guidelines and Standards were generated while keeping in mind the Downtown-design principles. The Design Commission will be the review body for all discretionary reviews and for larger clear and objective reviews.
- Design Principles, which are nine overarching statements that provide the foundation for the design of Downtown's built environment. The nine principles are:
 - Offer a Vibrant Mix of Uses and a Variety of Housing Types.
 - Promote Excellence in Design and Architectural Expression.
 - Create a Unique and Exciting Public Realm.
 - Create strong connections between Sub-Districts and Plan Districts.
 - Incorporate Sustainability.
 - Provide Context Sensitivity.
 - Preserve and Enhance Historic Context.
 - Create Appropriate Transitions in Height, Bulk, and Scale.
 - Increase Transportation Accessibility.
- New land-use Sub-Districts, including a new map and new standards for height, development intensity and permitted uses.
- Street type designations, which provide details on street and sidewalk width and character as well as building setbacks. These street types will control the relationship between the public realm of streets and sidewalks and the private realm (buildings, landscaping, stoops, etc.).
- Design Guidelines, both Downtown-wide and specific to Sub-Districts, for the discretionary track.
- Design Standards, both Downtown-wide and specific to Sub-Districts, for the clear and objective track.
- Definitions, which will be repeated in Article 3 of the Development Code.

Map Amendments (Volume 2 – Appendix C)

The Development Plan Map proposed amendments (see Figure 1):

- Replace the six current sub-districts (CUC, DT, DC-1, DC-2, DR-30, DR-12) with seven new sub-districts (DCC, DMU, DTM, DEM, DCL, DRL-1, DRL-2).
- Replace existing land-use districts north of Division (TLDR, CC, CMU, SC) with new Downtown sub-districts.
- Change one property along the Powell corridor from TR to DCL.

Figure 1: Existing sub-districts

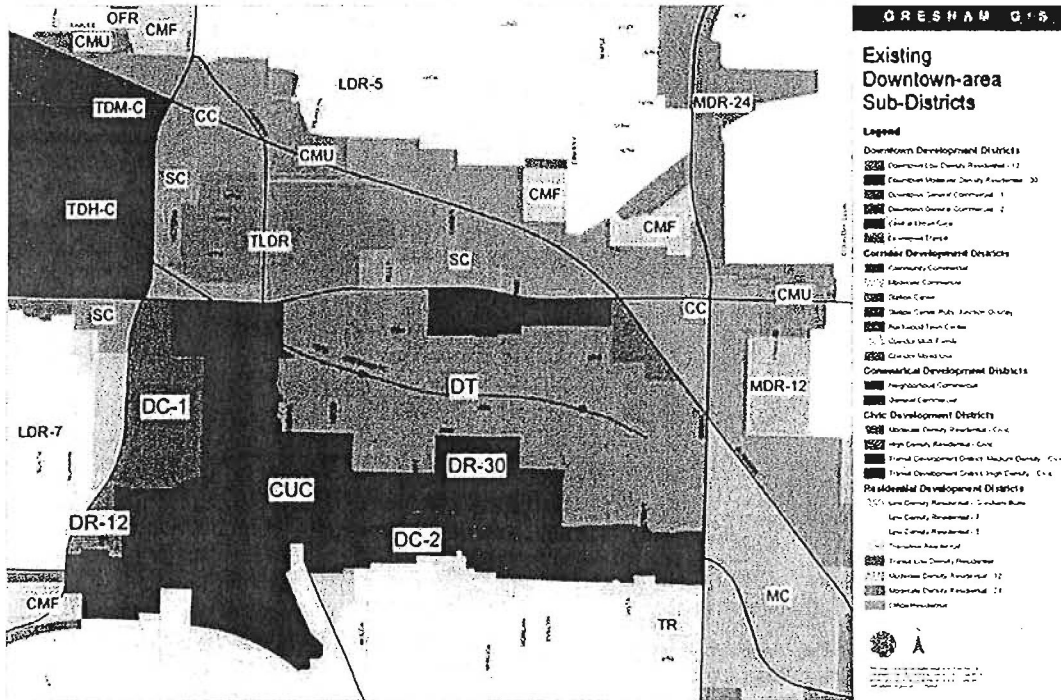
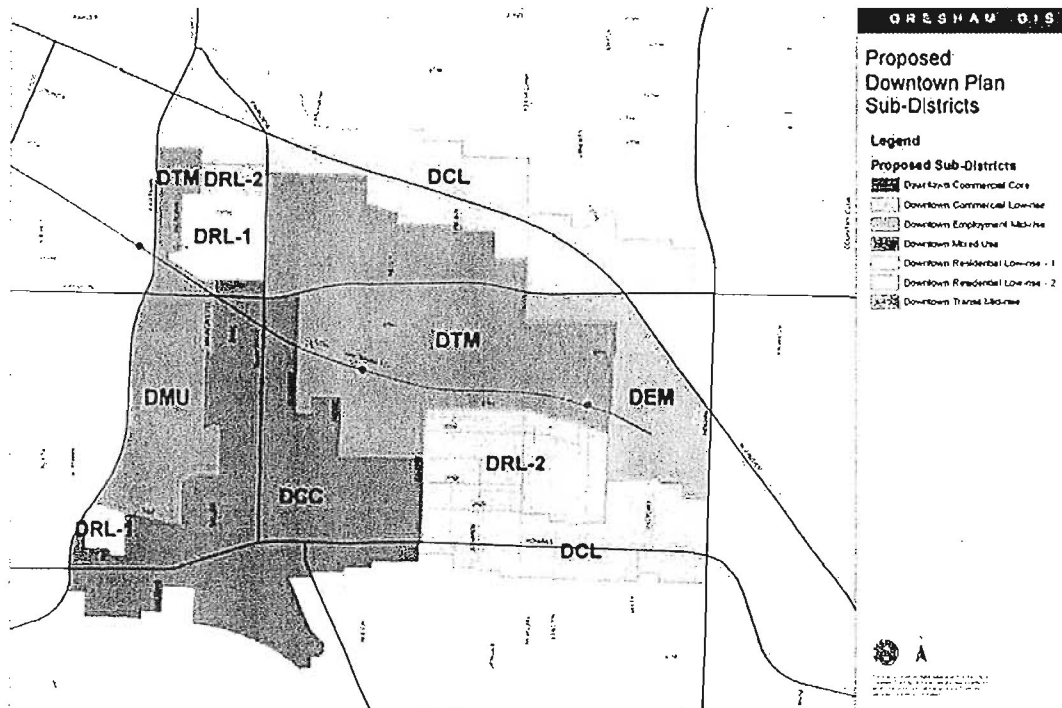


Figure 2: Proposed sub-districts



Staff Report Organization

- Sections II and III identify those current Community Development Plan procedures and policies that apply to the proposal.
- Section IV identifies the applicable Metro Urban Growth Functional Plan (UGMFP) titles and Statewide Planning Goals that apply to the proposal.
- Section V identifies applicable Statewide Planning Goals that apply to the proposal.
- Section VI contains specific findings of fact that detail how the proposal is consistent with Sections II through IV:
 - Subsection A is findings of fact for the Community Development Plan procedures.
 - Subsection B is findings of fact for the Community Development Plan policies.
 - Subsection C is findings of fact for the UGMFP Titles.
 - Subsection D is findings of fact for the Oregon Statewide Planning Goals.
- Sections VII and VIII summarizes staff conclusions and recommendations.
- Exhibit 'A' includes proposed amendments to Volume 1 (Findings), Volume 2 (Policies) and Volume 3 (Development Code) as well as commentary. The commentary provides additional findings for this proposal.
- Exhibit 'B' is the proposed new Community Development Plan Map.
- Exhibit 'C' is the proposed Downtown Plan District Design Manual (Volume 3 -- Appendix 2.000)

SECTION II

APPLICABLE COMMUNITY DEVELOPMENT CODE PROCEDURES

Section 11.0400	Legislative Actions
Section 11.0205	Type IV Procedure – Legislative
Section 11.0300	Public Deliberations and Hearings
Section 4.1000	Plan Districts

SECTION III

APPLICABLE COMMUNITY DEVELOPMENT PLAN GOALS & POLICIES

Section 10.014	Land Use Policies and Regulations
Section 10.100	Citizen Involvement
Section 10.314	Downtown Area Development
Section 10.600	Statewide Planning Goal 10: Housing

SECTION IV

APPLICABLE METRO URBAN GROWTH FUNCTIONAL PLAN TITLES

Title 1	Requirements for Housing and Employment Accommodation
Title 6	Central City, Regional Centers, Town Centers And Station Communities
Title 8	Compliance Procedures

SECTION V

STATEWIDE PLANNING GOALS

Goal 10	Housing
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SECTION VI

FINDINGS OF FACT

The proposed Community Development Plan amendments attached as Exhibit 'A' and 'C' and Community Development Plan Map attachment Exhibit 'B' are consistent with all applicable procedures, goals and policies of the Community Development Plan and applicable titles of the Metro Urban Growth Management Functional Plan as indicated in the following findings.

A. Community Development Code Procedures

- 1. Section 11.0400 - Legislative Actions.** This section requires that an amendment to the Community Development Code and the Community Development Plan be a legislative action under the Type IV Procedure pursuant to this section. This section applies to this proposal, as it is an amendment to the Community Development Code and the Community Development Plan.
- 2. Section 11.0205 - Type IV Procedure - Legislative.** This section requires that the Planning Commission shall hold a public hearing and make a recommendation to the Council. The Council shall hold another public hearing and make a final decision. Interested persons may present evidence and testimony relevant to the proposal. The Planning Commission and Council will make findings for each of the applicable criteria. The section also provides for a hearing process consistent with Section 11.0300. Both the Planning Commission and the City Council, at public hearings in conformance with provisions of

this section, will consider this proposal. Findings are made for the applicable criteria in this report or as revised in the record.

3. Section 11.0300 – Public Deliberations and Hearings. For a Type IV Comprehensive Plan Amendment this section requires that hearings be scheduled, a notice published in a newspaper of general circulation in the City and a copy of the decision be mailed to those required to receive such notice. Required notice of public hearing for these proposed text amendments has been published in the Gresham Outlook, as required by this section. The Planning Commission will make a recommendation and the Council will make a decision that will be based on findings of fact contained in this report and in the hearings record and a decision will be sent to those who participated in the hearings. A decision shall be made accompanied by findings and an order.

Additionally, a Measure 56 notice was distributed as part of this Hearing. Measure 56, contained in ORS 227.186, requires that a city send a notice between 40 and 20 days before the first evidentiary hearing to all property owners in land use districts where the proposed change may affect the permissible uses of the property. A Measure 56 notice was sent to all property owners who will see a change in land use district. The Measure 56 notice was sent on April 17, 2009, no sooner than 40 days and no later than 20 days before the first evidentiary hearing.

B. Community Development Code Plan District Enabling Legislation (4.1000)

1. Section 4.1000 – Plan Districts. This section provides that a Plan District can be amended through a Type IV legislative procedure. These Community Development Plan and Community Development Plan amendments will be completed using a Type IV legislative procedure. They amend the Downtown Plan District by expanding its boundaries to include areas bounded by Division Street, Eastman Parkway, and Burnside Road, including properties on the north and east sides of Burnside Road, as shown in Exhibit 'B'.

A. A Plan District may be designated when the city finds that conditions within a specific area are such that unique planning and regulatory tools are required to achieve desired results. A Plan District designation may be warranted based on specific land use, economic, transportation, public facilities, historic, or natural conditions found to exist in the area. Plan District designation provides a means to create or modify development districts and standards in ways which address specific opportunities and problems within the plan area. These new or modified development districts and standards are not transferable to properties outside the boundaries of the established Plan District. The Plan District designation is generally not intended to be applied to small areas or to small individual properties.

Findings

Findings under this section were made for establishing the Downtown Plan District when the district was originally adopted. As provided in subsection (B) below a Plan District can be modified when finding are made that it meets the specific criteria required for establishing a plan district.

Conclusions

The Downtown Plan District has been previously established as consistent with this section and the amending of the Downtown Plan District by adding approximately 220 acres north of Division Street and selected parcels on the south side of the Plan District is consistent with the subsection and the applicable provisions of 4.1000.

B. Except as provided in Section 4.1000(E), a Plan District shall be established, amended, or removed at the initiative of the Planning Commission or the City Council through the Type IV legislative procedure. In establishing a Plan District, findings satisfying all of the following criteria must be made:

1. The area proposed for the Plan District designation is generally affected by special characteristics or problems of a land use, economic, transportation, public facilities, historic, natural, or transitional use or development nature which are not common to other areas of the city.

2. *Existing development districts and standards applying in the area are inadequate to achieve goals and implementation strategies of the Community Development Code, or to address an identified problem in the area.*
3. *The proposed Plan District designation is the result of a study or plan which documents the special characteristics of the study area and includes measures to address the relevant issues.*
4. *Any proposed policies, procedures, development standards, or other measures to be implemented are in conformance with the purposes, findings and recommendations of the study or plan which serves as the basis for the Plan District.*
5. *The Plan District designation, and related policies, procedures, standards, and other measures are consistent with applicable policies and implementation strategies of the Community Development Code, and with any applicable locational criteria identified in the Community Development Code.*

Findings

The proposal for amending the Downtown Plan District is being processed under the Type IV legislative procedure.

1. The areas to be added have special characteristics that are not common to the rest of the City. The areas are inside the Regional Center area indicated on the Metro 2040 Growth Concept Map. Metro Regional Centers are "centers of commerce and local government services serving a market area of hundreds of thousands of people, regional centers become the focus of transit and highway improvements. They are characterized by two- to four- story compact employment and housing development served by high-quality transit." The areas also are near MAX light-rail stops and several bus lines that converge on the Gresham Transit Center.

While the areas are in or near the Regional Center, the Downtown core and high-capacity transit, much of the area has developed as low-density, auto-oriented development. Many parcels are underutilized considering their proximity to the center of the City.

The location of the added area north of Division Street also provides a special characteristic because of its location along Burnside. This highly traveled east-west route through Gresham provides an economic opportunity to announce to passers-by that they are in the center of Gresham. Incorporating this area into the Downtown Plan District will allow the City to encourage a more urban character that better relates to the Downtown core and presents the center of the City as a place that is more than somewhere to drive through.

Also, a problem identified is a lack of high-quality design in some developments in the City's center, including the areas to be added to the Downtown Plan District.

2. The proposed amendments provide numerous additional guidelines and standards that control site and building design and an entirely new process by which the Design Commission will review projects in the Downtown Plan District. These include requirements for sidewalk width, placing the building on the street frontage, setbacks designated for each street type, parking placement, landscaping, walkways, building massing, façade composition, and ground-level building details. These provide the City much greater control over the function and appearance of the added Plan District areas than the current code allows and provide applicants much more flexibility in how they meet the City's site and building design intent.

The current land-use districts or sub-districts have inadequate standards and processes to accomplish the land-use and urban design envisioned for the areas. For example, the Community Commercial land-use district found along Burnside Road in the existing Development Plan Map provides no standards for how much of the street frontage must be occupied by a building.

The few parcels added to the south of Powell have joint ownership with other parcels, are along or near a Downtown Plan District street type frontage or are adjacent to more intense uses that make them

associated with the Downtown Plan District rather than the adjacent residential area. These also require the land-use designations and site and building design standards to achieve the Downtown vision.

3. Community Development Plan Volume 1, Comprehensive Plan Findings, Appendix 37 contains the Downtown Plan study on which the proposed amendments are based, which was based on two years of study.

4. The Downtown Plan District's proposed amendments are consistent with the goals and policies in Section 10.314 of Volume 2 of the Community Development Plan, which when approved were found to be consistent with the Downtown Plan Section findings (Community Development Plan Volume 1, Comprehensive Plan Findings, Appendix 37).

5. The findings of fact made in relation to section 10.314 in this staff report are the applicable provisions that apply to the proposed modification of the Downtown Plan District. This section made findings and conclusions that the proposal is consistent with these applicable policies, goals, and action measure.

Conclusions

The process for approving these proposed amendments are consistent with the requirements.

1. The areas to be added to the Downtown Plan District have been identified as within the area Metro has designated as in the Regional Center's sphere of influence. The findings also have established that the areas have special characteristics and problems that are not common elsewhere in the City. The proposal is consistent with this subsection.

2. Gresham's existing code lacks the processes and guidelines/standards to address the problems and characteristics and implement the City's vision for the Downtown area. The proposed amendments have been crafted to address these. The proposal is consistent with this subsection.

3. The Downtown Plan District was based on two years of study and the Downtown Plan. This proposal is consistent with this subsection.

4. The proposed amendments are a direct result of the planning process that resulted in the Downtown Plan and would implement that plan. The proposal is consistent with this subsection.

5. Based on findings and conclusions in this staff report in relation to 10.314 Downtown Plan District, the proposal is consistent with this subsection.

C. Community Development Plan Goals and Policies (Volume II)

This section identifies the applicable Community Development Plan goals and policies. The text (*italicized*) of the policy is followed by corresponding findings and conclusions. The applicable policies are grouped by general categories.

1. General Goals & Policies

Section 10.014 Land Use Policies and Regulations

Section 1. Land Use Policies and Regulations.

Goal: Maintain an up-to-date Comprehensive Plan and implementing regulations as the legislative foundation of Gresham's land use program.

Policy 1: The City's land use program will be consistent with state and regional requirements but also shall serve the best interests of Gresham.

Policy 2: The City's land use regulations, actions and related plans shall be consistent with and implement the Comprehensive Plan.

Policy 20: The City shall periodically review and update the Comprehensive Plan text and the Community Development Plan Map(s) to ensure they remain current and responsive to community needs; provide reliable information and dependable, factually based policy direction, and conform to applicable state law, administrative rules and regional requirements.

Policy 21: Council may, upon finding it is in the overall public interest, initiate legislative processes to change the Comprehensive Plan text and Community Development Plan Map(s) and Development Code.

Findings

Policy 1, 2, and 21: The proposed amendments are part of the Downtown Design Standards project, which was requested by the Gresham City Council to provide an update to Gresham's Comprehensive Plan and its Downtown Plan District. The City Council endorsed the project by adopting it in its 2008 and 2009 work plans and officially initiated Downtown Plan code amendments in 2008. Gresham's Comprehensive Plan has been found in compliance with state and regional requirements, and the proposed amendments also are in compliance with Gresham's code and state and regional requirements, as described in Sections II, III, IV, V and VI of this staff report. The proposed amendments were created during a collaborative planning process that involved residents, property owners, business owners, elected and appointment officials and other interested parties.

Policy 20: The Downtown Plan District has not received significant review since it was enacted in 1995. The 2007-2009 Downtown Plan process conducted an extensive review of community needs and vision, which led to the proposed amendments. The City's outreach included community workshops, Downtown Focus Group meetings, Development Group meetings, Planning Commission and Design Commission work sessions, and individual meetings with stakeholders.

Conclusions

Policies 1 and 2 are addressed because the proposed amendments provide an update to the City of Gresham's Development Code, which is part of the City's Comprehensive Plan and implements the City's land-use regulations. The updates are consistent with state and regional requirements.

Policies 20 and 21 are addressed because the proposed amendments were completed in response to the City Council's direction and are responsive to community needs identified and clarified during public outreach.

The proposal is consistent with the applicable general goals and policies listed in this section.

2. Citizen Involvement Goals & Policies

Section 10.100 - Citizen Involvement

Goal: The City shall provide opportunities for citizens to participate in all phases of the planning process by coordinating citizen involvement functions; effectively communicating information; and facilitating opportunities for input.

Policy 1: The City shall ensure the opportunity for citizen participation and input when preparing and revising policies, plans and implementing regulations.

Policy 2: The City shall consider the interests of the entire community and the goals and policies of the Comprehensive Plan when making decisions.

Policy 3: The City shall foster regular and ongoing two-way communication between citizens and City elected and appointed officials.

Policy 4: The City shall provide opportunities for meaningful citizen involvement on site development proposals.

Policy 7: The City shall facilitate involvement of citizens in the planning process, including data collection, plan preparation, adoption, implementation, evaluation and revision.

Policy 8: The City shall ensure that citizen concerns are considered in land use decisions and shall provide feedback to the public regarding how these concerns have impacted decisions.

Policy 10: The City shall ensure the opportunity for the public to be involved in all phases of planning projects and issues.

Policy 11: The City shall ensure that the public has complete and timely access to all public information concerning land use projects and issues. This includes private development proposals once they are in the formal application process.

Findings

The public involvement goals and policies establish the City's intent that its citizens have meaningful opportunities throughout a planning project to be informed and to affect proposals.

City staff has conducted a robust and collaborative public outreach campaign to share information with and receive input and ideas from stakeholders and other interested parties. This included working with stakeholders on the 2007 Downtown Development Strategy project and working with residents, business and property owners, and elected and appointed officials to identify issues and opportunities Downtown, craft a land-use framework, prepare development concepts on key study areas and draft a Design Manual and other text amendments. This outreach included:

- Public workshops were held on the following dates to review the issues, discuss alternatives, or solicit feedback on the proposed Map and Code amendments: April 10, 2008; June 4, 2008; August 6, 2008; November 12, 2008; Jan. 7, 2009; and March 4, 2009.
- Downtown Focus Group meetings were conducted at key points in the process to receive feedback. The Downtown focus group included property owners, developers, real estate professionals, neighborhood representatives, and business owners.
- Staff met with the Gresham Downtown Development Association staff and board and the Historic Gresham Downtown Business Association leaders and members to solicit ideas and feedback.
- City staff met with the Neighborhood Coalition on April 14, 2009.
- Postcards about the draft Design Manual were mailed to property owners on Feb. 24, 2009.
- A Measure 56 notice was mailed to property owners on April 17, 2009.
- Advertisements regarding the map and code changes and community workshops were published in The Outlook and The Oregonian before the January workshop and in The Outlook before the March workshop.
- City staff prepared project documents (including public input summaries) and posted them on a project Web page on the City's Web Site. Interested parties were invited to evaluate the documents and propose revisions and new ideas.
- Information on the project has been made available at other Comprehensive Planning workshops.
- The askGresham e-mail tool has been used to alert interested parties when new materials are available on the Web site and when upcoming meetings will occur.
- Project information has been available at the Urban Design & Planning office.
- Planning Commission and Design Commission work sessions have been held throughout this process.

In addition, during an extensive Residential Districts Review process in 2008, staff discussed with the public what locations in Gresham were most appropriate locations for dense housing. Downtown was identified as an appropriate location during that public outreach process.

Conclusion

The *Citizen Involvement Goal (10.100)* and its policies are met by the combination of workshops, mailings and meetings as well as providing information on the proposal on the City Web site.

The proposal is consistent with the applicable citizen involvement goals and policies listed in this section.

3. Downtown Plan District

Section 10.314 – Downtown Vision Goal

Goal: Downtown will be the recognized center of Gresham, and will include most significant civic and governmental functions, including public parks and the Center for the Arts. It will include large numbers of professional sector jobs, medium and high density residential development and a thriving and unique entertainment, nightlife and shopping district.

Findings

The proposed amendments as a whole address making Downtown the center of Gresham, and allowed land uses and intensities support the creation of employment, adding residential units and providing an active, lively, diverse, thriving Downtown.

Conclusion

The proposal is consistent with the Downtown Vision Goal.

Section 10.314 – Land Use

Goal: Make Downtown the recognized business and social center of Gresham as a thriving, unique, mixed-use part of the Regional Center with connection to the Civic Neighborhood..

Policy 1: Provide a mix of land uses that will offer opportunities for people to live, work, shop, play and access cultural and entertainment activities. Downtown will include:

- a. Significant civic and government functions, including parks and the Center for the Arts,*
- b. Office buildings that will provide large number of professional sector jobs.*
- c. Medium and high density residential development that will substantially increase Downtown's population.*
- d. A shopping district that offers unique goods, entertainment, cultural activities and nightlife.*

Policy 2: Designate areas for medium and high-density residential development that will provide a variety of housing types for people of all income levels and that will support a vibrant shopping district. Allow the highest densities of housing near the Downtown core and the MAX line.

Policy 3: Create a thriving, pedestrian-friendly Downtown core area that will be the heart of Downtown and will have a mix of uses that encourage a variety of activities during at least 18 hours of each day by:

- a. Being visible and accessible from arterial streets that border Downtown and from the MAX line.*
- b. Having commercial areas appropriately sized for Downtown based on market potential and urban design considerations.*
- c. Having a uniquely Gresham character and being complementary to the Civic Neighborhood.*
- d. Having designated shopping streets (such as Main and Third) where commercial uses are found on the first floor of new buildings.*

Policy 4: Encourage redevelopment at key locations, such as the Gresham Town Fair site at the southeast corner of Division and Eastman Parkway, and the PGE site at the southwest corner of Burnside and Eighth. Provide more intense uses, such as major employment uses, that are better connected and compatible to the rest of Downtown and in the case of the Gresham Town Fair site, better connected to Civic Neighborhood.

Policy 5: Encourage the location of at least one major civic use anchor (such as a new city hall, library or a higher education institution) in or near the Downtown core area.

Policy 6: Provide housing for citizens with special needs, such as the elderly, and those requiring care for disabilities.

Policy 7: Aspire to provide the following in Downtown:

- 6,000 jobs
- 3,300 residences
- A wide variety of amenities, goods and services that will result in the Downtown becoming an "18 hour district"

Action Measures:

1. *Develop a new Downtown Plan with a new plan map, land use districts and development standards that will be guided by the above policies and Land Use Framework.*

Findings

The proposed Downtown sub-districts provide a wide range of allowed land uses that include single-family residential, duplexes, townhomes, apartments/condominiums, mixed-use, commercial, employment (small-scale manufacturing and information services), restaurants, entertainment, parks, community services (such as elderly and special needs housing and civic uses), and cultural facilities. This provides the opportunity for many different activities Downtown, and these activities will take place at various times of day. For example, office workers may be active during the day and residential and restaurant uses may be more active in the evening.

The sub-districts are:

Downtown Commercial Core (DCC): The DCC is the City's long-standing center and features unique local businesses, small-scale storefronts, and intimate sidewalks. Main Avenue has a small-scale, walkable quality appreciated by residents and visitors. This Sub-District is intended to preserve this small-scale character on Main Avenue while encouraging an active, engaging mix of old and new uses. The DCC Sub-District allows a wide range of uses – retail, service, office and residential – that will help create a vibrant Sub-District that is active all day and much of the night.

Downtown Mixed Use (DMU): This Sub-District is intended to evolve over time from a shopping center largely organized around automobile trips and parking into a mixed-use Sub-District with jobs, housing and commercial opportunities. This full-service Sub-District will contain new shopping streets, public spaces and better pedestrian and automobile connections to the DCC Sub-District and to the Civic Neighborhood. This DMU Sub-District allows a mixture of employment, retail, office and residential uses in a very dense, compact urban form by permitting the most intense, tallest development in Downtown.

Downtown Transit Mid-Rise (DTM): This mixed-use Sub-District provides a mid-rise, mixed-use character in the center of Downtown near the light-rail stations. Because of its proximity to transit, this Sub-District provides access opportunities for those who live Downtown to use buses and the MAX light rail to get to jobs and other destinations. It also supports the creation of employment uses Downtown so those who live outside the Downtown have opportunities and easy access to work Downtown. This Sub-District supports the continued presence of institutional uses, such as government offices. It also allows a mix of residential, commercial and employment uses at a mid-rise intensity.

Downtown Employment Mid-Rise (DEM): This mixed-use area is envisioned as an area that could support significant employment, whether retail or office in nature. It has excellent access to light rail as well as

several major streets – Hogan, Burnside, and Division. Buildings are allowed to have multiple stories with larger footprints here to accommodate market demand. This Sub-District allows for a substantial amount of general office, financial, corporate and institutional uses that employ large numbers of people. It also allows a significant retail presence and residential uses.

Downtown Residential Low-Rise-1 (DRL-1): This mixed-use Sub-District will encourage some residential areas to gently transform into a broader mix of residential uses. This Sub-District is intended to create distinctive, walkable neighborhoods within a short distance of transit and the Downtown core. The Sub-District encourages single-family homes to remain and allows duplexes and townhomes but not attached dwellings on a single lot. This Sub-District also will allow small-scale commercial uses only on certain streets where it is most appropriate.

Downtown Residential Low-Rise-2 (DRL-2): This mixed-use Sub-District will allow a gradual transformation into more varied and full-service residential neighborhoods that can take advantage of their proximity to transit and nearby shopping and job centers. This predominantly residential Sub-District will allow single-family homes to remain while also allowing attached single-family houses, small-scale apartments and condominiums, and small-scale commercial activities.

Downtown Commercial Low-Rise (DCL): This Sub-District contains major corridors with the types of businesses, services, stores, and offices that demand a higher level of automobile access to employees and customers. Structures may be single use and aimed at regional traffic. This Sub-District will still serve this role, but the corridors will become more balanced over time to meet the needs of pedestrians as well as automobile traffic. The Sub-District's character will evolve as buildings and more walkable streets become prominent and parking is located to the side or rear of properties. This Sub-District allows commercial, residential, and employment uses, including auto-related uses such as service stations, auto repair, and car washes.

The proposed sub-districts seek to size commercial areas appropriately for Downtown by designating Main, Third, Stanley (a future Town Fair street), and part of Powell Boulevard as shopping streets where first-floor commercial is required. In the remainder of mixed-use areas, first-floor commercial is not required. This allows property owners and developers to take market demand into consideration when determining what uses to put into new developments and does not require an undersupply or oversupply of commercial space.

Employment potential is higher with the proposed sub-districts because they allow more intense office development in the DMU district (the current Town Fair Shopping Center site) and other sub-districts.

The highest residential densities are in DCC, DMU, and DTM, which are the districts nearest the Downtown core and MAX light-rail stations. Table 2 shows the floor-area ratios allowed and minimum residential densities required in the Downtown Commercial Core, Downtown Mixed Use and Downtown Transit Mid-rise districts. There is no maximum residential density in these three districts because the FAR and building form requirements control the maximum build-out in the structures.

Table 2: Height and FAR requirements

		DCC		DMU	DTM	DEM	DRL-1	DRL-2	DCL
			Main						
Maximum Height ^{A-D} (feet)		85	45	125	85	70	35	50	65
Minimum Floor Area Ratio (base)		1.0	1.0	1.0	1.0	0.5	0.5	0.5	0.5
Maximum Floor Area Ratio		3.0	1.5	3.5	3.0	2.5	1.0	1.0	1.0
Maximum Floor Area Ratio with bonuses	Residential	3.5	2.0	5.0	3.5	3.0	2.0	2.0	2.0
	Non Residential	4.5	3.0	6.0	4.5	3.0	3.0	3.0	2.5
	Combined	5.0	4.0	7.0	5.0	5.0	4.0	4.0	3.0
Minimum Residential Density (units per acre)		17		20	20	17	8.71	8.71	17
Maximum residential density (units per acre)		No max.		No maximum	No maximum	No maximum	20	No maximum	60

Site and building design call for gateway features at prominent entry points to Downtown, including many key sites along boulevards bordering Downtown Gresham. Some gateway requirements for site and building design are to provide gateway plazas and ensure the buildings are oriented toward the corner with high-quality materials on the facades. Connections among sub-districts and the Civic Neighborhood also are encouraged through a gateway treatment at Division and Eastman Parkway and mid-block connections that will give pedestrians a more direct connection. Future streets also are shown for Downtown that will provide access through the current Town Fair site as it redevelops.

The proposed amendments include a new plan map, land use districts and development standards that will be guided by the above policies and Land Use Framework.

Conclusion

The mix and intensity of uses allowed, as well as the urban design features incorporated in the proposed amendments, address the Downtown Area Development – Land Use goal and its policies. Action Measure 1 is addressed in the proposed amendments.

The proposal is consistent with the applicable general goals and policies listed in this section, as well as action measure 1.

Section 10.314 – Urban Design

Goal: Make Downtown a special place that is visually interesting and that has buildings and streetscapes of high design quality.

Policy 1: Apply mandatory design standards to Downtown buildings and streetscapes.

Policy 2: Adopt special design standards for:

- a. *The designated shopping streets of the Downtown core area that will make them more pedestrian friendly by addressing such design elements as having adequately sized display windows, a minimum height for the first story, and quality exterior building materials.*
- b. *Redevelopment of the MAX facility (tracks and stations).*
- c. *How nearby development should relate to the MAX line.*
- d. *The design of the Beech Street "Park Block" (Center for the Arts to MAX).*
- e. *How adjoining development should relate to Beech Street.*

Policy 3: Develop an identifiable Downtown streetscape design that includes features with a unifying design theme, such as public signage and art, landscaping, pedestrian crossings, lighting, and street furniture.

Policy 4: Protect the small-scale character of Main Avenue buildings while allowing higher building heights in other appropriate locations Downtown.

Policy 5: Create good transitions (e.g. avoid abrupt changes in density, uses, building height, scale, etc.) between districts or neighborhoods

Policy 6: Encourage public and private exterior art throughout Downtown such as outdoor sculpture, wall murals, artistic signage and street furniture.

Policy 7: Encourage building/site design to feature extensions of the public realm, such as plazas, courtyards and gardens.

Policy 8: Provide a unique and pedestrian-friendly streetscape that is an interesting, safe and convenient place to walk by requiring, at least in the Downtown core area, that:

- a. *Buildings be located so that they assist in defining and enlivening the public realm. This includes siting new buildings to allow adequate street and sidewalk widths and putting buildings as close to the street as practical.*
- b. *Buildings orient views towards the street and public realm.*
- c. *Parking lots be located to the side or rear of buildings where feasible and screened from street views.*
- d. *Blank walls are limited, and entries and windows or other breaks in the façade face streets to enhance attractiveness and pedestrian interest.*
- e. *Pedestrian-oriented lighting that will help make Downtown a safer place.*
- d. *The convenience and safety of the disabled is provided for.*
- e. *Along non-arterial streets, the size, placement and appearance of signs are oriented to pedestrians rather than to autos.*

Policy 9: Encourage the incorporation of sustainable design and green development practices in new construction or rehabilitation projects.

Policy 10: Designate important viewpoints or view corridors and develop standards for protecting views of Mt. Hood and Gresham Butte.

Policy 11: Accentuate the main gateways into Downtown, such as the Main/Powell area, by the massing of buildings at street corners and/or by providing artwork, landscaping or other ornamental features such as archways.

Action Measures

1. *Develop a design manual with mandatory design principles, standards and guidelines that will apply to future Downtown buildings and streetscapes.*

2. *Create development concepts for specific opportunity areas that will help inform the development of design standards. The concepts should explore alternative use/design scenarios for the height, scale, massing and relationship of potential buildings to the public realm of streets, parks and plazas.*
3. *Develop development code (such as allowing higher buildings) and/or other incentives that will encourage developers to provide:*
 - a. *Plazas, courtyards or other extensions of the public realm.*
 - b. *Outdoor art such as sculpture and wall murals.*
 - c. *Green roofs and other sustainable design features.*
4. *Develop special design standards for:*
 - a. *Building facades/streetscapes of designated shopping streets.*
 - b. *Redeveloping the MAX facility and adjacent area.*
 - c. *Developing the Beech Street "park block" and adjacent area.*
 - d. *Emphasizing the major gateways (street entries into Downtown) that are shown on the Issues and Opportunities Map.*
 - e. *Promoting a unifying design theme for streetscapes including public signage, art, landscaping, pedestrian crossings, street lights, street furniture, etc.*
 - f. *Protecting important viewpoints of Mt. Hood and Gresham Butte.*
5. *Designate areas of Downtown that can have unlimited building height.*

Findings

The proposed amendments institute a mandatory design review for Downtown development applications with guidelines and standards.

These guidelines and standards create special standards for key shopping streets and special streets such as Beech by requiring a minimum percentage of storefront glass, requiring a minimum percentage of the building to be on the front setback line, requiring quality building materials, and ensuring first-floor heights are adequate for retail where appropriate. In addition, bonus FAR is allowed for projects that incorporate additional landscaping along the MAX pathway.

The proposed amendments limit heights to 45 feet within 110 feet of the centerline of Main Avenue between Powell Boulevard and Fifth Street, and a 10-foot setback is required above the third story. This will protect the small-scale character of the Main Avenue shopping street.

The proposed amendments detail some of the streetscape for Downtown streets by identifying amenity zones for landscaping and street furniture, requiring a minimum sidewalk width and requiring pedestrian lighting on Downtown buildings. A follow-up streetscape plan will be necessary to provide a higher level of detail on Downtown streetscape elements.

Public art is encouraged in the proposed amendments by providing a floor-area ratio bonus for projects that incorporate public art elements, public indoor wintergarden's public plazas, and public greenspaces. Art elements also are encouraged to prevent blank walls on Downtown buildings.

Proposed site and building design guidelines/standards also allow flexibility for new developments to include active public spaces adjacent to streets and sidewalks, thereby extending the public realm.

Street type and building design standards require that applicants put buildings on 50 percent to 75 percent of the frontage (at the setback) and provide adequate storefront glass on buildings with first-floor commercial uses. Residential buildings in many locations would be allowed to set the building back to provide landscaping, stoops and other elements between the sidewalk and the building. These frontage requirements force parking to be to the side or rear of the buildings.

Walls that would be blank are required to incorporate elements such as an architectural plinth (a stone or masonry base at least 36 inches high), belt course(s) of masonry, a trellis with vines planted that will grow vertically, recesses at least 4 feet wide and 2 feet deep, an overhanging eave less than 12-18 feet from the ground level, decorative tile work, accent lighting, artwork reviewed and approved for artistic merit by an arts review body designated by the City, evergreen hedge, and seating (benches or ledges).

The proposed amendments include measures to protect "view opportunities" of Mount Hood and the buttes near Downtown. These include requirements to conduct view corridor studies, design new developments to protect views of natural features, and orient entrances on open spaces toward view amenities.

Site and building design call for gateway features at prominent entry points to Downtown, including many key sites along boulevards bordering Downtown Gresham. Some gateway requirements for site and building design are to provide gateway plazas and ensure the buildings are oriented toward the corner with high-quality materials on the facades.

Development concepts for key study areas along Eastman Parkway, Third and Beech streets were developed to inform the development of the proposed amendments.

Height limits were increased in most areas of the Downtown Plan District to allow for additional development intensity. Maximum limits were retained to allow additional development and height without artificially raising land values and inhibiting property sales. Allowing additional height or unlimited height may be considered in the future.

Conclusion

The Downtown Area Development – Urban Design goals, policies, and action measures are addressed through the creation of a comprehensive set of land-use regulations, street types to shape the future streetscape, site design provisions, building design provisions, and incentives for special features such as environmental design, artwork, and key gateway elements. These regulations include:

- Street type regulations that begin with creating a high-quality, pedestrian-friendly public realm of streets and sidewalks.
- Site design regulations that bring uses near the sidewalk to encourage pedestrians to interact with the uses, bringing shoppers, diners and workers closer to their destinations and providing a more attractive, human-scaled area with a comfortable "room" on the street framed by buildings. The proposed amendments also recognize the importance of gateways and corner buildings.
- Building design requirements promote active, glass storefronts, and attractive buildings while reducing blank walls that deaden the public space. Incentives and requirements for art elements also will enliven the streetscape.

The proposal is consistent with the applicable general goals, policies and action measures listed in this section.

Section 10.314 – Transportation

Goal: Develop a transportation system that supports the vision of a vibrant Downtown and provides for the safe and efficient movement of pedestrians, automobiles, bicycles, transit and emergency vehicles.

Policy 1: Provide a high-quality transportation system that will:

- a. *Support a variety of modes, including walking, transit, and biking.*
- b. *Capitalize on the presence of light rail.*

- d. *Connect streets and provide more pedestrian linkages within Downtown and to adjacent areas.*

Policy 2: Identify a hierarchy of streets, including streets that are to be designed as major pedestrian/bicycle streets and transit routes. Adopt street design standards specific to each street type.

Policy 4: Provide greater connectivity between Downtown and the Civic Neighborhood by

- a. *Extending streets from the Downtown core to Eastman Parkway to provide more east-west connections.*
- b. *Making the Eastman Parkway/Division Street intersection more pedestrian friendly by such measures as providing wider sidewalks and improving the signalization timing to give pedestrians more time to cross streets.*
- c. *Providing a more direct pedestrian/bicycle link between Civic Neighborhood and the Downtown core area by constructing an enhanced connection through the northwest part of the Gresham Town Fair site.*

Policy 5: Provide a prominent connection between the Springwater Trail/Main City Park and the Downtown core to help link Main City Park both to Downtown and the MAX path.

Policy 6: Minimize the need for new surface parking by:

- a. *Encouraging new development to locate parking underground.*
- b. *Managing on-street parking more efficiently.*
- c. *Encouraging shared parking. Consider negotiating with property owners to share private parking lots with the general public during "off-peak" hours when there is surplus parking available.*
- d. *Developing an overall parking strategy for the Regional Center that will include addressing the long-term need for structured parking.*

Action Measures

1. *Offer incentives in the development code and consider reducing the TIF fee to encourage developers to locate parking underground.*

Findings

The street types defined in the proposed amendments contain non-discretionary standards that ensure optimal building-to-sidewalk relationships and well-designed streetscapes. Street design features and building-to-street relationships will support the development of the Downtown area as a focus of the community and as pedestrian-oriented, transit-supportive sub-districts. The streets types recognize that street design and how buildings interface with the street varies based on function, location, land uses and multi-modal capacity.

The street types include a hierarchy of streets:

- **Urban Boulevard:** Four-travel-lane boulevard meant for regional traffic on edges of Downtown and on Division.
- **Urban Commercial Street:** Downtown street intended to be more commercial in nature that will include on-street parking and encourage buildings to be closer to the sidewalk.
- **Urban Residential Street:** Downtown street intended to be more pedestrian-friendly with slower car traffic and residential buildings possibly set back from the street with layers of landscaping, stoops and other elements between sidewalk and building face.
- **Green/Shared Street:** A street shared by many modes - bicycles, pedestrians, cars – that provides an 18-foot meandering travel lane where auto traffic moves slowly and shares the space with other activity. Pockets of parking and wide sidewalks/landscape areas front the buildings on either side of the street. One of the shared streets, Beech Street, will provide a strong connection between Gresham Central Transit Center, the Center for the Arts, and the Downtown core.
- **Unique Streets (Main Avenue, Stanley Avenue, Third Street, Cleveland Avenue):** For Main, Stanley and Third, streets are geared toward their unique roles and shopping streets with three

different characters. Cleveland is intended to be a green through-street with a more residential character, bike lanes, and buildings set back from the sidewalk edge.

As new streets are constructed or existing streets are redeveloped, environmentally friendly features for the management of stormwater are required.

Transportation elements of the proposed amendments also show future streets through the Town Fair Shopping Center site so that better connections to Civic Neighborhood are established as redevelopment occurs. The proposed amendments also encourage or require mid-block connections and prohibit massive superblocks, which will provide more pedestrian connections throughout Downtown.

Parking is limited in the proposed amendments by requiring a minimum amount of development intensity (floor area ratio) on each site and by limiting the maximum size of any parking lot to 40,000 square feet. A more comprehensive set of parking requirements for Downtown are being developed as part of the City's Centers Parking Strategy project. In addition, incentives are provided for developments that include structure parking, which could include underground parking.

Conclusion

The Downtown Plan District Transportation goals, policies and action measures are addressed through creating a hierarchy of streets; providing a high-quality, pedestrian-friendly public realm of streets and sidewalks; increasing connectivity; and providing adequate but limited parking.

The proposal is consistent with the applicable general goals and policies listed in this section.

Section 10.314 – Parks and People Places

Goal: Create a cohesive and linked public and private system of parks, plazas, courtyards, gardens, and major pedestrian streets/paths, etc. that will help make Downtown a great place to live, work and visit.

Policy 2: Provide for a variety of neighborhood parks and plazas. Each site should have a definitive image or identity. Imaginative design, incorporating different features, should be used to avoid "sameness."

Policy 3: When possible, locate parks/plazas near other public facilities such as the library, government buildings and schools. Parks and plazas should also be visible from nearby residential units to enhance public safety ("eyes on the park").

Policy 6: The MAX pedestrian path should be designed like a linear park and integrated into the adjacent urban fabric of Downtown:

- a. It should have trees and other landscaping; benches, interpretive panels and artwork.*
- b. Adjacent development should be encouraged to expand the public realm of the path by locating landscaping, entryways, plazas, etc. near it.*

Policy 7: Enhance Downtown's streetscapes and encourage pedestrian activity by adding elements, within a unifying design theme, with variation for individual neighborhoods or districts, such as:

- a. Street furniture, like benches and kiosks;*
- b. Street trees, flower baskets and other landscaping;*
- c. Decorative street lamps;*
- d. Bike racks;*
- e. Other visual features, such as fountains, squares, and sculptures; and*
- f. Signage and markers to assist in way finding.*

Findings

The proposed amendments encourage park and plaza creation by providing bonus floor-area ratio to projects that incorporate greenspace and plazas into their sites. Additional City parks and plazas will be studied as part of future implementation efforts.

The proposed amendments incorporate most of the Development Code's Section 7.0201(H) provisions regarding crime prevention, which require streets and common areas to have windows of frequently used rooms looking out on them. Also, on-site publicly accessible open spaces incorporated into developments are required to have active windows and doors facing the spaces.

Bonus floor-area ratio is allowed for projects that incorporate additional landscaping along the MAX pathway.

The proposed amendments include provisions to ensure sidewalk widths and amenity zones for landscaping and street furniture and encourage public art, but specific streetscape designs would be part of a follow-up streetscape design project.

Conclusion

The proposed amendments address the Downtown Area Development – Parks and People Places goals and policies by encouraging or requiring private developments to incorporate publicly accessible open spaces and artwork into their sites. They also encourage additional landscaping along MAX and provide provisions for high-quality streets and sidewalks.

The proposal is consistent with the applicable general goals and policies listed in this section.

Section 10.314 –Economic Development

Goal: Use development tools and incentives to encourage redevelopment of Downtown and the creation of more businesses and housing.

Policy 5: The City will work to:

- a. Attract more employers, especially those with living wage jobs, to locate Downtown.*
- b. Attract more people to live Downtown.*

Findings

The proposed amendments allow the potential for more jobs than the current Downtown Plan District sub-districts. For example, the Town Fair Shopping Center at the southwest corner of Eastman Parkway and Division has been proposed for a change from small-scale, mostly commercial allowed uses to mixed-use zoning that allows significant height and FAR. This area and most of the areas in the Downtown Plan District will be allowed additional development intensity under the proposed amendments, which will provide the potential for more employment.

In addition to relying on existing roads and transit to ensure employment areas are accessible, the text amendments contemplate future streets and improved existing streets to allow better automobile and pedestrian circulation.

The proposed amendments also provide numerous opportunities for small companies and stores to locate into Downtown Gresham, including requirements on some streets that larger buildings have multiple storefronts.

The proposed amendments also allow more residential density in most areas of Downtown and remove requirements for first-floor commercial in large areas of Downtown, which will pave the way for additional all-residential buildings.

The design requirements also are intended to make Downtown a more attractive, vibrant place for both residents and businesses.

Conclusion

The economic development goal and policy 5 are addressed by allowing additional intensity of housing and jobs Downtown in an attractive environment.

C. Metro Urban Growth Management Functional Plan

Title 1 Requirements for Housing and Employment Accommodation

Findings

Title 1 requires Gresham to determine its housing capacity and provide that housing capacity. Title 1 calls for Gresham to have the capacity to add 20,020 units between 1994 and 2017.

Gresham conducted a Housing Capacity Analysis Update that was completed in January 2008. The City delivered the analysis to Metro in April 2008. The HCAU found that Gresham had the capacity to add 17,741 units within the City's 1994 boundary and 3,203 units in the Pleasant Valley area, for a total of 20,944. This exceeded the 20,020 housing capacity target found in Title 1 by 924 units.

The HCAU found that Gresham remained in compliance with Title 1.

In October 2008, Gresham conducted an analysis to estimate the potential loss of housing capacity that could result from the Map and Code amendments related to the Residential Districts Review project. Using a similar methodology as the HCAU¹, staff found that the City's housing capacity would drop by about 600 units. Because the City exceeded Title 1 housing capacity target by 924 units, the City remains above the 20,020 target and in compliance with the requirement.

The Downtown Plan update and associated Map and Code amendments generally provide additional maximum height and require more floor-area ratio than the current Code. Minimum residential densities in the more intense mixed-use Districts – DCC, DMU, DTM and DEM – are at least as high in the most intense districts of the current Code, such as for CUC and DT. The four districts also have no maximum residential density in the proposed text amendments. Minimum residential densities in areas that currently are more residential in nature, DRL-1 and DRL-2, were lowered to allow single-family residential units, which could affect housing capacity. But the density experienced in DRL-2 is likely to be much higher because the sub-district has no maximum density and instead relies upon a floor-area ratio of 4.0 and a height limit of 50 feet to limit intensity.

Title 1 also requires that the City allow at least one accessory dwelling unit for each detached or attached single-family dwelling unit in a Regional Center. The proposed amendments allow accessory dwelling units with single-family detached or attached dwellings.

A City Capacity Analysis Report in 2000 determined that Gresham has adequate job capacity. The permitted land uses and intensities in the proposed amendments provide additional job capacity for Downtown Gresham.

¹ The October 2008 analysis overstates the potential reduction in housing capacity because areas with greater than 15% slope were left out when calculating potential units on properties where zoning is proposed to be changed.

Conclusion

Gresham remains in compliance with Title 1 and overall provides additional capacity in Downtown with these proposed text amendments.

The City's employment capacity will remain in compliance with Title 1 as the proposed amendments allow additional intensity in uses that generate jobs.

The proposal is consistent with Title 1.

TITLE 6: Central City, Regional Centers, Town Centers And Station Communities

Findings

Metro's Title 6 says the "success of the 2040 Growth Concept depends upon the maintenance and enhancement of the Central City, Regional and Town Centers and Station Communities as the principal centers of urban life in the region. Title 6 intends to enhance Centers by encouraging development in these Centers that will improve the critical roles they play in the region and by discouraging development outside Centers that will detract from those roles. As used in this title, the term "Centers" includes the Central City, Regional and Town Centers and Station Communities."

The proposed amendments generally allow greater intensity of housing and employment to promote a lively, active, intensely developed Downtown. These intensities are described in Table 2 (above). Low-density development generally is not allowed except in limited areas where existing low-density or historic neighborhoods exist.

Conclusion

The allowed uses and intensities support making Downtown the main center of urban life in Gresham and East County. The proposed amendments encourage development in the Regional Center. The proposal is consistent with Title 6.

Title 8 Compliance Procedures

Findings

Section 3.07.820 of this title requires that at least 45 days prior to the first evidentiary hearing on an amendment to a comprehensive plan or land use regulation that the City submits the proposed amendments to Metro. Metro may review the amendments and can request that the City provide an analysis of the compliance of the amendment with the Functional Plan.

The City submitted the proposed amendments to Metro on February 25, 2009, which was at least 45 days prior to the first evidentiary hearing of May 11, 2009. Metro did not respond to the notice.

Conclusion

The City has submitted the proposed amendments to Metro at least 45 day prior to the first evidentiary hearing. The City has supplied a copy of this staff report to Metro.

The proposal is consistent with Title 8.

D. STATEWIDE PLANNING GOALS

Findings

Statewide Planning Goal 10: Housing

Statewide Planning Goal 10 requires that cities provide for the housing needs of citizens by planning for the "availability of adequate numbers of needed housing units at price ranges and rent levels which are

commensurate with the financial capabilities of Oregon households and allow for flexibility of housing location, type and density."

The amendments provide opportunities for single-family homes, duplexes, townhomes, apartments and condominiums, as does the Gresham Development Code as a whole. These different types of housing are geographically dispersed in various parts of the City, and the Development Code and the proposed amendments do not prevent housing from being constructed at a variety of price ranges and rent levels.

Oregon Administrative Rule 660-007-0030: New Construction Mix requires that the City "provide the opportunity for at least 50 percent of new residential units to be attached single-family housing or multiple-family housing." The City conducted a Housing Capacity Analysis Update that was completed in January 2008. It found that the City provided an opportunity for approximately 53.5 percent of its new residential units on vacant or partially vacant land to be attached single-family or multi-family housing. Proposed amendments under the Residential Districts Review amendments that took effect in early 2009 reduced Gresham's housing capacity by about 600 and reduced potential attached or multi-family units by approximately 325. This resulted in Gresham having the opportunity for approximately 53.5 percent of its new residential units to be attached or multi-family units, which is still above the 50 percent requirement.² The percentage remains the same, 53.5, even though the number of potential attached or multi-family units falls because the number of total potential units also falls.

The proposed amendments for Downtown will reduce single-family attached dwellings in the limited areas inside DRL-1. It will increase residential densities allowed in most other area of Downtown, however, and greatly expand residential opportunities in DCL and DMU, which formerly were in DC-1 and DC-2 districts that allowed less residential development and more commercial development.

Overall, these amendments are expected to increase the capacity for multi-family and single-family attached development.

Oregon Administrative Rule 660-007-0045: Minimum Residential Density Allocation for New Construction requires that the City "provide for an overall density of ten or more dwelling units per net buildable acre." The City currently has an overall potential density of nearly 20 units per net buildable acre. The proposed amendments would accommodate more residential density and would not reduce that figure.

Oregon Administrative Rule 660-008-0015: Clear and Objective Approval Standards Required requires that "local approval standards, special conditions and procedures regulating the development of needed housing must be clear and objective, and must not have the effect, either of themselves or cumulatively, of discouraging needed housing through unreasonable cost or delay." The City's new design review process for Downtown provides a clear-and-objective route for applicants who wish to develop Downtown by meeting clear and objective standards. It also provides a discretionary route for applicants seeking to meet the more flexible guidelines.

Conclusion

The proposed amendments comply with Statewide Planning Goal 10 and applicable Oregon Administrative Rules.

² These numbers do not include the Pleasant Valley area, which has within its plan an opportunity for 50 percent attached or multi-family housing

SECTION VII

CONCLUSION

The proposed comprehensive plan amendments attached as Exhibit 'A' and 'C' and map amendments attached as Exhibit 'B' are consistent with applicable criteria and policies of the Community Development Plan, the applicable Development Code of the Community Development Plan; Applicable Metro UGMFP code, and Statewide Planning Goals as indicated by findings contained or referenced in Section VI of this report.

SECTION VIII

RECOMMENDATION

Staff recommends **adoption** of the proposed comprehensive plan amendments as contained in the attached Exhibit 'A', 'B', and 'C'.

End of Staff Report



MEMORANDUM

URBAN DESIGN & PLANNING Comprehensive Planning

ADDENDUM STAFF REPORT TYPE IV HEARING—COMPREHENSIVE PLAN AMENDMENT DOWNTOWN PLAN/DESIGN STANDARDS PROJECT

To: Gresham Planning Commission

From: Mike Abbaté, Urban Design & Planning Director
Jonathan Harker, AICP, Comprehensive Planning Manager
Lauren McGuire, Senior Planner
Brian Martin, AICP, Associate Planner

Hearing Date: May 11, 2009

Report Date: May 8, 2009

File: CPA 09-017

Exhibits: 'E' – Downtown Plan District Design Manual – All Downtown Proposed Modifications
'F' – Downtown Plan District Design Manual -- District-Specific Proposed Modifications

This addendum suggests modifications to the proposed amendments in CPA 09-017 to provide clarifications where the guidelines and standards are unclear or silent. These items came to staff's attention after the April 30 staff report was issued or were further analyzed by staff after that date.

Suggested additions to the proposed amendments are shown double underlined.
Suggested deletions to the proposed amendments are shown with ~~strikeout~~.

All edits in this document show the text as it appears in the proposed amendments, with additional recommended modifications shown in double-underline or strikeout.

Issue A. Review of single-family detached homes

Issue:

Although the level of review for different project types is described in Table 3.0010, readers of the proposed amendments could assume that single-family detached homes are to be reviewed with the same process using the same guidelines and standards as condominiums and apartments. This level of review, which would be much higher than elsewhere in the City and was not intended, is not necessary to achieve the Downtown goals. The specific standards for review also are not identified.

Commentary:

This would apply to land in DRL-1 and DRL-2. Single-family areas already existing in these neighborhoods have a character similar to the City's TR district. Adequate single-family detached dwelling review can be accomplished by applying the TR standards for lot size, lot dimension and minimum street frontage; Development Code standards regarding yard setbacks for special side yard conditions (4.0132(A)), allowing only one unit per lot (4.0132(C)), Safe Neighborhood Design Standards (4.0132(D)); height and setback requirements from the proposed amendments (4.1130); and parking standards (9.0800).

Setbacks are provided in 4.1130, so 4.0132, (A)(1) and (B), which both relate to setbacks, do not apply.

Recommendation:

The following language is proposed as a footnote to Table 4.1130 to clarify the review process/standards for single-family detached homes and two-unit attached dwellings and make clear that architectural design standards in 7.0201(J) do not apply Downtown.

4.1130 NOTES ON TABLE:

O. Single-family detached dwellings shall comply with the following:

1. A minimum lot size of 4,000 square feet.
2. 4.0130 standards for minimum lot dimensions and minimum street frontage for single-family detached dwellings in TR; and
3. 4.0132 Additional Standards, except (A)(1) and (B); and
4. 4.0136 Manufactured Dwelling Requirements; and
5. Standards for maximum height and setbacks in 4.1130; and
6. Standards for parking in 9.0800.

Issue B. Review of two-unit attached dwellings (duplexes)

Issue:

The review process for two-unit attached dwellings is unclear. Although the level of review for different project types is described in Table 3.0010, readers of the proposed amendments could assume that two-unit attached homes are to be reviewed with the same process using the same guidelines and standards as condominiums and apartments. Some design review for duplexes is necessary because of the particular design challenges brought by the two-unit format, such as two entrances, two potential vehicular access points, and the need for compatibility with adjacent single-family detached dwellings. The proposed amendments lack specific details about how this housing type will be reviewed.

Commentary:

This would apply to land in DRL-1 and DRL-2. All duplexes would be subject to design review. As it states in the Design District, Design Review definition in the proposed amendments, one or two duplexes on a site would be reviewed by staff and three or more duplexes would be reviewed by Design Commission. The Development Code provisions that apply would be those regarding minimum lot dimensions and street frontage for TR (4.0130); Safe Neighborhood Design Performance Standards (4.0132(D)); height and setback requirements from the proposed amendments (4.1130); parking standards (9.0800); and applicable Design Review Standards and Criteria (7.0201).

For those applicants that elect to take the discretionary track, the duplexes would be reviewed using the standards listed under 4.1130 Table Note (P)(6), which involve Integrated Site Planning, Façade Composition, Building Orientation, and Landscaping.

Recommendation:

The recommended modifications list which standards in the Development Code apply to duplexes and, if the applicant chooses the discretionary process, which guidelines apply.

The following language is proposed as a footnote to Table 4.1130 to clarify the review process/standards for two-unit attached dwellings.

P. Two-unit attached dwellings shall comply with the following:

1. A minimum lot size of 7,000 square feet.

2. 4.0130 standards for TR minimum lot dimensions for duplexes and minimum street frontage; and
3. 4.0132(D) Safe Neighborhood Design Performance Standards; and
4. Standards for maximum height and setbacks in 4.1130; and
5. Standards for parking in 9.0800.
6. For developments with more than one two-unit attached dwelling on a site, the applicable design review standards in 7.0201.
7. Two-unit attached dwellings may be reviewed under the discretionary review process described in 4.1101(A)(1)(a) using the following guidelines:
 - a. 4.1151(A)(1); and
 - b. 4.1151(B)(2)(c)(1) and (6) through (9); and
 - c. 4.1155(A)(2)(c)(1) and (2); and
 - d. 4.1155(A)(4)(c)(1) and (2).

Issue C. Maximum Residential Density in DRL-1

Issue:

The proposed maximum density for DRL-1, which is 20 units per acre, does not reflect the intended character, which is a neighborhood of single-family dwellings and duplexes with some opportunities for small-scale commercial.

Commentary:

The proposed maximum density in Table 4.1130 could be changed to 12.45, which corresponds to a duplex on a 7,000-square-foot lot.

Recommendation:

Alter the proposed amendments to say that the maximum density in DRL-1 is 12.45.

Issue D. Architectural Design Standards

Issue:

The proposed amendments are not clear about whether the architectural design standards in 7.0201(J) apply Downtown.

Commentary:

The Downtown Plan Design Manual provides for architectural design standards, except for duplexes. If the Architectural Design standards from the Design Review section also applied, conflicts could arise during review.

Recommendation:

Staff recommends that language be added exempting the Downtown from the Design Review Architectural Design provisions as shown in the following modification to the proposed amendments are recommended.

7.0201 Dwelling Structures Containing Two or More Units, Single Family Attached Dwelling Units and for Elderly Housing

J. Architectural Design

Architectural Design Standards apply to all uses subject to Design Review except:

1. Single-family attached dwellings, which are subject to the standards of subsection (L) or the applicable standards in the Downtown Plan District.
2. Downtown Plan District uses, except duplexes, which must comply with applicable standards.

~~Architectural Design Standards for all uses except single family attached dwellings in all districts (which are subject to the standards of subsection (L)) and except those uses other than single family attached dwellings in for the Downtown Plan District:~~

Issue E. Landscaping standards clarification

Issue:

Green roofs are encouraged in the Downtown Plan District, but the proposed amendments do not specifically allow eco-roofs to be counted toward an applicant's landscape requirements.

Commentary:

Adding eco-roofs as counting toward site landscaping requirements will allow urban developments, especially on smaller sites, to satisfy the landscaping requirements. The eco-roofs provide a public benefit because of the reduced stormwater runoff from the building and a reduction in the heat island effect often generated by roofs exposed to the sun. Some eco-roofs also are accessible to building occupants and provide a rooftop open space opportunity with living plant material integrated.

Recommendation:

Modify the proposed amendments to add eco-roofs to 4.1151(A)(5)(f)(3).

f. Standards: Site Landscaping

3. Landscaping as defined in 3.0010, all ~~all~~ landscaped setback areas, landscaped common open space, eco-roofs, preserved natural areas, planter areas, landscaped screening areas, as well as landscaped strips located between the curb and sidewalk, can be included to meet the 15% landscaping requirement. In addition, shared walkways within landscaped areas may count for up to 66% of the total required landscape area. ~~10% of the total landscaping requirement.~~

Issue F. Community service uses and Street Type building frontage requirements

Issue:

As written, the Street Type standards apply to all community service uses. For example, a building constructed in a park might have to be constructed on the frontage (rather than in a central location in the park) to meet the minimum frontage requirements.

Commentary: Exempting some community service uses from the requirements for the amount of frontage that must be occupied by a building would allow greater flexibility in design of uses such as parks or public spaces.

Recommendation:

The following modifications to the proposed amendments are recommended to exempt parks, plazas, trails, cemeteries and similar uses.

4.1142 Street Type General Provisions

F. The following community service uses are exempt from requirements for the minimum amount of building frontage required on a street type:

1. Public urban plazas and public walking/hiking trail with associated trail access points and trailheads.
2. Public neighborhood parks.
3. Cemeteries.
4. Public community parks.

5. Public multi-use paths with associated trail access points and trailheads.

Issue G. Street Type building frontage requirements flexibility

Issue:

The street type general provisions only provide a clear-and-objective track method to vary from the building frontage requirements. This does not allow the Design Commission the ability to reduce the requirement for odd-shaped lots or situations with site-design challenges, such as whole-block developments on four street types or developments on multiple street types with high frontage percentage requirements.

Commentary:

Clarifying the clear-and-objective track language and providing a discretionary route to modify the frontage requirements provides more flexibility in the process and allows the Design Commission to allow a high-quality development that may not be able to meet a frontage requirement.

Recommendation:

The following recommended modifications to the proposed amendments clarifies the standards and allows an applicant to use the discretionary track to achieve greater flexibility in site design.

4.1142 Street Type General Provisions

B. If the development site has frontage on more than one street type, the minimum percentage of street frontage that must be occupied by a building façade may be reduced in one of the following ways:

1. Except for the Main, Third, Stanley and Boulevard street type frontages, the minimum percentage of street frontage that must be occupied by a building façade may be reduced on one street type frontage by the Design Commission or Manager up to 25% of the site's total frontage as part of the clear-and-objective track. For example, a requirement that 75% of the frontage must be occupied by a building façade could be reduced to 50% of the frontage on that one street type.
2. The minimum percentage of street frontage that must be occupied by a building façade may be reduced through the discretionary review process. The applicant must demonstrate that the application complies with applicable site-design Design Guidelines in 4.1151 and 4.1152.

B. Except on the Main, Third, Stanley and Boulevard street types, the minimum percentage of street frontage that must be occupied by a building façade may be reduced on one street type frontage by the Design Commission or Manager up to 25% of the total frontage as part of the discretionary review process if the development site has frontage on more than one street type.

Issue H. Street Type transparency requirements flexibility

Issue:

Some desirable uses may not be able to meet the transparency requirements listed in the street types. The proposed amendments do not allow applicants to choose the discretionary track to meet transparency guidelines rather than clear and objective standards.

Commentary:

The proposed amendments contain guidelines regarding transparency by which projects can be judged. Providing a discretionary route to approval would allow the Design Commission to approve a high-quality project that could not meet the clear-and-objective transparency requirements.

Recommendation:

The following modifications to the proposed amendments are recommended to allow transparency requirements to be modified to add flexibility in building design.

4.1142 Street Type General Provisions

G. The Design Commission may reduce the transparency requirements in the Street Type standards if the Design Commission determines transparency guidelines in 4.1151(B) have been met.

Issue I. Images are not standards

Issue:

Images in the Downtown Plan District Design Manual are not standards but are intended to illustrate standards and guidelines found in the text. Some readers of the document, though, could interpret the images to be standards.

Commentary:

Including an explanation of the roles of the images and labeling the images should avoid confusion.

Recommendation:

The following modifications are recommended to clarify the role of images in the proposed amendments.

4.1101(B)(6) Images

Most images, including photographs, illustrations, and maps, included in the Downtown Plan District Design Manual (4.1100) are not part of the Development Code and do not act as guidelines or standards. Images that are not part of the Development Code are labeled as figures. Images that are part of the Development Code will be labeled with a Development Code section number.

Issue J. Residential vs. non-residential guidelines and standards

Issue:

With some guidelines and standards in the proposed amendments (4.1151 through 4.1155), it is not clear to which uses they apply. The same guidelines and standards may not fit the particular characteristics of several different uses – mixed use, commercial, single-family attached, condominiums, apartments – so some clarification is needed.

Commentary:

The attached modified pages of the proposed Downtown Plan Development Code to show additions and deletions that describe to which projects the guidelines and standards apply, add headings to the document, and provide other minor edits. These changes clarify which guidelines and which standards apply to different uses.

Recommendation:

Staff recommends that the attached modified sections be approved in place of those in the proposed amendments to clarify which standards and guidelines apply to different uses.

Issue K. Minor corrections to Public Works Standards Coordination text

Issue:

Section 4.1141 in the proposed amendments contains some garbled language in the first paragraph and, in the second paragraph, uses a term inconsistent with the rest of the amendments.

Commentary:

Deleting three words fixes the garbled language and changing the word "portico" to "colonnade" allows the language to match wording found elsewhere in the proposed amendments. The Department of Environmental Services is planning to update the street engineering construction standards and cross-sections found in the Public Works Standards (PWS) to remain consistent with the cross-sections presented in the Street Types in the proposed Appendix 2.000. The PWS update will be done through an administrative process. The anticipated completion of the Public Works Standards documents is July 2009.

Recommendation:

Staff recommends approval of the minor corrections to the proposed amendments for Section 4.1141.

4.1141 Public Works Standards Coordination

Applicants shall consult specific street engineering construction standards in the Public Works Standards and the Green Streets Standards in combination with these Street Type Standards and the street standards included elsewhere in the Gresham Community Development Code. If compliance with the ~~all of the~~ standards results in a conflict between the Community Development Code and Public Works Standards, the Public Works Standards govern the street design. The Department of Environmental Services may grant exceptions to the Public Works Standards through the Design Modification Process if the City receives benefit from the suggested standard deviation.

Where existing public right-of-way is too narrow to accommodate the required Street Type improvements, the applicants shall dedicate the required right-of-way. On Third Street, an applicant may chose to dedicate the required right-of-way or provide a sidewalk easement for the portion of the sidewalk under the portico colonnade on the north side of the street.

Issue L. Clarification of Design Review for Innovative Housing Demonstration Projects

Issue:

Section 7.0303 requires applicants under the Innovative Housing Demonstration Projects code to be reviewed under two separate sets of design guidelines and standards for one site.

Commentary:

Requiring applicants to meet only one set of standards and guidelines would provide more clarity, certainty and simplicity for applicants and the review body. To allow the innovation and flexibility inherent to the innovative housing code, projects would need to be reviewed under that code's guidelines and standards. However, reviewers of Downtown projects could keep in mind the Design Principles when considering whether the projects are compatible with the rest of the Downtown Plan District.

Recommendation:

To avoid a project having to comply with two different sets of Design Guidelines and Standards but to inject the Downtown Design Principles into the review process, the following modifications to the proposed amendments are recommended:

7.0303 Review of Projects

- A. Innovative housing demonstration projects will be reviewed by the Planning Commission using a Type III procedure as detailed in Section 11.0500, except for those in a Design District which will be reviewed by the Design Commission. This process is a discretionary review, using the Development Principles and Guidelines in Section 7.0310 to guide decision-making. Projects within a Design District will also be reviewed using the guidelines Design Principles of the Design District to assist in judging compatibility with the applicable district or Plan District. Once deemed complete and prior to the Planning Commission hearing on a proposal outside of a Design District, the Design Commission will issue a recommendation on the design aspects of the proposal in accordance with Section 11.0402(D)(1)(e) for review and comment on applications. For those applications in a Design District, the Design Commission will issue a decision on all aspects of the proposal. Appeals of the decision will be heard by the City Council.

Issue M. Parking Proposed Amendments incorporating minor changes from DCIP Round 2, enacted May 7, 2009

Issue:

A few minor text edits from the DCIP project were not incorporated into the proposed amendments.

Commentary:

The proposed modifications below incorporate those changes, which were enacted May 7, and correct the numbering.

Recommendation:

Staff recommends the following modifications to the proposed amendments.

9.0852 Plan District Minimums and Maximums

B. Downtown Plan District

Off-street parking facilities provided in connection with new development must be appropriate for the unique characteristics of the Downtown PD. This means that parking should be a clearly secondary use, designed so that it does not interfere with easy and safe pedestrian movement. By ensuring that parking lot sizes do not become excessive, and that appropriate landscaping is provided, the Downtown area will be more attractive, and more conducive to creating a lively, prosperous district for the benefit of area residents, employees, and visitors.

1. ~~Standards for Minimum Auto Off-Street Parking shall be as follows:~~
 - a. Minimum Off-Street Parking for commercial per 1,000 square feet of floor area: Off-street parking is not required for new commercial development in the Sub-Districts. In the DMU, DEM, DRL-1, DRL-2 and DCL Sub-Districts, minimum required parking for new commercial uses is 80% of the minimum number of spaces required in **Table 9.0851**. If required by the Building Code, accessible parking spaces shall be provided regardless of the provisions of this subsection. The minimum amount of off-street parking required in any sub-district of the Downtown PD may be reduced by up to 50%, as provided in **Section 9.0853(E)**.
 - b. Minimum Off-Street Parking for residential: 1 space per unit for residential.
2. ~~Standards for Maximum Auto Off-Street Parking shall be as follows:~~
 - a. Maximum Off Street Parking for commercial per 1,000 square feet of floor area: Maximum off-street parking permitted for commercial uses that exceed 90,000 square feet of floor space shall be 80% of the amount required in **Table 9.0851**. Additional off-street parking beyond these maximums may be permitted within the Downtown Plan District, as provided in **9.0852(B)3**.
 - b. Maximum Off-Street Parking for residential: no maximum for residential, except 1.5 spaces per unit for attached dwelling units on a single lot, which does not include single-family attached dwellings.
23. Parking in addition to maximums: Under Type II procedures, the Manager may approve off-street parking in excess of the maximum allowed parking spaces specified in this section when the applicant can show that all the following criteria are met:

34. New surface parking lots in the Downtown Plan District are limited to 40,000 square feet in size per site.
45. Commercial Parking Facilities on surface parking lots are prohibited in the Downtown Plan District. Public parking facilities owned by a local, county, or regional governments, an urban renewal agency or a transportation management association are allowed on surface lots or in structures.
56. New surface parking lots can only be constructed in the Downtown Plan District as approved with a development permit for an allowed use that requires off-street parking, except for public parking facilities.
67. Parking facilities are not allowed as the first phase of a development but must be constructed in conjunction with buildings that are part of the development.
78. Bicycling parking shall be provided in conformance with Section 9.0830 and Table 9.0851.

Issue N. Adding amendments to Community Development Plan Map

Issue:

A specific reference to changing the Community Development Plan Map was omitted from the proposed amendments.

Commentary:

Downtown Plan/Design Standards Code Amendment
May 7, 2009

Staff Report Addendum
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The following section would clarify that the Development Plan Map will change.

Recommendation:

Staff recommends adding the following language to the proposed amendments:

Section 10, Volume 2, Policies, Appendix C, is amended as follows:
Volume 2, Appendix C, of the Community Development Plan Map, is amended to reflect changes as shown in Exhibit 'B' attached hereto.

Issue O. FAR Exemptions

Issue:

In DCC, DRM, and DTM, the minimum required FAR is 1.0. If an applicant wishes to construct a 1-story building, they would be required to fill the entire site with a building. This may not provide the possibility for ground-level community amenities such as plazas, walkways, and mid-block pedestrian cut-throughs.

Commentary:

Including a provision that allows applicants to count some ground-level features toward the floor area for purposes of the floor-area-ratio calculation would allow some ground-level amenities while still encouraging more intense development near the Downtown core and transit facilities.

Recommendation:

Staff recommends adding the following language to provide limited areas for ground-level amenities adjacent to buildings in areas with a minimum FAR of 1.0.:

4.1130 Downtown Plan Sub-District Standards

- C. For purposes of floor-area-ratio calculations in DCC, DMU and DTM, applicants may include the following in the ground-level floor area up to a total of 1,000 square feet or 10% of the site, whichever is larger:
1. Publicly accessible open space.
 2. Walkways that include landscaping and provide access to publicly accessible open spaces or provide a through-block connection.
 3. Required minimum setbacks.
 4. Area removed from the first-floor building footprint to create corner features to comply with 4.1151(B)(8). For example, a beveled corner will have a slightly smaller footprint than a 90-degree corner because of the bevel.
 5. Public space required for gateway locations.



MEMORANDUM

**URBAN DESIGN & PLANNING
Comprehensive Planning**

**ATTACHMENT F - ADDENDUM STAFF REPORT
TYPE IV HEARING—COMPREHENSIVE PLAN AMENDMENT
DOWNTOWN PLAN/DESIGN STANDARDS PROJECT**

To: Mayor Bemis and Gresham City Councilors

From: Mike Abbaté, Urban Design & Planning Director
Jonathan Harker, AICP, Comprehensive Planning Manager
Lauren McGuire, Senior Planner
Brian Martin, AICP, Associate Planner

Hearing Date: June 2, 2009

Report Date: May 14, 2009

File: CPA 09-017

For the May 11, 2009, Planning Commission hearing, staff recommended that the design standards and design guidelines in the Downtown Plan District Design Manual be labeled as to whether they applied to all development or to commercial or residential development only. These labels were not included in the last draft of the manual (dated April 30), even though the intent was to specify which guidelines and standards apply to which uses. Staff made the labeling changes, and they were included as Exhibits E & F of the Planning Commission staff report. The Planning Commission recommendation included those changes.

However, it was discovered after the hearing that Planning Commission Exhibit F was an incomplete version and did not have all of the staff-recommended labels. In keeping with the Planning Commission's recommendation to complete the labeling, staff has incorporated the missing labels into the Council Bill in the City Council hearing packet. Council approval of the Council Bill will mean approval of all of staff's recommended labeling.

The effected standards and guidelines are found in the Downtown Plan District Design Manual, which is Exhibit A of Council Bill 07-09. The recommended amended sections are:

4.1152 Design Guidelines and Standards – Downtown Commercial Core

Ground-Level Details

- Section 4.1152(B)(3)(c)(1) is labeled as All Development
- Section 4.1152(B)(3)(c)(2) is labeled as Non-Residential
- Section 4.1152(B)(3)(d)(1) is labeled as Non-Residential

4.1154 Design Guidelines and Standards – Downtown Transit Mid-rise

Ground-Level Details

- Section 4.1154(B)(3)(c)(1) is labeled as All Development
- Section 4.1154(B)(3)(d)(1) is labeled as All Development
- Section 4.1154(B)(3)(d)(2) is labeled as Residential

4.1155 Design Guidelines and Standards – Downtown Residential Low-rise

Integrated Site Planning

- Section 4.1155(A)(1)(c)(1) and (2) are labeled as Residential
- Section 4.1155(A)(1)(d)(1) and (2) are labeled as Residential

Building Orientation

- Section 4.1155(A)(2)(c)(1) and (2) are labeled as Residential
- Section 4.1155(A)(2)(d)(1) and (2) are labeled as Residential

Publicly Accessible Open Space

- Section 4.1155(A)(3)(c)(1), (2), and (3) are labeled as Residential
- Section 4.1155(A)(3)(d)(1) and (2) are labeled as Residential

Landscaping

- Section 4.1155(A)(4)(c)(1) and (2) are labeled as All Development
- Section 4.1155(A)(4)(d)(1), (2) and (3) are labeled as All Development

Pedestrian Circulation

- Section 4.1155(A)(5)(c)(1) is labeled as Residential
- Section 4.1155(A)(5)(d)(1) is labeled as Residential

Façade Composition

- Section 4.1155(B)(2)(c)(1) is labeled as Residential
- Section 4.1155(B)(2)(d)(1) is labeled as Residential

Proposed new language is double-underlined;
Proposed deleted language is ~~stricken~~.

CB 07-09

ORDINANCE NO. 1675

AMENDMENT TO VOLUME 1, FINDINGS; VOLUME 2, POLICIES; VOLUME 3,
CODE; OF THE GRESHAM COMMUNITY DEVELOPMENT PLAN, REGARDING
THE DOWNTOWN PLAN AND DESIGN MANUAL

THE CITY OF GRESHAM DOES ORDAIN AS FOLLOWS:

Section 1. Gresham Community Development Plan, Volume 3, Code, Article III,
Definitions, is amended as follows:

3.0002. List of Terms

Terms used in the Development Code are presented below. General terms that apply throughout the Code are listed in **Section 3.0010**. Terms that are specific to a Development Code section are listed after the General Terms. These categories are:

- A. Habitat Conservation Area Terms and Definitions. **Section 3.0020**
- B. HCA, ESRA, and Article V Terms and Definitions. **Section 3.0030**
- C. Solar Related Terms and Definitions. **Section 3.0040**
- D. Tree Related Terms and Definitions. **Section 3.0050**

If a term is defined in both the General Definitions and in a Section-specific category, the Section-specific definition shall be used if the application of the term is within the parameters of the Code Section.

General Terms

Awning

Bay (building façade)

Building Massing

Building Modulation

- Horizontal Modulation
- Vertical Modulation

Bulkhead

Butterfly Roof - See Roof

<u>Canopy</u>	*****
<u>Cornice</u>	*****
<u>Eco-Roof</u>	*****
<u>Fore-Court</u>	*****
<u>Gable Roof – See Roof</u>	*****
<u>Green Street</u>	*****
<u>Hipped Roof – See Roof</u>	*****
<u>Light cut-off</u>	*****
<u>Lintel</u>	*****
<u>Live-work</u>	*****
<u>Modulation – See Building Modulation</u>	*****
<u>Mural</u>	*****
<u>Outdoor storage</u>	*****
<u>Pervious (Porous) Pavement</u>	*****
<u>Plinth</u>	*****
<u>Rain Garden</u>	*****
<u>Roof</u>	
• <u>Butterfly Roof</u>	
• <u>Gable Roof</u>	
• <u>Hip Roof</u>	
• <u>Shed Roof</u>	

<u>Scoring</u>	*****
<u>Shed Roof – See Roof</u>	*****
<u>Stormwater Planter</u>	*****
<u>Sun Screen/sun shade</u>	*****
<u>Transom Window</u>	

Visible Transmittance

Xeriscaping

3.0010 General Terms and Definitions

Awning. A shelter that provides weather protection, usually constructed of non-rigid canvas or canvas-like materials on a supporting framework that projects from the exterior wall of a building.

Bay (of building façade). Any division of a building between vertical lines or planes, especially the entire space between two adjacent supports.

Bulkhead. The solid portion of wall below the glass of a storefront window whose purpose is to protect against abrasion or impact loads. The bulkhead is also commonly referred to as a knee-wall.

Butterfly Roof. See Roof.

Canopy. An architectural projection that provides weather protection, identity or decoration and is supported by the building to which it is attached. A canopy is comprised of a rigid structure over which a rigid covering is attached. See also sun screen/sun shade.

Cornice. The uppermost section of projecting ornamental moldings along the top of a building just below a roof or the top of a wall.

Design District. Provides Guidelines and Standards for development activity in clearly defined special design areas. It can be used to ensure the conservation, continuity, enhancement, and continued vitality of the identified scenic, architectural, and cultural values of each design district and to promote quality development in centers, near transit facilities, and similar areas. Six initial design districts are generally described as:

- **Downtown Design District** is generally all of the Downtown Plan District (DPD) lands as described in Section 4.1100. This area generally encompasses properties between Eastman on the west, Hogan on the east, both sides of Burnside to the north and both sides of Powell to the south. Station Center (SC) lands that are on the east side of Eastman north of Division and south of Burnside, and north side of Division north of the DPD; Community Commercial (CC) lands that are north of Division, north and south of Burnside and east of Eastman; Corridor Mixed Use (CMU) on both the north and south side of Burnside, east of Eastman and Transit Low Density Residential (TLDR) north of Division and south of Burnside, east of Eastman.

Design District, Design Review. Within a design district two options are available for design review. One is to meet the requirements of the Design Standards. The second is to meet the requirements of the Design Guidelines. Larger developments that propose to meet the Design Standards or any development that proposes to meet the Design Guidelines will be reviewed by the Design Commission at a public hearing. Smaller developments that propose to meet the Design Standards will receive an administrative review by the Manager. Larger developments include:

- Residential development with: five (5) or more units if the proposal is adjacent to ~~or within~~ the LDR-5, LDR-7, TR, ~~or~~ TLDR, DRL-1, and DRL-2, districts or within TLDR, DRL-1 and DRL-2; or ten (10) or more units if adjacent to other land use districts;
- Mixed-use, live-work, community service use, ~~and~~ commercial and employment developments with either 10,000 square feet of new floor area (20,000 square feet in the Corridor Design District) or 50 feet of building frontage on a street including new buildings or additions to existing buildings; and
- All public building community services uses.

Design Guidelines. A set of design parameters for development in design districts that are based on the established Design Principles. The Design Guidelines are discretionary in nature, ~~provide a statement of intent~~, and are used to evaluate the acceptability of a project's design. Design Guidelines provide the opportunity for creative design flexibility.

Design Principles. General statements that will guide the design of the built environment in design districts. They are the connection between general planning goals and policies and implementing Design Guidelines and Standards. ~~Design Principles are normally described by several sentences and usually amplified with graphics such as diagrams, sketches, illustrations, and photographs.~~

Design Standards. A set of objective requirements for development in design districts that are based on design principles. Design standards provide a clear and objective way of evaluating the acceptability of a project's design.

Eco-Roof. Also known as Green Roof. A roof that has been constructed with an impermeable barrier, overlain with a layer of planting media (such as soil or other) and vegetation, with the purpose of slowing and filtering stormwater, insulating the building, and reducing the urban heat-island effect.

Fore-Court. An open area in front of a building's main entrance.

Floor Area. The gross area, under roof, of all of the habitable floors of a building, measured from the interior of exterior walls, excluding only space devoted to off-street parking or loading and excluding basement area.

Green Street. A street that incorporates Green Development Practices within or adjacent to the right-of-way to treat, retain, and infiltrate stormwater runoff. Green Street section and design standards are included in the City of Gresham's Public Works Standards for each street classification. Green Streets typically use rain gardens, stormwater planters, or porous pavement to manage stormwater runoff.

Ground Floor Height. The vertical distance from the upper surface of the ground floor to the upper surface of the floor immediately above. (See also Story).

Light Cut-off. An outdoor lighting fixture designed to direct light (usually downward) and prevent light from being emitted outside the area intended for lighting.

Lintel. A horizontal structural beam above an opening, such as a window or door that may be expressed externally as an architectural feature.

Live-work. A live-work unit is a structure that combines a limited office, retail service, and/or business service use with a residential living space. The commercial space may be used by anyone residing at the unit and by no more than two non-resident employees.

Massing, Building. The mass or bulkiness of a building is its three-dimensional form, and its relationship to exterior spaces.

Modulation, Building. Design technique that breaks the massing of large buildings down into smaller units by providing varying depths for exterior walls.

- **Vertical modulation.** Used to make large buildings appear to be an aggregate of smaller elements or to add visual relief to long stretches of facades. Techniques can include the use of architectural features, setbacks or varying rooflines.
- **Horizontal Modulation.** Used to reduce the apparent mass of multi-story buildings and provide continuity at the ground level of a large building. Building facades can be divided with horizontal elements so that the façade appears less massive than those with sheer, flat surfaces. Techniques can include step-backs, balconies, and roof treatment.

Mural. A painting applied to a wall surface that will be reviewed and approved for artistic merit by an arts reviewing body designated by the City.

Outdoor Sales Display. An outdoor arrangement of objects, items, products, or other materials that is capable of rearrangement, is not in a fixed position and which is designed and used for the purpose of sales. An outdoor sales display does not exceed 15% of the ground floor area of the building(s) on the site or a maximum of 1,000 square feet, whichever is less. An outdoor sales display also does not remain outdoors for more than 24 hours and maintains an accessible pathway as defined in the Building Code.

Outdoor Storage. The keeping of materials or goods associated with the rental, distribution, wholesale, manufacturing, processing or repair of equipment or supplies in the same outdoor place for more than 24 hours.

Plinth. The base or platform upon which a wall, column, pedestal, statue, monument, or structure rests.

Pervious Paving. Also known as porous pavement or permeable paving. A paving method for paved areas such as roads, parking lots and walkways that allows the movement of water and air through the paving material. Pervious materials allow precipitation to percolate through areas that would traditionally be impervious and instead infiltrates the stormwater through to the soil below. Examples include: porous asphalt, concrete, paving stones, bricks, or paver systems that lock together but include small gaps to allow water to pass between the pavers.

Portico. A covered walkway or major porch with columns on at least one side; a covered colonnade.

Rain Garden. A shallow landscaped basin that accepts stormwater runoff from rooftops, parking lots, streets, or other impervious surfaces, with the purpose of storing and filtering or infiltrating the water into the ground.

Roof. A structural covering over any portion of a building or structure including projections beyond the walls or supports of the building or structure.

- **Roof, Butterfly.** A roof where planes are designed so the middle of the roof is lower and the outer edges are higher..
- **Roof, Gable.** A roof identified by the straight slope falling from ridge to eave, creating a peak or triangle on the side or front façade. Gabled structures have rakes on the gable facades and eaves on the non-gabled facades.
- **Roof, Hipped.** A roof that that slopes on all sides up to a peak or ridge.
- **Roof, Shed:** A roof having only one sloping plane..

Scoring. A technique used to divide a sidewalk area by patterning grooves in the concrete for aesthetics or to control cracking.

Stormwater Planter. A rain garden with structural side walls constructed of concrete, steel, or other durable building material.

Sun Screen/Sun Shade. Attached projecting, architectural feature designed to provide shading from the sun. A sun screen/sun shade is a rigid structure and can add a decorative element to building design, but provides a functional energy conservation benefit to the building by deflecting solar heating away from building windows or walls. See also canopy.

Transom Window. A window above a door that is usually hinged to a horizontal crosspiece over the door.

Visible Transmittance. The amount of light that passes through a glazing material.

Xeriscaping: A landscaping method that utilizes water-conserving techniques, such as the use of drought-tolerant plants and efficient irrigation.

Section 2. Gresham Community Development Plan, Volume 3, Code, Article IV, Land Use Districts, is amended by deleting Section 4.1100 in its entirety and replacing it with new section 4.1100, Downtown Plan District, as set forth in the “Downtown Plan District Design Manual,” which is adopted as Gresham Community Development Plan, Volume 3, Code, Appendix 2.000 by Section 12 of this ordinance, and as follows:

**Section 4.1100
Downtown Plan District**

Section 4.1100 Downtown Plan District is incorporated by reference into Appendix 2.000 of this Code.

Section 3. Gresham Community Development Plan, Volume 3, Code, Section 4.1400, Pleasant Valley Plan District, is amended as follows:

Table 4.1407 Residential Permitted Uses			
Use Categories:	LDR-PV	MDR-PV	HDR-PV
(A) Single-Family Detached Dwelling	P	P	NP
(B) Manufactured Homes on Individual Lots	P	P	N
(C) Attached Dwellings on a Single Lot	NP	P	P
(D) Single Family Attached Dwellings (2 units)	L ¹	P	P
(E) Single Family Attached Dwellings (3 or more units)	NP	P	P
(F) Two-unit Attached Dwellings	L ²	P	P
(G) Accessory Dwellings	P	P	NP
(H) Community Services	L ³	L ⁴	L ⁴
(I) Accessory Structures	P	P	P
(J) Home Occupations	P	P	P
(K) Temporary Uses	P	P	P

(L) Residential Facility	P	P	P
(M) Residential Home	P	P	NP
(N) Live-Work ⁵	NP	P	P

Key:

P = Permitted

L = Limited

NP = Not Permitted

Table 4.1407 Notes:

⁵ For purposes of Table 4.1407, a live-work unit is a structure that combines a limited office, retail services, and/or business services use with a residential living space. The commercial space may be used by anyone residing at the unit and by no more than two non-resident employees. The commercial portion of the structure shall face the street front, is limited to the first floor, and garage access must be from the alley. A fascia, awning, or painted wall sign limited to 32 square feet is permitted per each unit.

Table 4.1420 (A) Commercial and Mixed Use Permitted Uses			
Use Categories:	TC-PV	NC-PV	MUE-PV
Commercial Uses			
(A) Offices	P	L ¹	L ²
(B) Clinic	P	L ¹	L ²
(C) Retail Trade	P	L ¹	L ^{2,3}
(D) Retail Service	P	L ¹	L ^{2,3}
(E) Business Services	P	L ¹	L ²
(F) Auto-Dependent Use	NP	L ¹	L ²
(G) Outdoor Commercial	L ³	P	L ⁴
(H) Mini-Storage Facilities	NP	NP	P
Residential Uses:			
(A) Attached Dwellings on a Single Lot	L ⁵	L ⁵	L ⁷
(B) Single Family Attached Dwellings	L ⁵	L ⁵	L ⁷
(C) Mixed Use Development	P ^{5,6}	P ^{5,6}	P ^{6,7}
(D) Live-Work ⁸	P	P	P
(E) Community Services	L ⁹	L ⁹	L ⁹
(F) Temporary Uses	P	P	P
(G) Home Occupations	P	P	P

Table 4.1420(A) Notes:

⁸ For purposes of Table 4.1420, a live-work unit is a structure that combines a limited office, retail services, and/or business services use with a residential living space. The commercial space may be used by anyone residing at the unit and by no more than two non-resident employees. The commercial portion of the structure shall face the street front, is limited to the first floor, and garage access must be from an alley. A fascia, awning, or painted wall sign limited to 32 square feet is permitted per each unit.

Section 4. Gresham Community Development Plan, Volume 3, Code, Section 4.1500, Springwater Plan District, is amended as follows:

Table 4.1507 Residential Permitted Uses			
Use Categories:	VLDR-SW	LDR-SW	THR-SW⁷
(A) Single-Family Detached Dwelling	P	P	L ¹
(B) Manufactured Homes on Individual Lots	P	P	P
(C) Single Family Attached Dwellings	NP	NP	P
(D) Two-unit Attached Dwellings	NP	L ²	NP
(E) Accessory Dwellings ³	P	P	P
(F) Community Services	L ⁴	L ⁵	L ⁵
(G) Accessory Structures	P	P	P
(H) Home Occupations	P	P	P
(I) Temporary Uses	P	P	P
(J) Residential Facility	P	P	P
(K) Residential Home	P	P	NP
(L) Live-Work ⁵	NP	NP	L ⁶

Key:

P = Permitted

L = Limited

NP = Not Permitted

Table 4.1507 Notes:

⁶ For purposes of Table 4.1507, a live-work unit is a structure that combines a limited office, retail services, and/or business services use with a residential living space. The commercial space may be used by anyone residing at the unit and by no more than two non-resident employees. The commercial portion of the structure shall face the street front, is limited to the first floor, and garage access must be from the alley. A fascia, awning, or painted wall sign limited to 32 square feet is permitted per each unit. Live-work units are permitted only for buildings fronting on a collector within 300 feet of a VC-SW zone.

Section 5. Gresham Community Development Plan, Volume 3, Code, Article VII, Site Design Review, is amended as follows:

7.0003 Applications. An application for Design Review is subject to the highest possible level of review, given the thresholds for different reviews described below. Design Review A is the lowest level; Design Review ~~ED~~ is the highest level Design Review. For uses not listed in this section, the Manager shall determine which level of Design Review is appropriate.

- A. Design Review A. Design Review A is reviewed as part of the building permit review. Design Review A may apply when the primary use is not proposed to change. The Design Review may include one of the following:
 - 1. Interior tenant improvements;
 - 2. Exterior façade updates including: additions of canopies, awnings or other mounted structures to an existing façade or roof; alteration of window or door location; or
 - 3. Additions with the total addition area not to exceed 1,000 square feet including:
 - a. Floor area additions of up to five percent of the existing floor area of the entire site. New floor area shall not create a separate structure; or
 - b. Outdoor area additions of up to five percent of the existing total outdoor area of the entire site.

- B. Design Review B. Design Review B is a Type I review. Design Review B may apply when: the primary use is not proposed to change; parking and landscaping requirements are already met; buffer requirements are already met; public facilities will already be adequate; and no additional residential units are proposed. The Design Review may include one of the following, provided it is greater than the thresholds in **Section 7.0003(A)**:
 - 1. Additions to an industrial land use up to 25,000 square feet, not to exceed twenty-five (25) percent of the existing floor area of the site;
 - 2. Additions to non- industrial land uses with the total addition area up to 10,000 square feet:
 - a. Floor area additions not to exceed twenty-five (25) percent of the existing floor area of the entire site. New floor area shall not create a separate structure; or
 - b. Outdoor area additions not to exceed twenty-five (25) percent of the existing outdoor area of the entire site.

- C. Design Review C. Design Review C is a Type II review, but is not subject to a pre-application conference. Design Review C may apply when there is: no increase in residential density that requires an increase in building area; no new buildings; or no new outdoor area, except as provided for below. The Design Review may include one of the following:
 - 1. A change to the primary use;
 - 2. Duplexes in all districts except LDR-5, LDR-7, TR and TLDR;
 - 3. A change to public facility requirements;
 - 4. A change to buffers, including an alternate buffer;
 - 5. A new driveway access;
 - 6. A change in landscaping requirements;
 - 7. An expansion to an existing parking lot; or
 - 8. A new parking lot.

- D.** Design Review D. Design Review D is a Type II review and is subject to a pre-application conference. Design Review D is a review by the Manager. The following are Design Review D, provided they are greater than the thresholds in **7.0003(A)-(C)** above. Proposals that do not meet the Design District Threshold for review by the Design Commission as noted in Section 3.0010 are also Design Review D.
1. Multi-family residential construction;
 2. Single-family attached residential construction;
 3. Manufactured dwelling parks construction;
 4. Commercial construction;
 5. Mixed Use construction;
 6. Industrial construction;
 7. Community Services Uses; or
 8. Parking garages.
- E.** Design Review E. Design Review E is a Type III review and is subject to a pre-application conference. Design Review E is a review and decision by the Design Commission for applications within the Design District that meet the Design District thresholds and the types of projects as noted in Section 3.0010 Design District, Design Review and in Table 11.0204 Land Use Applications and Review Authorities. The Design Review Type E may also be used by the applicant for smaller development projects within a Design District that propose to meet the Design Principles and Guidelines.
- FE.** **Table 7.0003** is provided to illustrate the application of the different levels of Design Review. This table does not supersede the information in **7.0003 (A)-(DE)**. For each row of the table, a person assesses if the work to be completed is part of a proposal. If so, the design review type specified for that type of work is circled. Once a person goes through the entire table, the highest level of review circled applies for the proposal.

TABLE 7.0003 DESIGN REVIEW APPLICATION

Work to be Completed	D.R. Level				
	A	B	C	D	E
Interior tenant improvements	X				
Façade updates	X				
Additions not to exceed 1,000 square feet and less than 5 percent increase in floor area or outdoor use area	X				
Additions greater than in Design Review A, up to 25,000 square feet for industrial uses, and up to 10,000 square feet for all other uses; and not to exceed 25 percent increase in floor or outdoor use area		X			
Change to primary use			X		
Duplex in all districts except LDR-5, LDR-7, TR or TLDR			X		
Change in public facility requirement			X		
Change in buffer, or alternative buffer requested			X		
New curb cut, driveway access			X		
Change in landscaping requirements			X		
New or expanding parking lot			X		

Increase in residential density without an increase in existing building area			X		
Additions greater than in Design Review B, exceeding 25,000 square feet for industrial uses; 10,000 square feet for all other uses <u>and not in Design District</u>				X	
Increase in number residential units and building area <u>and not in Design District</u>				X	
Manufactured Dwelling Park				X	
Community Services Uses <u>not in Design District</u>				X	
Site is currently undeveloped and <u>not in Design District</u>				X	
Parking Garage <u>not in Design District</u>				X	
<u>Smaller developments as defined in Section 3.0010 Design District, Design Review that are in a Design District and that choose the Design Standards process</u>				<u>X</u>	
<u>Smaller developments as defined in Section 3.0010 Design District, Design Review that are in a Design District and that choose the Design Guidelines process</u>					X
<u>Larger residential developments as defined in Section 3.0010 Design District, Design Review that are in a Design District and that choose the Design Guidelines process</u>					<u>X</u>
<u>Larger mixed-use, live/work, community service use, commercial and employment developments as defined in Section 3.0010 Design District, Design Review that are in a Design District and that choose the Design Guidelines process</u>					<u>X</u>
<u>All public building community service uses</u>					<u>X</u>
<u>Parking garages in Design District</u>					<u>X</u>

7.0201 Dwelling Structures Containing Two or More Units, Single Family Attached Dwelling Units and for Elderly Housing

E. Building Orientation

1. Any building abutting a public street right-of-way shall be oriented to the street. The street orientation standard is met when the following criterion is satisfied for buildings other than those in the Downtown Plan District:

The primary entry or entries for all ground-floor units abutting the street shall open directly onto the street right-of-way, not to the interior of the site or to a parking lot. Secondary entrances may face parking lots or other interior site areas. Non-residential buildings, such as recreation or community centers, which abut a public street right-of-way shall have at

least 20% of the ground floor wall area facing the street in windows, doorways, or display areas, including an entry opening directly onto the abutting street. The primary entry for attached single-family dwellings with frontage on both a public street and an alley shall be oriented to the street, not to the alley.

2. At least 50% of the site's frontage (not including access driveways) on any street shall be occupied by buildings oriented to the abutting street (excepting buildings in the Downtown Plan District and excepting courtyard style single family attached developments. See **Section 7.0201(M)**). Where a site has less than 70 feet of street frontage this standard may be modified as needed to accommodate a driveway meeting code standards.

G. Vehicular Circulation and Parking

Vehicular circulation and parking shall be provided in accordance with the following standards:

2. Where new parcels or blocks are created within a development site as a result of required public street dedications, individual parcels or blocks need not meet minimum residential density standards for the district, provided the development as a whole meets the standard. Applicable standards of **Section 7.0201** regarding site landscaping shall also apply to the development as a whole, except that any parcel or block within the development that is not in the Downtown Plan District shall have a minimum of 20% of its total area in landscaping, as defined in **Section 3.0010**. In additional districts, the Manager may require that deed restrictions be recorded to ensure that where project amenities such as swimming pools, community centers, and shared open space are on separate parcels within the same development, all residents of the development will have on-going access to those amenities and facilities.

6. Except for single-family dwelling structures, which are subject to parking and access standards within **Section 7.0201(M)**, dwelling structures containing two or three dwelling units shall have additional off-street parking when the lot has limited frontage or where on-street parking is not allowed. This provision does not apply to two or three dwelling unit structures in the Downtown Plan District.

H. Crime Prevention.

7. Safe Neighborhood Design Performance Standards of **Section 4.0132(D)** shall apply to single-family attached dwellings except those in the Downtown Plan District.

J. Architectural Design

Architectural Design Standards apply to all uses subject to Design Review except single-family attached dwellings (which are subject to the standards of subsection (L) or the applicable standards in the Downtown Plan District) and Downtown Plan District uses except duplexes (duplexes must comply with applicable standards in subsection (J)):

Architectural Design Standards for all uses except single family attached dwellings in all districts (which are subject to the standards of subsection (L)):

M. Other standards for Single Family Attached Dwellings

2. Driveways and Unit Access:

The following standards are intended to promote access at the rear of the lots so as to limit the number of driveways to the street and to allow for availability of on street parking.

- c. The maximum width for individual single garage driveways located at the front of a dwelling unit is 10 feet wide (excluding the driveway approach width). Alternately, a driveway may be wider to access a garage that has a width no larger than 50% of the total dwelling width.
- d. The maximum width for a shared driveway that serves two units or a double car (side-by-side) garage is 20 feet (excluding the driveway approach width), except in the Downtown Plan District, where the maximum driveway width is 16 feet.

Table 7.0201(M)(3)(a)

Minimum Setback Standards for SFA Dwelling Units	Districts with Maximum Density generally at or less than 24 units per acre (TLDR, MDR-12, MDR-24, <u>and</u> OFR, and DR-12 districts)	Districts with Maximum Density greater than 24 units per acre and the CMF and CMU Districts
Front facade	10 feet	5 feet
Front porch	6 feet	3 feet
Front face of garage	20 feet	20 feet
Interior side (not common wall)	5 feet	5 feet
Side (common wall)	0 feet	0 feet
Street side (façade)	6 feet	5 feet*
Street side (porch)	6 feet	5 feet*
Street side (garage face)	20 feet	20 feet*
Rear (no alley)	10 feet	10 feet
Rear (alley)	6 feet	5 feet

*For street side in MDR-C, see Table 4.1230(E).

- (g) Except for the Downtown (4.1100), Pleasant Valley (4.1400) and Springwater (4.1500) Plan Districts, heights for single family attached dwellings are as identified in the following table:

Table 7.0201(M)(3)(g)

Building Heights for SFA Dwelling Units	<u>TLDR, MDR-12, MDR-24, and OFR, and DR-12</u> districts	All other districts where SFAs are permitted
Minimum building height	1 story	1 story in CMF and CMU; all others 2 stories
Maximum building height	35 feet*	45 feet*

*See Building Height and Grade definitions from Section 3.0000.

- (9) Except in the Downtown Plan District, the single family attached dwelling standards in these sections shall take precedence over individual plan district standards in case of conflict except as otherwise stated.

7.0202 Community Service, Commercial, Industrial, and Mixed Use Developments

B. Energy conservation is promoted by one or all of the following measures except for those developments in the Downtown Plan Districts:

1. Evergreens are planted on the north and east sides of the proposed structure so that the trees will be within 10 feet of the structure at maturity to buffer against the cold north and east winds.
2. Large areas of window glass and unprotected entry doors are avoided on the north and east sides of the structure. Where north and east facing entries cannot be avoided, there shall be an effective windbreak such as a wall, fence or double entry door.
3. The long axis of the building is oriented east and west with unobstructed solar access to the south wall and roof.
4. The majority of the windows are located on the south side of the structure to take advantage of passive solar collection and architectural shading devices (such as window overhangs) are incorporated to reduce summer heat gain.

Innovative Housing Demonstration Projects

7.0302 Applicability

A. Land-use districts: Innovative housing demonstration projects are allowed in the following land-use districts: LDR-5, TLDR, TR, MDR-12, MDR-24, CMF, CMU, DRL-1, DRL-2 and THR-SW.

7.0303 Review of Projects

- A. Innovative housing demonstration projects will be reviewed by the Planning Commission using a Type III procedure as detailed in Section 11.0500, except for those in a Design District which will be reviewed by the Design Commission. This process is a discretionary review, using the Development Principles and Guidelines in Section 7.0310 to guide decision-making. Projects within a Design District will also be reviewed using the Design Principles of the Design District to assist in judging compatibility with the applicable district or Plan District. Once deemed complete and prior to the Planning Commission hearing on a proposal outside of a Design District, the Design Commission will issue a recommendation on the design aspects of the proposal in accordance with Section 11.0402(D)(1)(e) for review and comment on applications. For those applications in a Design District, the Design Commission will issue a decision on all aspects of the proposal. Appeals of the decision will be heard by the City Council.

7.0321 Permitted Uses

The following uses are permitted as part of an innovative housing demonstration project:

- A. Single-family detached homes.
- B. Single-family attached homes. In the LDR-5 and DRL-1 districts, these units may make up no more than 30 percent of the total number of units in a project.
- C. Narrow-lot houses.
- D. Cottages.
- E. Duplexes, tri-plexes and four-plexes, if the following are satisfied:
1. The units are designed to appear as a single-family home in that they have similar scale, height, building orientation, and articulation.
 2. The units make up no more than 30 percent of the total number of units in a project in LDR-5, DRL-1, TLDR, and TR, and THR-SW.
 3. The units make up no more than 50 percent of the total number of units in a project in MDR-12, MDR-24, CMF, CMU.
- F. Home occupations.
- G. Accessory dwellings.
- H. Accessory structures.
- I. Other residential uses consistent with the Development Principles and Guidelines may be reviewed and approved through this process.

7.0322 Density

- A. Minimum Density. The minimum density for a demonstration project shall be the same as the underlying land use district.
- B. Maximum Density. The maximum density for a demonstration project shall be the same as the underlying land use district.

In the LDR-5, TLDR, and TR, and DRL-1 districts, the maximum density for an innovative housing demonstration project comprised of detached single family dwellings shall be the same as the underlying land use district. If other housing types are used, the number of units may be increased as follows:

1. Cottage – two cottages may be allowed in place of one detached single family dwelling.
2. Duplex – two housing units (a duplex building) may be allowed in place of one detached single family dwelling.
3. Tri-plex and Four-plex – 1.5 housing units may be allowed in place of one detached single family dwelling.
4. Narrow-lot house - 1.5 housing units may be allowed in place of one detached single family dwelling.

7.0323 Unit Size

- A. The maximum unit size for specific housing types are:
 - 1. Single-family detached home: No limit.
 - 2. Cottage: 1,500 square feet, with the second story not to exceed 50 percent of the square footage of the first story.
 - 3. Duplex, tri-plex and four-plex: 1,500 square feet per unit.
- B. In the LDR-5, TR, and TLDR, and DRL-1 land use districts, where more residential units may be permitted if certain housing types are provided, the total floor area of each unit in that demonstration project may not be increased above the unit sizes listed above. A note shall be placed on the plat of the property that notifies future property owners that any increase in the total square footage of the unit is limited to the standards identified above for the life of the unit.

7.0324 Dimensional Requirements

- A. Minimum setbacks:
 - 1. Primary structures from adjacent property lines along the perimeter of the site, including any public street, shall have a setback of 10 feet, except for structures in DRL-1 and DRL-2 which can follow the setback requirements in the underlying districts. Accessory structures shall meet the setbacks for Accessory Structures as outlined in Section 10.0200.
 - 2. The minimum distance between structures in a demonstration project is 10 feet.

7.0325 Open Space

- A. Common open space requirements:
 - 1. For each unit in a demonstration project, a minimum of 400 square feet shall be allocated to common open space, except in DRL-1 or DRL-2, where a minimum of 200 square feet per unit shall be allocated to common open space.
 - 2. The location and configuration of common open space areas should provide for recreational opportunities and take into consideration sensitive areas.

Section 6. Gresham Community Development Plan, Volume 3, Code, Article VIII, Special Uses, is amended as follows:

8.0102 Review of Community Services

- A. Type I Community Service uses are reviewed under the Type I procedure.
- B. Type II Community Service uses are reviewed under the Type II procedure.
- C. Type III Community Service uses are reviewed under the Type III procedure. All Type III Community Service Uses, except those Community Services uses in a Design District which are associated with a Design Commission Design Review, are heard by the Hearings Officer with the Planning Commission as the appeal body except the following major community services, which will be heard by the Planning Commission with the City Council as the appeal body:
 - 1. Public or private high schools or colleges;
 - 2. Stadium, arena or auditorium;
 - 3. Public buildings such as, but not limited to a city hall, post office or library; and
 - 4. Hospital.

For those Community Service uses that are in a Design District and are associated with a Design Commission review, the applications shall be reviewed and decided on by the Design Commission with an appeal to City Council.

8.0114 Design Review and Approval Criteria for Community Services

- A. Most Community Services require submission of a Design Review application as provided by **Article VII** of the Community Development Code. Type I Community Services and the following uses are exempt from design review and public facilities requirements:
1. Child care facility for 12 or fewer children and adult foster homes within an existing residential structure.
 2. Recycling drop box.
 3. Expansion of the floor area of a community service use by less than 2,000 square feet.
- B. An applicant shall provide a narrative that details how the proposal fulfills the applicable requirements found in **Article VII**, those of the applicable plan district(s) for the site and other applicable Code sections. The narrative shall also describe how the proposal is designed to be as compatible with the surrounding land uses as possible and measures are taken to mitigate any unavoidable negative impacts. Factors to be considered include:
1. location of parking and loading, and effects on off-site parking;
 2. generation of high amounts of traffic;
 3. street access points;
 4. buffering and screening to protect privacy;
 5. noise or illumination controls;
 6. structure height;
 7. hours of operation;
 8. crime prevention;
 9. visual elements (e.g., scale, structural design and form, materials);
 10. signage;
 11. noxious odors;
 12. lighting;
 13. effects on air and water quality;
 14. potential for glare, noise and dust, vibration, and other environmental effects which may disturb neighboring property owners;
 15. other impacts which are unique to the specific use.

- E. An applicant for a public walking/hiking trail or public multi-use path including associated trail access points and trailheads shall provide narrative and plans that demonstrate consistency with applicable provisions of the Gresham Trails Master Plan Chapters 7 and 10 and any applicable district requirements.

If the application includes a surface parking lot it shall be consistent with **Section 9.0823** and any applicable district parking requirements except that the applicant may prepare an alternative landscaping plan and specifications which meets the intent of the requirement in **Section 9.0823(C)(1), (2) and (3)** and the general intent of the Gresham Trails Master Plan that planting for buffer, screening and revegetation shall be of native plant species compatible with and blending with the natural surroundings.

- F. An applicant for a cellular communications facility that includes a cell tower must co-locate on an existing cell tower, unless it can be reasonably demonstrated that such is not feasible, in which case the new cell tower shall be grouped at the same site, or, be located no closer than 2,000 linear feet from another cell tower. No cell tower shall be located within the LDR-5, LDR-7, TLDR, and TR, DRL-1 and DLR-2 Districts, unless such location is absolutely necessary for the

maintenance of cellular communications within that cell area. For all cell tower proposals, the facility shall be located at least 200 feet from the nearest residence and must be constructed to allow co-location of additional antenna facilities.

8.0121 Standards

Community Services shall be located in conformance with the following standards:

F. Commercial parking facilities are not permitted in the LDR-5, LDR-7, DRL-1, TLDR, TR or MDR-12 Districts.

H. Community Service structures shall meet the following site development requirements, except for proposed uses in an existing residential structure:

1. Minimum yard setbacks in LDR-5, LDR-7, TLDR, TR MDR-12, MDR-24, and CMF:
 - a. Front yard = 30 feet
 - b. Side yard = 20 feet for one-story buildings and 25 feet for two or more stories.
 - c. Rear yard = 25 feet
 - d. Exception: structures included with a public trail development shall be as required in the district, and, structures fronting a transit street or arterial street shall be subject to the front yard setbacks as required in the district.
2. Minimum yard setbacks in OFR, NC, GC, RTC, SC, CMU, CC, MC, GI, ~~and HI~~, and all Downtown Plan Districts: All yards shall be as required in the district.

K. Helicopter landing facilities shall meet the following standards:

1. Are permitted in GC, RTC, CC, GI, HI Districts, and the Downtown Plan District and the Civic Neighborhood Plan District.
2. Are permitted in the LDR-5, LDR-7, DRL-1, DRL-2, TLDR, TR, MDR-12, MDR-24, CMF, CMU, SC, MC, OFR and NC Districts as an accessory to a community service use. Only trips which support the community service use are allowed.

Section 7. Gresham Community Development Plan, Volume 3, Code, Article IX, Common Requirements, is amended as follows:

9.0821 Parking Lot Location on Transit Streets and in the ~~Downtown~~ and Civic Neighborhood Plan Districts

- A. Auto parking lots shall be located behind or beside buildings on one or both sides.
- B. Auto parking and maneuvering areas (including any permitted drive-through service lanes) shall not be located between the street and the building facade with the primary entrance (abutting primary or secondary pedestrian street in the Civic Neighborhood).

- C. Auto parking lots and maneuvering areas located to the side of a building cannot occupy more than 50% of the site's frontage onto a transit street, ~~any street in the Downtown Plan District~~, or a primary or secondary pedestrian street in the Civic Neighborhood. When a site in the Civic Neighborhood has frontage on both a primary and a secondary pedestrian street, an auto parking lot located to the side of a building may occupy more than 50% of the site's frontage on the secondary pedestrian street.
- D. Wherever possible, auto parking lots and maneuvering areas on corner lots should not be located adjacent to intersections.
- E. In the Civic neighborhood, if a multi-block development includes a block or blocks with frontage on a primary pedestrian streets and a block or block without frontage, no block that has frontage on the primary pedestrian street may be entirely occupied with parking.
- ~~F. If a site in a Downtown Plan District has frontage on two intersecting streets, then all Building Orientation and Ground Floor Window standards of Section 7.0210(A)(4) & (5) apply to both adjacent streets, and auto parking lots shall be located behind or beside buildings on at least one of the street frontages.~~
- FG. Exemption. On sites of less than 10 acres with an average down-slope from the abutting transit street in excess of 7%, parking may be located between the transit street and a building or located adjacent to a transit street intersection, provided that:
 1. On-site pedestrian connections are made from the parking lot to all adjacent transit streets in a direct as practical manner. Stairs may be required where curb ramps are impractical. Circuitous routes shall be avoided.
 2. There is at least one direct accessible route, from the transit street and to the primary building entrance, part of which may be located along an adjacent intersecting street.
 3. If the sloped site is adjacent to another street that intersects a transit street, then Transit Street, Downtown Plan and Civic Neighborhood design standards that apply to Transit Streets shall apply to the adjacent street instead of the transit street.
 4. On the adjacent street frontage, auto parking lots shall be located behind or beside buildings on one or both sides. Auto parking and maneuvering areas shall not be located between the building facade with the primary entrance and the adjacent street.

(For purposes of subsection (GF), the slope perpendicular to the street, shall be the average as measured at regular intervals between the transit street frontage. The average slope shall be measured between the street and the rear lot line, prior to any grading.)

9.0823 Landscaping of Parking Lots

- 6. Alternate Landscaping Plan. For parking lots with 20 or fewer vehicle parking spaces located in the Rockwood Town Center, and Station Centers, ~~Central Urban Core, Downtown Transit, and Downtown Moderate Density Residential (DR-30)~~ districts, an applicant may prepare an Alternate Landscaping Plan and specifications which meets the intent of the requirement in **Subsections (C)(1), (2), and (3)** above and the intent of the district. In addition, alternate landscaping plans also can be proposed as part of the discretionary review process in the Downtown Plan District.

9.0852 Plan District Minimums and Maximums

B. Downtown Plan District

Off-street parking facilities provided in connection with new development must be appropriate for the unique characteristics of the Downtown PD. This means that parking should be a clearly secondary use, designed so that it does not interfere with easy and safe pedestrian movement. By ensuring that parking lot sizes do not become excessive, and that appropriate landscaping is provided, the Downtown area will be more attractive, and more conducive to creating a lively, prosperous district for the benefit of area residents, employees, and visitors.

1. Minimum/Maximum Off-Street Parking: per 1,000 square feet of floor area
 - a. Minimum Off-Street Parking for commercial per 1,000 square feet of floor area: Off-street parking is not required for new commercial development in the CUC, DT, DR-30, and DR-12 DCC and DTM Sub-Districts. In the DC-1, and DC-2 DMU, DEM, DRL-1, DRL-2 and DCL Sub-Districts, minimum required parking for new commercial uses is 80% of the minimum number of spaces required in **Table 9.0851**. If required by the Building Code, accessible parking spaces shall be provided regardless of the provisions of this subsection. The minimum amount of off-street parking required in any sub-district of the Downtown PD may be reduced by up to 50%, as provided in **Section 9.0853(E)**.
 - b. Minimum Off-Street Parking for residential: 1 space per unit for residential.

2. Maximum Off-Street Parking:
 - a. Maximum Off-Street Parking for commercial per 1,000 square feet of floor area: Maximum off-street parking permitted for commercial uses shall be 125% of the amount of parking required in **Table 9.0851** when such uses are part of a mixed-use development. Maximum parking permitted for commercial uses which are not part of a mixed-use development shall be 100% of the amount of parking required in **Table 9.0851**, except that when such uses that exceed 90,000 square feet of floor space the maximum parking permitted shall be 80% of the amount required in **Table 9.0851**. Additional off-street parking beyond these maximums may be permitted within the Downtown Plan District, as provided below in **9.0852(B)3**.
 - b. Maximum Off-Street Parking for residential: no maximum for residential, except 1.5 spaces per unit for attached dwelling units on a single lot, which does not include single-family attached dwellings.

3. Parking in addition to maximums: Under Type II procedures, the Manager may approve off-street parking in excess of the maximum allowed parking spaces specified in this section when the applicant can show that all the following criteria are met:
 - a. The proposed development is highly supportive of the intent of the Gresham Downtown Plan, as contained in Volume 2 – Policies 1– Findings of the Community Development Plan; and
 - b. The need for additional parking cannot reasonably be met through provision of on-street parking or shared parking with adjacent or nearby uses; and
 - c. The proposed development demonstrates that its design and intended uses will support high levels of both transit and pedestrian activity; and
 - d. The site plan shall indicate where additional parking can be redeveloped to more intensive transit-supportive use in the future; and

- e. The parking in excess of the maximum is constructed as part of a parking structure.
 - e. ~~The development incorporates special features beyond the minimum design standards of Sections 4.1135, 4.1136, 4.1137 and 4.1142. Special features will include at least one of the following: Covered exterior pedestrian arcades or public plazas with fountains/public art; ground floor retail uses, or mixed uses, or community services which especially promote high levels of pedestrian and transit activity; multiple story buildings; underground or structured parking.~~
4. New surface parking lots in the Downtown Plan District are limited to 40,000 square feet in size per site.
 5. Commercial Parking Facilities on surface parking lots are prohibited in the Downtown Plan District. Public parking facilities owned by a local, county, or regional governments, an urban renewal agency or a transportation management association are allowed on surface lots or in structures.
 6. New surface parking lots can only be constructed in the Downtown Plan District as approved with a development permit for an allowed use that requires off-street parking, except for public parking facilities.
 7. Parking facilities are not allowed as the first phase of a development but must be constructed in conjunction with buildings that are part of the development.
 8. Bicycle parking shall be provided in conformance with Section 9.0830 and Table 9.0851.

9.0854 Exceptions to Maximum Parking Space Standards

- A. Except in the Downtown Plan District, if application of the maximum parking standard would result in less than 6 parking spaces for a development with less than 1,000 square feet of floor area, the development shall be allowed up to ~~six~~ parking spaces. If application of the maximum standards would result in less than 10 parking spaces for a development of 1,000 to 2,000 square feet of floor area, the development shall be allowed up to 10 parking spaces.
- B. In the following land use districts, the Manager, under Type II procedures, may approve exceptions to the maximum parking space standards based on the criteria provided below:

9.0862 Commercial Parking Facilities

- A. ~~In the Downtown PD, off-street parking facilities should generally be secondary uses, designed so that they do not interfere with easy and safe pedestrian movement or discourage transit usage. However, there may be instances where off-street parking provided in connection with individual development projects and in public parking facilities is inadequate to meet the demand for parking in the vicinity. It is the purpose of this section to allow for development of commercial parking facilities to address documented demand for off-street parking, in ways which do not conflict with the objectives of creating a lively, prosperous, and pedestrian-friendly downtown area.~~

B. Criteria for Commercial Parking Facilities:

A commercial parking facility may be permitted within the Downtown PD as a community service use. Commercial parking facilities shall be exempt from the standards of **Section 4.1137**. In addition to meeting other applicable standards and requirements, an applicant for a proposed commercial parking facility within the Downtown PD shall provide findings to satisfy the following criteria:

1. An inventory of existing on-street and off-street parking, and an analysis of demand for additional off-street parking in the vicinity indicate an unmet demand for additional off-street parking; and
2. The need for additional parking cannot reasonably be met through provision of on-street parking or shared parking involving adjacent or nearby uses; and
3. The commercial parking facility will not conflict directly with the purposes of the **Downtown PD and the Gresham Downtown Plan (Volume 1 Findings)**, particularly the purposes of supporting high levels of transit usage and pedestrian activity.

9.0863 MAX Park-and-Ride

Park-and-ride facilities serving MAX stations shall be subject to the following requirements:

- A. New surface park-and-ride facilities shall not exceed 50 spaces on a contiguous site.
- B. New park-and-ride facilities which exceed 50 spaces on a contiguous site shall be contained within a multi-story structure.
- C. Expansion of surface park-and-ride facilities outside of the site as it existed on the effective date of this sub-sections is not allowed except within a multi-story structure. Existing surface park-and-ride facilities may be maintained, redeveloped, or re-striped. Redevelopment of existing surface park-and-ride sites for structured parking shall be encouraged, in accordance with standards of **Section 9.0861**.

Section 8. Gresham Community Development Plan, Volume 3, Code, Article XI, Procedures, is amended as follows:

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TABLE 11.0204

LAND USE APPLICATIONS AND REVIEW AUTHORITIES

R = RECOMMENDATION D = DECISION AUTHORITY A = APPEAL AUTHORITY

Code Citation	Application	Pre-app required?	Type	Manager	Hearings Officer	Urban Forestry Sub-committee	Historic Resources Advisory Committee	Design Commission	Planning Commission	City Council
RESIDENTIAL										
10.0100	Accessory Dwelling Units	N	II	D	A					
10.0300	Ancillary Dwelling Units	N	I	D	A					
10.0410	Conversion of Elderly Housing	Y	III		D					A
10.0400	Conversion of Residential Units	N	II	D	A					
10.0502	Home Occupation	N	I	D	A					
10.0502	Home Occupation	N	II	D	A					
10.1300	Health Hardship Dwelling Unit	N	I	D	A					
4.0135	Single Family Residence/Duplex on a Lot	N	I	D	A					
LOTS AND PARCELS (6.0000)										
6.0020	Condominium Creation	Y	II	D	A					
6.0200	Partition	Y	II	D	A					
6.0300	Planned Developments	Y	III		D				A	
6.0100	Property Line Adjustments and Lot Consolidations	N	I	D	A					
6.0200	Subdivision - Preliminary	Y	II	D	A					
6.0001	Land Division - Final Plat	N	I	D	A					
6.0410	Extension of land use decision	N	I	D	A					
OVERLAY DISTRICTS (5.0000 and A4.000)										

TABLE 11.0204

LAND USE APPLICATIONS AND REVIEW AUTHORITIES

R = RECOMMENDATION D = DECISION AUTHORITY A = APPEAL AUTHORITY

Code Citation	Application	Pre-app required?	Type	Manager	Hearings Officer	Urban Forestry Sub-committee	Historic Resources Advisory Committee	Design Commission	Planning Commission	City Council
5.0000	Special Purpose District Boundary Revisions	Y	III		D				A	
5.0300	Historic Landmarks									
A4.004	Removal from List	Y	II	D	A		R			
5.0322	Demolition of Historic Landmark	Y	III		D		R	A		
5.0321	Alteration of Historic Landmark	N	III		D		R	A		
A4.002	Addition to list	N	IV				R			D
5.0400	Habitat Conservation Area									
5.0411, 5.0412	Development in HCA	Y	II	D	A					
5.0413	Minor HCA Variance (5.0413.D or 5.0413.E)	Y	II	D	A					
5.0413	Major HCA Variance (5.0413.D or 5.0413.E)	Y	III		D					A
5.0414	City-initiated modification/correction of HCA map	N	I	D	A					
5.0414	Applicant-initiated modification/correction of HCA map	Y	II	D	A					
5.0500	Open Space									
5.0500	Open Space Dedication, in Open Space or Natural	N	I	D	A					

TABLE 11.0204

LAND USE APPLICATIONS AND REVIEW AUTHORITIES

R = RECOMMENDATION D = DECISION AUTHORITY A = APPEAL AUTHORITY

Code Citation	Application	Pre-app required?	Type	Manager	Hearings Officer	Urban Forestry Sub-committee	Historic Resources Advisory Committee	Design Commission	Planning Commission	City Council
	Resource Site									
5.0500	Open Space Dedication in other areas	N	II	D	A					
5.0100	Floodplain Development Permit	Y	II	D	A					
5.0100	Floodplain Development Permit (if restoration)	N	I	D	A					
MODIFICATIONS AND VARIANCES										
10.1520	Adjustment to Regulations	N	II	D	A					
11.0107	Code Interpretation	N	I	D	A					
10.1530	Major Variance	Y	III		D					A
10.1510	Minor Variance	N	II	D	A					
11.0104	Modification of Conditions	N	*							
10.1521	Modification of Regulations	N	II	D	A					
9.0110	Alternative Buffers	N	II	D	A					
7.0212	Solid Waste Collection Standard exception	N	II	D	A					
TREES (9.1000 & A14.000)										
9.1010	Tree removal (street tree, parking lot tree, etc.)	N	I	D	A					
9.1010	Regulated Tree Removal	N	II	D	A					
A14.000	Significant Tree Designation	N	III		D	R				A

TABLE 11.0204

LAND USE APPLICATIONS AND REVIEW AUTHORITIES

R = RECOMMENDATION D = DECISION AUTHORITY A = APPEAL AUTHORITY

Code Citation	Application	Pre-app required?	Type	Manager	Hearings Officer	Urban Forestry Sub-committee	Historic Resources Advisory Committee	Design Commission	Planning Commission	City Council
9.1010	Removal of Significant Tree	N	III		D					A
A14.000	Significant Tree Designation Removal	N	III		D	R				A
DESIGN REVIEW (7.0000)										
7.0003	Design Review A	N	I	D	A					
7.0003	Design Review B	N	I	D	A					
7.0003	Design Review C	N	II	D	A					
7.0003	Design Review D, Manager decision	Y	II	D				A		
7.0003	Design Review D, Design Commission decision	Y	II					D		A
<u>7.0003</u>	Design Review <u>D</u> , Design District	Y	II	D				A		
<u>7.0003</u>	Design Review <u>E</u> , Design District	Y	III					D		A
COMMUNITY SERVICE USES (8.0100)										
8.0110	Community Service Use I	N	I	D	A					
8.0111	Community Service Use II	Y	II	D	A					
8.0112	Community Service Use III	Y	III							
	Minor	Y	III		D				A	
	Major	Y	III						D	A
	<u>Major in Design District</u>	<u>Y</u>	<u>III</u>					<u>D</u>		<u>A</u>
TEMPORARY USES (10.1400)										

TABLE 11.0204

LAND USE APPLICATIONS AND REVIEW AUTHORITIES

R = RECOMMENDATION D = DECISION AUTHORITY A = APPEAL AUTHORITY

Code Citation	Application	Pre-app required?	Type	Manager	Hearings Officer	Urban Forestry Sub-committee	Historic Resources Advisory Committee	Design Commission	Planning Commission	City Council
10.1401	Temporary Use Permit	N	I	D	A					
NON-CONFORMING USES (8.0200)										
8.0250	Establishment	N	I	D	A					
8.0220	Replacement	N	I, II	D	A					
8.0200	Expansion	Y	II	D	A					
TRANSPORTATION										
9.0700	Future Street Plans									
9.0700	New or revised, in conjunction with a land division and/or design review	Y	II	D	A					
9.0700	New or revised future street plan, independent of other land use review	Y	III		D					A
9.0700	Revision in conjunction with the Community Development Plan	N	IV						R	D
A13.000	Street Name Change	N	IV						R	D
OTHER										
11.0106	Extension of land use approval	N	I	D	A					
A1.0000	Annexation	Y	IV							D
12.0000	Code or Plan Map Amendment									

TABLE 11.0204

LAND USE APPLICATIONS AND REVIEW AUTHORITIES

R = RECOMMENDATION D = DECISION AUTHORITY A = APPEAL AUTHORITY

Code Citation	Application	Pre-app required?	Type	Manager	Hearings Officer	Urban Forestry Sub-committee	Historic Resources Advisory Committee	Design Commission	Planning Commission	City Council
12.0000	One parcel or small group of parcels	Y	III						R	D
12.0000	Large area/multiple ownerships	N	IV						R	D
11.0206	Code or Plan Text Amendment	N	IV						R	D
4.1470-4.1486, 4.1530-4.1535	Master Plans	Y	III						D	A
A6.010	Sign Permit (Design Commission review if sign is part of a project requiring Commission review)	N	I	D	A			R		
A7.000	Vacations	Y	IV						R	D
4.1432, 4.1452, 4.1572 & 4.1592	ESRA Modification		II	D	A					
	Miscellaneous Type I	N	I	D	A					
	Miscellaneous Type II	N	II	D	A					
	Miscellaneous Type III	Y	III		D					A
*	Level of review for modification shall be the same level of review as for the component of the application for which the modification is sought									

11.0402 Type II Procedures

- A. Pre-Application Conference and Optional Design Commission Consult
 - 1. **Table 11.0204** identifies all Type II applications that require a pre-application conference.
 - 2. Pre-application conference procedures are included in **Section 11.0700**.
 - 3. The Optional Design Consult procedures for those projects within a Design District are described in **Section 11.0502(3)**

11.0502 Type III Procedures

- A. Pre-Application Conference and Optional Design Commission Consult
 - 1. **Table 11.0204** identifies all Type III applications that require a pre-application conference.
 - 2. Pre-application conference procedures are included in **Section 11.0700**.
 - 3. Optional Design Commission Consult
 - a. An applicant of a Type III proposal that is to be reviewed by the Design Commission may choose to meet with the Design Commission after the Pre-Application Conference and prior to the formal application submittal for advice on the proposal.
 - b. Design Commission Consult procedures are included in **Section 11.0700**.

- G. Type III Notice of Decision
 - 1. A written decision in the form of a land use order shall be prepared regarding the Type III application within approximately 60 days from the date the application is deemed complete unless the applicant chooses to extend the application. For Design District applications to be reviewed and decided upon by the Design Commission, the applicant may choose to request a continuance of the hearing in order to make the requested modifications to the application. In this circumstance, the applicant shall return to the Design Commission at a later date for a final decision. The land use order shall include:

HI. Appeal of a Type III Decision

11.0700 Pre-Application Conference and Optional Design Commission Consult

11.0701 Purpose

- A. Pre-Application Conference. The purpose of the pre-application conference is to acquaint the applicant or representative with the requirements of this code, including relevant approval criteria, standards and procedures. It is designed to assist the applicant. The pre-application conference is not an exhaustive review of all potential issues, and the conference does not bind or preclude the City from enforcing all applicable regulations or from applying regulations in a manner differently than may have been indicated in the pre-application conference.

B. Optional Design Commission Consult. The purpose of the Design Commission consult is to provide an opportunity for the applicant to gain initial insight into the Design Commission's preliminary evaluation of whether the proposal appears to meet the Design Principles, Guidelines and Standards of the Design District. The Design Commission is not rendering a decision at this meeting nor is this meeting an exhaustive review of all potential issues. The conference does not bind or preclude the City from enforcing all applicable regulations or from applying regulations in a manner differently than may have been indicated in the Design Commission consult.

11.0702 Applicability

Table 11.0204 identifies all applications that require a pre-application conference. The Optional Design Commission consult is an option for the applicant and not a requirement.

11.0703 Pre-Application Conference and Optional Design Commission Consult Procedures

A. Initiation and Scheduling of Pre-Application Conference and Optional Design Commission Consult

1. The applicant shall submit a completed form provided by the Manager for that purpose, the relevant fee, and copies of information required on the form.
2. The City will schedule and conduct the pre-application conference within approximately twenty (20) calendar days of receipt of a request for a pre-application conference.

The City will schedule and conduct the Design Commission Consult within approximately twenty (20) calendar days of receipt of a request for a Design Commission Consult meeting.

3. The Manager shall coordinate the involvement of City staff responsible for planning, development review, roads, drainage, and other subjects, as appropriate, in the pre-application conference. The pre-application conference is not open to the general public.

The Manager shall coordinate the involvement of City staff and the Design Commission members responsible for providing consult meeting input.

B. Pre-Application Conference and Optional Design Commission Consult Summary

1. Within approximately fourteen (14) calendar days after a pre-application conference or Optional Design Commission consult, the Manager may provide to the applicant, or the applicant's agent, a written summary of the conference. The purpose of the written summary is to provide a preliminary assessment of a proposal and is not to be construed as a final recommendation by the City, by the Design Commission or by any other outside agency or service provider on the merits of the proposal.
2. The written pre-application conference summary should:
 - a. Summarize the proposed use and relevant characteristics of the proposal;
 - b. Identify necessary application submittal requirements;
 - c. Identify the relevant approval criteria and development standards, with a disclaimer that the approval criteria and development standards in effect at the time an application is received will control;
 - d. Identify specific additional information that is needed to respond to the relevant criteria and development standards or is recommended to respond to other issues; and

- e. Identify applicable application fees, with a disclaimer that fees are subject to change and that the fees in effect at the time a complete application is received will control.

3. The written Design Commission consult should:

- a. Summarize the proposed use and relevant characteristics of the proposal;
- b. Identify Design Commission comments on the proposal with regard to the relevant Design Principles, Guidelines and Standards; and
- c. Identify the applicant’s response to the Design Commission comments.

C. Pre-Application Conference Validity Period, Optional Design Commission Consult Validity Period & Follow-Up Conferences

- 1. If a complete application relating to a proposed development action that was the subject of a pre-application conference has not been submitted within eighteen months of the conference, the applicant shall schedule a follow-up pre-application conference.
- 2. An applicant may request a follow-up pre-application conference or an additional Design Commission consult, if desired.
- 3. A new or follow-up pre-application conference is required in the following instances:
 - a. The number of residential units increases by twenty (20) percent or more;
 - b. The proposed use changes between residential, mixed-use, commercial, or industrial;
 - c. There is a significant change in circulation;
 - d. The type of community service use is changed; or
 - e. Additional application reviews that require a pre-application conference as per Table 11.0204 are needed to accommodate the proposal.

A new or follow-up pre-application conference is not needed if these instances were discussed in the pre-application conference and incorporated in the City’s meeting notes.

Section 9. Gresham Community Development Plan, Volume 1, Findings, Appendix 37, Gresham Downtown Plan, is amended as follows:

Section 1

Introduction

1.1 EXECUTIVE SUMMARY

The Downtown Plan District contains approximately ~~330~~ 550 acres. This area generally encompasses properties between NW Eastman Parkway on the west, NE Hogan Drive on the east, both sides of NE Burnside Road to the north and both sides of E Powell Boulevard to the south. ~~It is bounded by NW Division Street to the north, NE Burnside Road and NE Hogan Drive to the east, Powell Boulevard (both sides) to the south and NW Eastman Parkway to the west.~~

The Downtown is planned to have a capacity for approximately 1,800 housing units and 7,700 jobs. The Council identified an aspirational goal of 3300 housing units and 6,000 jobs through a mix of commercial, office, mixed-use and residential uses that provide unique opportunities for excellent development while allowing the flexibility to respond to market conditions.

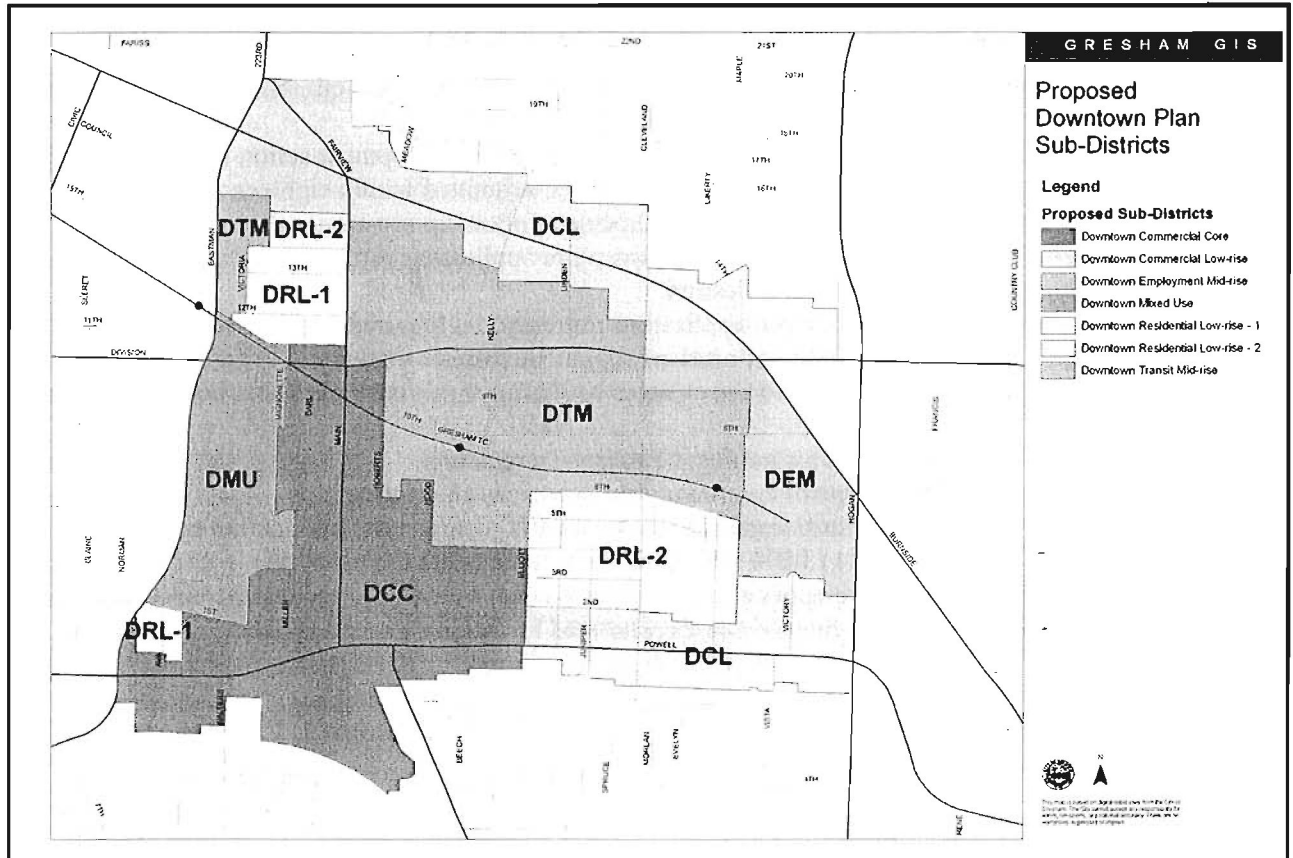


Figure 1 –FINAL DOWNTOWN PLAN

1.1 EXECUTIVE SUMMARY continued

- Develop Downtown Design Concepts: The consultant, working with City staff and the Advisory Committees, created two detailed urban design development concepts for each of the three Downtown areas. Each of the scenarios was evaluated and a consensus urban design Preferred Development Concept plan for the subject sites was created.
- Create Design Manual: The consultants, again working with City staff, the Advisory Committees and the public, assembled the Design Manual with Design Principles, Standards and Guidelines and Standards by which the Design Commission and staff can evaluate the design of proposed projects in the Downtown. This step is still in process.

- Implement Code Amendments and Legislative Process: This phase made changes to the City's Development Code that incorporated findings, goals, policies, action measures and map and code changes developed throughout the process. ~~This step is still in process.~~

1.3 PLAN AREA

2007 Plan Area.

The study area consisted of the Downtown Plan district of the Gresham Regional Center described in **Figure 2**. The area ~~is~~ was bounded by NW Division Street to the north, NE Burnside Road and NE Hogan Drive to the east, E Powell Boulevard (both sides) to the south and NW Eastman Parkway to the west. The entire area ~~contains~~ contained approximately 330 acres excluding public right of way. The MAX Light Rail Transit (LRT) line serves the district with two LRT stations.

2008 Plan Area.

The study area for the 2008 planning effort remained the same as in the 2008 study. The Downtown Plan area as shown in **Figure 2** ~~is~~ was bounded by NW Division Street to the north, NE Burnside Road and NE Hogan Drive to the east, E Powell Boulevard (both sides) to the south and NW Eastman Parkway to the west.

2009 Plan Area

The Downtown study area was expanded in 2009 to encompass additional properties north of NW Division Street up to the NE Burnside Road Corridor. The new Downtown Plan District area includes all properties between NW Eastman Parkway on the west, NE Hogan Drive on the east, both sides of NE Burnside Road to the north and both sides of E Powell Boulevard to the south.

The Downtown Plan District area was expanded to include these additional lands for the following reasons:

- METRO Regional Center Location. The additional properties are inside or adjacent to Metro's Regional Center as indicated on the Metro 2040 Growth Concept Map. Metro has defined this area as a center for commerce and local government services for the East Multnomah County region.
- Mass Transit Access. The area is currently served by the MAX and a high-quality transit system of bus and vehicle service that have a major transit hub at the Gresham Transit Center on NE 8th Street and NE Kelly Avenue.
- Growth and Redevelopment Potential. The properties that were added to the new Downtown Plan District area are characterized by underutilized, low density employment and housing developments that are generally less than three stories in height with a concentration of auto-oriented development along NE Burnside Road. An analysis of the improvement value to the value of land by parcel determined that the improvement value for many parcels was low compared with the land value. Therefore, the land is considered more likely to redevelop. Including this area into the new Downtown Plan District area increases the potential for more dense urban development.
- High Visibility and Proximity to Major Arterials. The location of many of the new Downtown properties north of NE Division Street, NE Burnside Road and NW Eastman Parkway provide prime exposure along major traffic corridors. This high visibility not

only offers economic benefits to the potential individual businesses but also provides potential marketing opportunities to the City. Currently, the Downtown core is located on Main Avenue with a minimal amount of visibility either on NW Division Street or on E Powell Boulevard. Many visitors to Gresham never know that a Downtown core shopping area exists in the City. Additional exposure along the major streets of high-quality new development can help create a very positive image for Downtown Gresham and can provide a stronger relationship to the original Downtown Core along Main Avenue.

- Establish High-Quality Urban Design. With the creation of the Design Commission, the City made a commitment to improving the quality of design in the City. The first area elected in which to establish Design Principles, Guidelines and Standards was the Downtown Plan District. The City identified a need to enlarge the Design District for the Downtown in order to more closely regulate the built environment throughout the Regional Center. The Design regulations include street, site and building Design Principles, Guidelines and Standards in categories such as the building placement along streets, walkway design, sustainable landscape design, building massing, gateway recognition, and architectural façade design. The design regulations provide the City with more control over the appearance of the future built environment and will help to create an active, vibrant, attractive community center.
- Downtown Design District Area Consistency. The new 2009 Downtown Plan District area is consistent with the larger Downtown Design District. An expanded Downtown Plan District will affect design changes for the built environment which will be more visible to the public along the major traffic corridors like NE Burnside Road and NW Division.

Section 2

Figure 2 –2007 DOWNTOWN PLAN AREA BASE MAP

Downtown Plan Process

2.2. (3) 2008 Process: Research and Analysis Documents, Including 2007 Study.

The City ~~is currently in the process of implementing~~ followed a Type IV legislative amendment to the Comprehensive Plan to replace the existing 1995 Downtown Plan Vision.

2.2. (7) 2008 Downtown Design Concepts.

Ultimately, LMN Architects, with City staff, created two different urban design concepts for each of three key development opportunity areas within the Downtown Plan area and one final Preferred Development Concept. The three key development opportunity areas were:

After significant public input, including meetings with the Downtown Technical Advisory Staff project Team, the Downtown Focus Group, the Design Commission, the Council Transportation Advisory Subcommittee, the Planning Commission, the City Council and the general public at Community Forums, the urban design elements of the two concepts ~~will be~~ were combined into a Preferred Development Concept. ~~preliminary Urban Design Plan and ultimately a Final Urban Design Plan. This work is currently in process as of the writing of this Draft Downtown Findings Document and is anticipated for completion in 2009.~~

Specific input was received from the January 7, 2009 workshop on the two draft development concepts for the three key areas Downtown. This input helped inform the Preferred Development Concept.

2009 Preferred Development Concept.

The resultant final concept for the Downtown area was titled the Preferred Development Concept. It contained the following attributes:

Gresham Town Fair

The Gresham Town Fair Mall area in the Preferred Development Concept was a mixed use residential district organized around a grand, formal village green or town square. The concept contained a signature gateway intersection at NE Division Street and NW Eastman Parkway with a large pedestrian plaza space forming a direct connection between the Civic Neighborhood and the Downtown. Other features included a prominent hotel anchor, mixed use or large scale retail development, office and townhouse development, a parking structure and a new library and plaza terminating views both in Town Fair and Third Street.

Third Street

The Third Street Corridor proposed small scale sensitive, infill development to enliven the street as a primary Downtown shopping street. Third Street featured a market and a series of smaller pedestrian friendly plazas and courtyards. The performing arts center plaza and buildings anchored the east end of the street.

Beech Street Corridor

Beech Street was a shared, “woonerf” multi-modal street which favored pedestrians but accommodated vehicles and bikes as well. The street consisted of live-work units, townhouses, residential mixed-use buildings, offices and a possible City Hall location at the MAX. The mix of uses was intended to enliven the area with activities through the day and evening.

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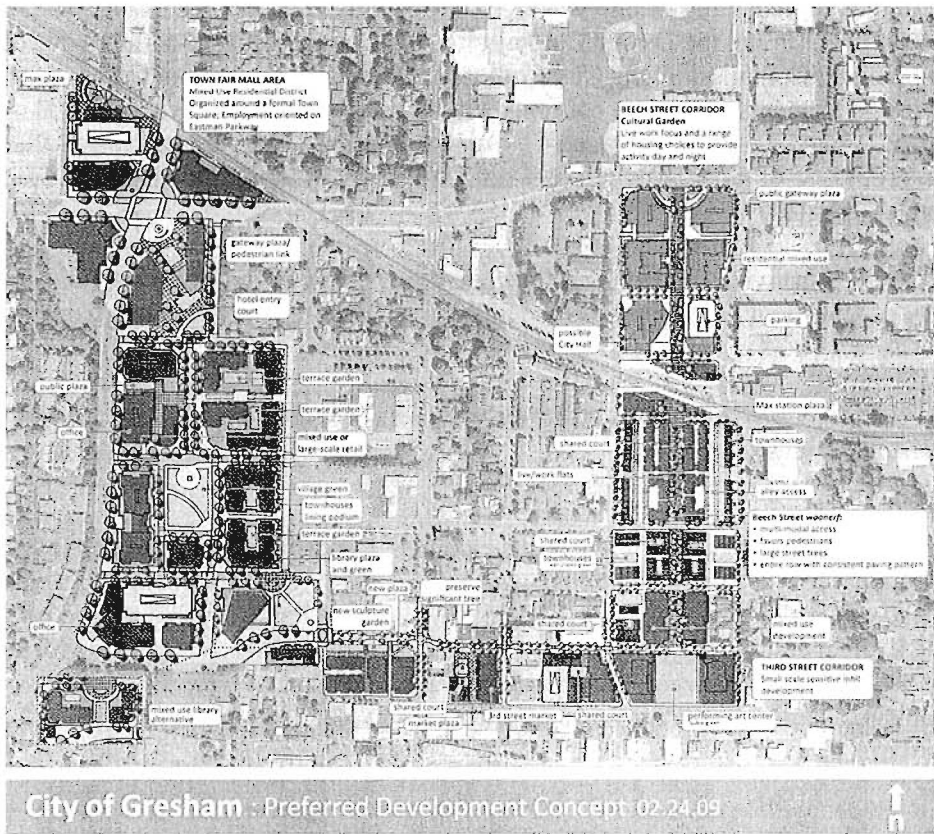


FIGURE 13 – PREFERRED DEVELOPMENT CONCEPT

The City, working with LMN Architects, the Design Commission, Advisory Committees and the public, ~~has established is currently working on assembling~~ a new Downtown Plan District and new Downtown Plan District regulations referred to as the Downtown Plan District Design Manual. The Design Manual includes Design Principles, Standards and Guidelines by which the Design Commission and staff can evaluate the design of proposed projects in the Downtown Plan District Regional Center. The Design Manual’s purpose is to encourage quality design consistent with the community’s vision for Downtown.

~~This document is in process as of the writing of this Draft Downtown Findings Document and is anticipated for completion in 2009.~~—The Downtown Plan District and Design Manual adoption schedule in 2009 is noted below:

- Planning Commission Hearing – May 11, 2009
- City Council Hearing – June 2, 2009
- City Council Enactment – June 16, 2009

2.2. (10) 2008 Code Amendments and Legislative Process.

This phase, ~~when completed, will make~~ made changes to the City's Community Development Plan that incorporated new Downtown Findings, Goals, Policies and Action Measures in Volumes 1 and 2 and also amended Volume 3 to include a new Downtown Plan District and corresponding regulations. ~~Code that incorporates findings, goals, policies, action measures and map and code changes developed throughout the process.~~

Planning Commission

The Planning Commission review process is noted below: ~~thus far was as follows:~~

- June 23, 2008 Downtown Plan Overview, Issues & Opportunities, and 3 Land Use Concepts
- August 11, 2008 Draft Land Use Framework Plan
- October 13, 2008 Consultant Selection for Urban Design/ Downtown Plan
- November 24, 2008 Downtown Goals, Policies & Action Measures
- December 22, 2008 Development Concepts Review
- February 23, 2009 Downtown Plan & Design Manual Review
- March 9, 2009 Downtown Plan & Design Manual Review
- April 27, 2009 Downtown Plan & Design Manual Review
- May 11, 2009 Downtown Plan & Design Manual Hearing

Planning Commission Future Steps:

~~On February 9, 2009, the Planning Commission is scheduled to review and make a Type IV decision on the Downtown Plan Findings, Goals, Policies and Action Measures. Adoption of the Downtown Land Use Plan and Volume III Gresham Community Development Code Amendments will occur as a future step.~~

City Council

The City Council input process is noted below: ~~thus far was as follows:~~

- August 19, 2008 Initiate 2008 RCPI Downtown Plan Update
- September 2, 2008 Draft Land Use Framework Plan
- October 7, 2008 Consultant Selection for Urban Design/ Downtown Plan
- November 18, 2008 Downtown Plan Review
- December 16, 2008 Downtown Goals, Policies & Action Measures Review
- March 3, 2009 Downtown Goals, Policies & Action Measures Type IV Hearing

- March 10, 2009 Downtown Plan & Design Manual Review
- April 7, 2009 Downtown Goals, Policies & Action Measures Enactment
- June 2, 2009 Downtown Plan District & Design Manual Type IV Hearing
- June 16, 2009 Downtown Plan District & Design Manual Enactment

Future Steps:

~~On January 20, 2008, the City Council is scheduled to review the Draft Downtown Design Manual. On March 3, 2009, the City Council is scheduled to hold a Type IV Hearing on the Downtown Plan Findings, Goals, Policies & Action Measures.~~

Section 3

Public Involvement

3.2 KEY METHODS

3.2. (2) 2008 and 2009 Regional Center Planning Implementation (RCPI or the Downtown Plan).

Likewise for the 2008 and 2009 Downtown Planning effort, significant efforts were made to engage the Gresham residents, elected and appointed officials, interested parties and the general public. The Downtown staff project team developed a public involvement and outreach plan that included the following elements:

- Creation of two Downtown project advisory groups including an internal Downtown Technical Advisory Staff (TAS) formed to evaluate the technical components of the Downtown Plan and a Downtown Focus Group formed to garner Downtown Plan input from the GDDA, the development community, real estate professionals, the neighborhood associations, representatives from the Downtown Civic Neighborhood Architectural Review Subcommittee and the Planning Commission. Numerous meeting have been held with the TAS. ~~Four~~Five meetings have been held with the Downtown Focus Group as well as four meetings with the Development Group.

- Scheduling of ~~five~~three Community Forums to date used to gain input regarding preferred development patterns, issues to address, and ideas to consider on April 10, 2008, June 4, 2008 and August 6, 2008, as well as on January 7, 2009 and March 4, 2009;

- Involving the newly formed Design Commission in the Downtown Plan update;

October 16, 2008	Downtown Plan area walking tour with LMN Architects
November 6, 2008	Downtown Plan Process and Draft Land Use Framework
November 20, 2008	Downtown Goals and Policies and Design Principles
December 4, 2008	Downtown Urban Planning Concepts
<u>January 15, 2009</u>	<u>Downtown Street Typologies</u>
<u>January 29, 2009</u>	<u>Downtown Plan & Design Manual</u>
<u>February 26, 2009</u>	<u>Downtown Plan & Design Manual</u>
<u>March 5, 2009</u>	<u>Downtown Plan & Design Manual</u>
<u>March 19, 2009</u>	<u>Downtown Plan & Design Manual</u>
<u>April 2, 2009</u>	<u>Downtown Plan & Design Manual</u>

3.3 KEY FINDINGS

3.3. (1) 2007 Findings.

For the 2008 and 2009 Downtown Plan project, however, one of the key purposes for the additional project was to gather community input and support. A series of community forums were held to maximize the community's voice.

3.3. (5) 2009 Community Forum 4 on January 7, 2009.

At this forum held on January 7, 2009, the City presented its draft development concepts for three key areas Downtown: Town Fair shopping center, Third Street and Beech Street. The concepts provided a variety of ideas about how Downtown could develop in the future, suggesting such things as land uses, building heights, and park and plaza locations. The workshop was designed to elicit input that would influence the Downtown Plan and the Design Manual.

A summary of the commentary was as follows:

January 7, 2009 Input – Summary

Participants at the small-group discussions were presented with the following questions:

1. What parts of the Land Use Framework concept do you like? Why?
2. What do you like about the Scale Integration Concept? Why?
3. What opportunities did we miss in these two concepts? Any issues/concerns not addressed in the concepts?
4. If the City could do ONE thing to make Downtown a better place, what would it be?

The input summary for each concept was as described below.

Land Use Framework Concept

Town Fair: The hotel and conference center configuration with the diagonal pedestrian access was preferred. Office/employment uses along Eastman Parkway were favored because the diagonal orientation of the pedestrian/plaza spaces provided a direct walking route and good site lines to the Downtown core. Office uses in the Town Fair area were well received.

Beech: A new City Hall near the MAX station was well received. Participants favored a mix of land uses around the transit center. A special shared street was preferred. A new City Hall south of the Transit Center was favored. Arts uses across from the Center for the Arts would be welcome, but arts uses should be found throughout the Downtown.

Third: Terminating the view west down Third Street (possibly with a new library) was favored.

Scaled Integration Concept

Town Fair: The mix of office, retail, and residential uses in Town Fair was well received. The large town square was favored. A library and the public plaza as a bridge to the core and Third Street also was favored.

Third Street: The re-use of the former Hicks department store building (at Third and Roberts) as a market was favored.

Beech: The lower-scale townhomes and live-work units were favored for part of the intimate scale, “woonerf” (shared street). An office campus with a gateway open space at the north intersection of Beech and Division was preferred.

The document “Downtown Plan & Design Manual Public Input Summary-Development Concepts” (footnote 12) provides additional forum summary information.

3.3. (6) 2009 Community Forum 5 on March 4, 2009.

This community forum held on March 4, 2009 presented the 2nd draft of the Downtown Plan District Design Manual which is to be used to regulate the built environment in the Downtown. The Manual draft had written descriptions of land use districts, permitted uses, heights and intensities of development, as well as illustrations that were intended to help explain the regulations. The meeting’s purpose was to secure community input, ideas and comments on the most recent draft of the Downtown Plan District Design Manual for incorporation into the 3rd draft. The forum included an exercise in the use of the Manual to evaluate potential development proposals. The Manual was well received as a positive step forward in regulating the future built environment for the Downtown.

3.4. GOALS AND POLICIES ADOPTED.

- **Vision:** Downtown will be the recognized center of Gresham, and will include most significant civic and governmental functions, including public parks and the Center for the Arts. It will include large numbers of professional sector jobs, medium and high density residential development and a thriving and unique entertainment, nightlife and shopping district.

Section 4

Downtown Plan

4.1 INTRODUCTION

This section of the report is a place holder to summarize the Downtown Plan. The Plan provides recommendations regarding the Downtown plan area.

This summary consists of the following elements:

- Land Use
- Economic Development
- Natural Resources
- Transportation
- Sustainable Development
- Public Facilities (Water, Wastewater, Stormwater)
- Parks

Insert new Final Downtown Plan and Summary

Figure XXX final downtown land use plan

The Downtown Plan District/Downtown Design Manual (Code Standards) project establishes a new vision for a vibrant Downtown Gresham that envisions Downtown as an active, mixed-use, pedestrian oriented center of the community where people can live, work, shop and play in an exceptional, sustainable environment. The Downtown Plan has an aspiration for approximately 3,300 housing units and 6,000 jobs through a mix of commercial, office, mixed-use and residential uses that provide unique opportunities for high-quality development while allowing the flexibility to respond to market conditions.

The Downtown Plan reflects the current community aspirations and needs; sets the stage for the redevelopment of Downtown; establishes greater physical and visual connectivity with the Civic Neighborhood as a Regional Center; and promotes a strong public-private sector partnership to ensure future investment and realization of the Downtown vision.

4.2 DOWNTOWN PLAN LAND USE SUB-DISTRICTS

The new Downtown Plan District has established seven Sub-Districts. Development within each Sub-District is governed by a different combination of basic regulations like uses, height limits, allowable floor area ratios and densities. See Figure 1 –FINAL DOWNTOWN PLAN. The land use Sub-Districts are as follows:

4.2. (1) Downtown Commercial Core (DCC)

The DCC is the City's long-standing center and features unique local businesses, small-scale storefronts, and intimate sidewalks. Main Avenue has a small-scale, walkable quality appreciated by residents and visitors. This Sub-District is intended to preserve this small-scale character on Main Avenue while encouraging an active, engaging mix of old and new uses.

The DCC Sub-District allows a wide range of uses – retail, service, office and residential – that will help create a vibrant Sub-District that is active all day and much of the night.

4.2. (2) Downtown Mixed Use (DMU)

This Sub-District is intended to evolve over time from a shopping center largely organized around automobile trips and parking into a mixed-use Sub-District with jobs, housing and commercial opportunities. This full-service Sub-District will contain new shopping streets, public spaces and better pedestrian and automobile connections to the DCC Sub-District and to the Civic Neighborhood.

This DMU Sub-District allows a mixture of employment, retail, office and residential uses in a very dense, compact urban form by permitting the most intense, tallest development in Downtown.

4.2. (3) Downtown Transit Mid-Rise (DTM)

This mixed-use Sub-District provides a mid-rise, mixed-use character in the center of Downtown near the light-rail stations. Because of its proximity to transit, this Sub-District provides access opportunities for those who live Downtown to use buses and the MAX light rail to get to jobs and other destinations. It also supports the creation of employment uses Downtown so those who live outside the Downtown have opportunities and easy access to work Downtown.

This Sub-District supports the continued presence of institutional uses, such as government offices. It also allows a mix of residential, commercial and employment uses at a mid-rise intensity.

4.2. (4) Downtown Employment Mid-Rise (DEM)

This mixed-use area is envisioned as an area that could support significant employment, whether retail or office in nature. It has excellent access to light rail as well as several major streets – Hogan, Burnside, and Division. Buildings are allowed to have multiple stories with larger footprints here to accommodate market demand.

This Sub-District allows for a substantial amount of general office, financial, corporate and institutional uses that employ large numbers of people. It also allows a significant retail presence and residential uses.

4.2. (5) Downtown Residential Low-Rise-1 (DRL-1)

This mixed-use Sub-District will encourage some residential areas to gently transform into a broader mix of residential uses. This Sub-District is intended to create distinctive, walkable neighborhoods within a short distance of transit and the Downtown core.

The Sub-District encourages single-family homes to remain and allows duplexes and townhomes but not attached dwellings on a single lot. This Sub-District also will allow small-scale commercial uses only on certain streets where it is most appropriate.

4.2. (6) Downtown Residential Low-Rise-2 (DRL-2)

This mixed-use Sub-District will allow a gradual transformation into more varied and full-service residential neighborhoods that can take advantage of their proximity to transit and nearby shopping and job centers.

This predominantly residential Sub-District will allow single-family homes to remain while also allowing attached single-family houses, small-scale apartments and condominiums, and small-scale commercial activities.

4.2. (7) Downtown Commercial Low-Rise (DCL)

This Sub-District contains major corridors with the types of businesses, services, stores, and offices that demand a higher level of automobile access for employees and customers. Structures may be single use and aimed at regional traffic. This Sub-District will still serve this role, but the corridors will become more balanced over time to meet the needs of pedestrians as well as automobile traffic. The Sub-District's character will evolve as buildings and more walkable streets become prominent and parking is located to the side or rear of properties. This Sub-District allows commercial, residential, and employment uses, including auto-related uses such as service stations, auto repair, and car washes.

Generally the new Downtown Plan Land Use Sub-Districts provide more flexibility in permitted land uses, including permitting less intense residential development in some areas, and allowing small-scale manufacturing (as an accessory to other commercial uses), information services, and live-work uses. Some areas also were changed from primarily commercial districts to allowing a range of office, retail, service, and housing. The changes provide additional limits on auto-dependent uses, drive-throughs, and outdoor storage.

The Sub-Districts also include a new approach for areas where commercial uses are required on the ground floor. The current approach is to require first-floor commercial in the existing Central Urban Core, Downtown Commercial-1 and Downtown Commercial-2 Sub-Districts. The proposed approach limits the commercial requirement to certain key shopping streets: Main, Stanley (a new street envisioned as Town Fair redevelops), Third and part of Powell. All-residential buildings (condominiums and apartments with no first-floor commercial) will be allowed in those districts, except on those key shopping streets. The new Sub-Districts permit additional building height in most of Downtown.

4.3 DOWNTOWN PLAN DISTRICT DESIGN MANUAL

The Downtown Plan District Design Manual is the regulatory framework that provides specific urban design strategies and recommendations to ensure that the City's physical environment – uses, buildings, streetscapes, pedestrian environments and open spaces – matches the City's vision for Downtown.

The Downtown Plan District Design Manual establishes:

- A two-alternative Design Review Process with a discretionary process and a clear and objective process.
- Design Principles that are the general, over-arching statements and considerations that guide the design of the built environment.

- Downtown Sub-districts and Development Standards that prescribe the basic building envelope, permitted uses, building heights, floor area ratios, densities, setbacks and other regulations.
- Street Type Standards that provide direction concerning building locations and relationships to adjacent streets, multi-modal circulation, and provision of public spaces and pedestrian amenities.
- Guideline and Standards that provide the means for a development to show consistency with the Design Principles. Guidelines are the basis for the discretionary process and Standards for the clear and objective process.

4.3. (1) Two Track Process

The Downtown Plan District has been established as a Design District in which new developments are to be regulated primarily by the Design Commission. Under the two-track process, an applicant may choose either a discretionary process governed by Guidelines or a clear and objective process governed by Standards. The Design Commission is the review and decision body for all the larger developments following the Standards track and any applications following the discretionary Guidelines track. The purpose of the two-track system is to permit greater flexibility, creativity and excellence in architectural and site design.

4.3. (2) Downtown Design Principles

The Design Principles are the general, over-arching statements and considerations that guide the design of the built environment in design districts like Downtown. The Guidelines and Standards are written to support and carry out the Principles on a project-specific level. In instances where the applicant chooses the discretionary process, the relevant principles will be reviewed for compliance during the decision making process.

Design Principles

A. Offer a Vibrant Mix of Uses and a Variety of Housing Types.

Mixing uses and a variety of housing types shall be developed to support a more diverse, vibrant, 18-hour Downtown for a broad range of ages and backgrounds. Mixed-use development may be either horizontal or vertical, depending on the scale and intensity appropriate for a specific sub-area.

B. Promote Excellence in Design and Architectural Expression.

Each site, building and streetscape improvement must be treated as a long-term addition to Downtown. Exterior design and building materials shall exhibit both the permanence and quality appropriate to an urban district setting. Great visual interest and innovative design are critical elements of this Principle with buildings that assist in defining, enlivening the public realm and accentuating the main Gateways into the Downtown.

C. Create a Unique and Exciting Public Realm.

Emphasize building and site design elements that reinforce the experience of walking and biking, and promote active streets and lively public spaces. This helps achieve the Downtown Plan area goal to create a cohesive, linked system of animated public and private open spaces, parks, plazas and pedestrian pathways.

D. Create strong connections between Sub-Districts and Plan Districts.

Each development shall contribute aspects of City -wide connectivity, whether through big moves like through-block connections, or small surprises like distinct lighting, to sustain attention and lead the eye down the street. The Downtown Sub-Districts shall have a strong connection to Civic Neighborhood.

E. Incorporate Sustainability.

A project's design approach to infrastructure and site development shall reflect a commitment to sustainable development that contributes to a healthier and greener community.

F. Provide Context Sensitivity.

The Downtown Plan District is composed of several sub-areas, each with their own unique characteristics and potential to enhance district identity. Context-sensitive redevelopment must take into account proximity to existing uses, height and massing relationships, surrounding building character, street widths and functions, open spaces, desired land uses and view corridors.

G. Preserve and Enhance Historic Character.

Where appropriate, new developments shall build on historic and other cultural assets by recognizing the smaller patterns of the townscape without being historicist.

H. Create Appropriate Transitions in Height, Bulk and Scale.

New development must strive to be compatible in scale with its surroundings. Elements which contribute to the perceived scale of new construction are addressed in the context of specific site conditions, as well as in relationships between Sub-Districts.

I. Increase Transportation Accessibility.

Building, site and street design shall increase accessibility to and within the Downtown Plan area by encouraging transportation by many modes including pedestrians, automobiles, bicycles, transit and other vehicles in a functional and visually attractive manner.

4.3. (3) Street Types

The Downtown Plan District process also established a set of Street Types specific to Downtown. The respective Street Type Standards provide important direction concerning building locations and relationships to adjacent streets, efficient multi-modal circulation, and the provision of public spaces and pedestrian amenities. For example, Main Avenue, Third Street and a future Stanley Street have been identified as unique streets with a commercial emphasis and therefore commercial is required on the first floor on these streets. The Street Types are indicated in Figure 14 below.

Section 10. Gresham Community Development Plan, Volume 2, Policies, is amended as follows:

10.314 DOWNTOWN PLAN DISTRICT

~~This is the first set of goals, policies and action measures. A second set will be added when the final plan is adopted next year. These may refine or change the first set as more knowledge and insight is gained about Downtown's potential to be a special place for the community.~~

The Downtown Plan District contains approximately 330 acres. It is bounded by N.W. Division Street to the north, N.E. Burnside Road and N.E. Hogan Drive to the east, Powell Boulevard (both sides) to the south and N.W. Eastman Parkway to the west. The MAX light rail system serves the area with two stations.

Background

The Downtown Plan District contains approximately 550 acres. It includes additional properties north of NW Division Street up to the NE Burnside Road Corridor. The new Downtown Plan District area includes all properties between NW Eastman Parkway on the west, NE Hogan Drive on the east, both sides of NE Burnside Road to the north and both sides of E Powell Boulevard to the south.

2008 Downtown Plan Project

The 2008 Downtown Plan project picked up where the 2007 DRCDS project left off to create a more detailed vision of Downtown's future built environment and public realm. It will consist of a new Land Use Framework Plan and a closer look at urban design. This work will then provide the basis for the Development Code/Comprehensive Plan amendments that will replace the 1995 Plan.

The Land Use Framework Plan provided a land use vision for the future of Downtown Gresham, showing land use sub-areas with various desired characters, improved transportation connections, and potential locations of parks/plazas.

The Land Use Framework Plan included the following major elements:

The 2008 Downtown Plan project also included creating development concepts for specific opportunity areas and producing a Design Manual. ~~This work will extend into 2009.~~ These are described as follows:

- The development concepts ~~will~~ graphically explored different land uses (where appropriate), the scale/massing of buildings and their relationship to the public realm of streets, sidewalks, parks and plazas. Studies of transitions between districts and connections among sub-areas also will be important. This more detailed part of the process ~~will be~~ was used to refine the Land Use Framework and ~~will to~~ provide key information for developing code changes for Downtown, including design requirements.
- The Design Manual ~~will have~~ has Design Principles, Design Guidelines and Design Standards that the newly formed Gresham Design Commission will apply to new development and streetscapes within specific Downtown areas.
 - The design principles ~~will be~~ are general statements that will guide the design of the built environment in Downtown.
 - The design guidelines ~~will be~~ are design parameters for development in design districts that support the design principles. They ~~will be~~ are discretionary in nature with a statement of intent. The design guidelines are intended to provide opportunity for creative designs. They would be for applicants who propose a project that does not meet the design standards but is still of high design quality.
 - The design standards ~~will be~~ are a set of objective requirements for development that will be very specific. They ~~will~~ provide a “clear and objective” way to evaluate a development proposal.

Summary of Major Issues

The following are some of the major issues that have been identified for Downtown during the 2007, and 2008 and 2009 projects:

The following Code amendments are not related to the Downtown Plan District but are changes subsequent to the Development Code Improvement Project.

Section 11. Gresham Community Development Plan, Volume 3, Code, Article II, Procedures, is amended as follows:

2.0001 Title and Code Designation

This ordinance shall be known as the Gresham Community Development Code, and shall be referred to herein as “Community Development Code”. All ordinances included in this and the following articles are Volume 3, Code, of the Gresham Community Development Plan and are designated the “Gresham Community Development Code” and will be referred to as “code” or “Community Development Code.” When referring to specific sections of the Community Development Code, the letters “GCDC” shall precede the numerical designation.

2.0002 Publication

In preparing revisions of the code for publication and distribution, the manager shall not alter the sense, meaning, effect, or substance of the code or an ordinance. With regard to the code and ordinances the manager may renumber or re-letter sections and parts of sections, change the wording of catch lines, rearrange sections, change reference numbers to agree with renumbered articles, chapters, sections, or other parts, substitute the proper subsection, section, chapter, article or other division numbers, strike out figures or words that are merely repetitious, change capitalization for the purpose of uniformity, and correct clerical or typographical errors.

2.00023 Purpose

2.00034 Scope and Compliance

2.00045 Conditions

Section 12. Gresham Community Development Plan, Volume 3, Code, Appendix 2.000, is amended to add the "Downtown Plan District Design Manual," which is attached as Exhibit 'A' to this council bill.

Section 13. Gresham Community Development Plan, Volume 2, Policies, Appendix C, Community Development Plan map, is amended as follows:

Gresham Community Development Plan, Volume 2, Policies, Appendix C, Community Development Plan Map, is amended to reflect changes to the Downtown Plan District as shown in Exhibit 'B' which is attached to this council bill.

First reading: June 2, 2009

Second reading and passed: June 16, 2009

Yes: Strathern, Widmark, Craddick, Warr-King, Nielsen-Hood

No: _____

Absent: Remis

Abstain: _____

City Manager

Mayor

Approved as to Form:

Senior Assistant City Attorney

Exhibit A

DOWNTOWN PLAN DISTRICT Design Manual

Council Hearing Draft
May 14, 2009

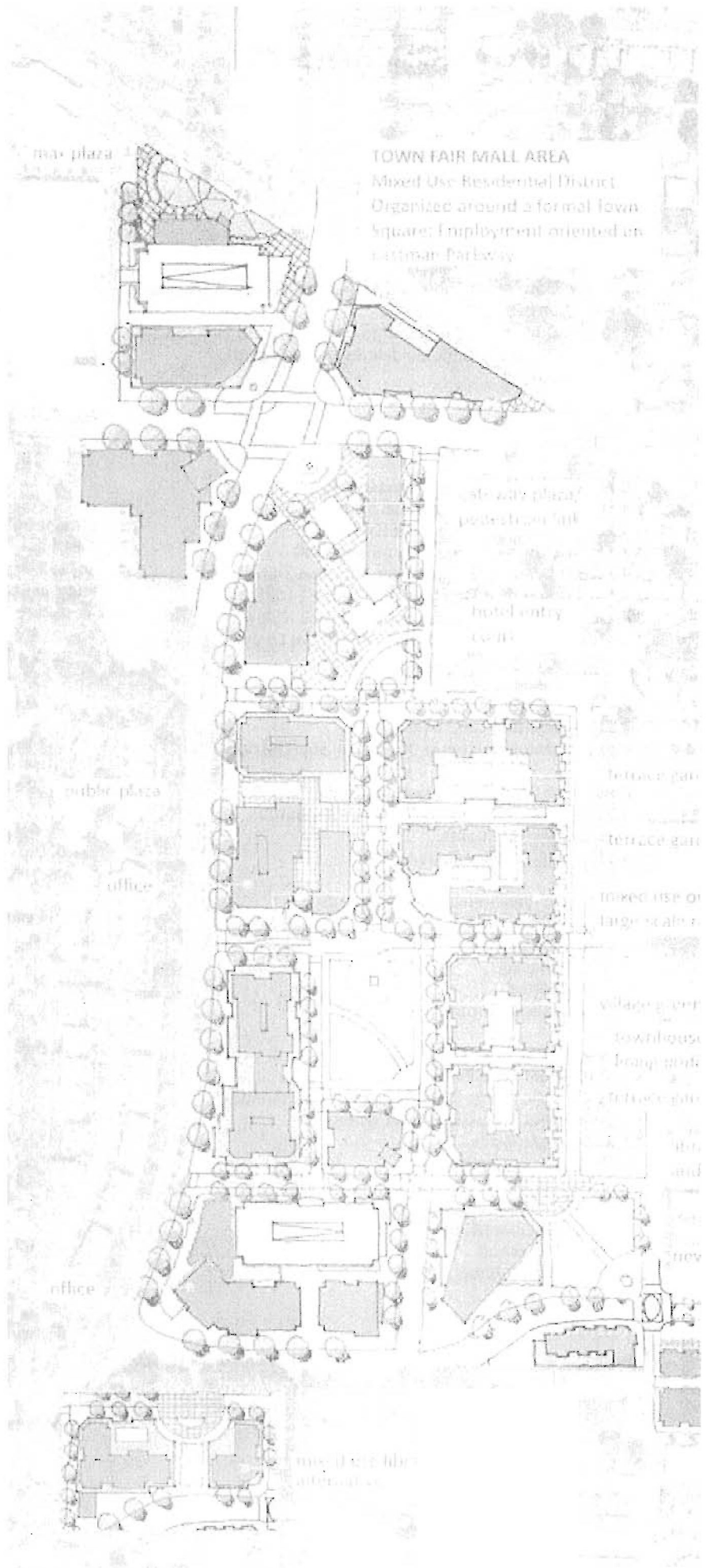


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 - 6. Pedestrian Circulation
- B. Building Design
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3.0000 Definitions

General

4.1101 Purpose

The 2009 City of Gresham Downtown vision embodies a vision of a Downtown that is one of the region's great urban settings - a lively, diverse and appealing place to live, work, shop and play as the basis for a truly sustainable City. This Development Code will help implement that vision, providing specific urban design strategies and recommendations that raise the bar for design excellence in the built environment. Downtown Public and private entities working together will make this vision for Downtown Gresham a reality. This document aims to inspire and guide development proposals towards meeting this goal.

The Development Code contains Design Guidelines and Standards that provide a framework within which to review projects in Downtown, aiding architects, designers, developers and the community in understanding the City's urban design expectations. City staff and the City's Design Commission are responsible for reviewing the design of new and significantly modified buildings Downtown, evaluating their contribution to the public's enjoyment of the building and the immediate vicinity.

The Design Guidelines and Standards contained in the Development Code provide the measures that the Design Commission and staff use to determine a proposal's success. Where there is a conflict between the provisions of the Downtown Plan District and those of other provisions of the Community Development Code, the Downtown Plan District's provisions control.

A. Design Review Process

New development and additions and remodels (as noted in Article 3.0000 Definitions of Design Districts and Article XI) within the Downtown Sub-Districts are subject to design review for determination of consistency with the guidelines and/or standards contained in this Code. Projects subject to design review are either brought before the Design Commission or administered by City staff. Either the Commission or staff shall make findings and decisions concerning conformance with the design standards or guidelines, based on which review process is selected.

1. Two Alternative Processes

The City has set up two alternative Design Review processes:

- The Discretionary Process; and
- The Clear and Objective Process.

Applicants have the choice of complying with either option. If the Clear and Objective Process is chosen, the applicant must meet all development standards. Deviation from any of the standards in 4.1100 or referenced standards in 7.0200 will place the application in the Discretionary Process.

- a. **Discretionary Process.** The Discretionary Process is intended for applicants that would prefer to propose a quality project that might not comply with all or some of the standards in 4.1100 or referenced standards in 7.0200. The aim is to encourage applicants to propose exciting, innovative designs, while still ensuring the City's design concerns and objectives for Downtown development are met. The Design Commission may waive a Guideline to achieve the flexibility necessary to support a particularly creative proposal. Approval requires the applicant to demonstrate to the Design Commission that the waiver from the Guideline would result in a development that better meets the applicable Downtown Design Principles and the intent statement preceding the Guidelines.

TYPE III DESIGN COMMISSION REVIEW PROCESS

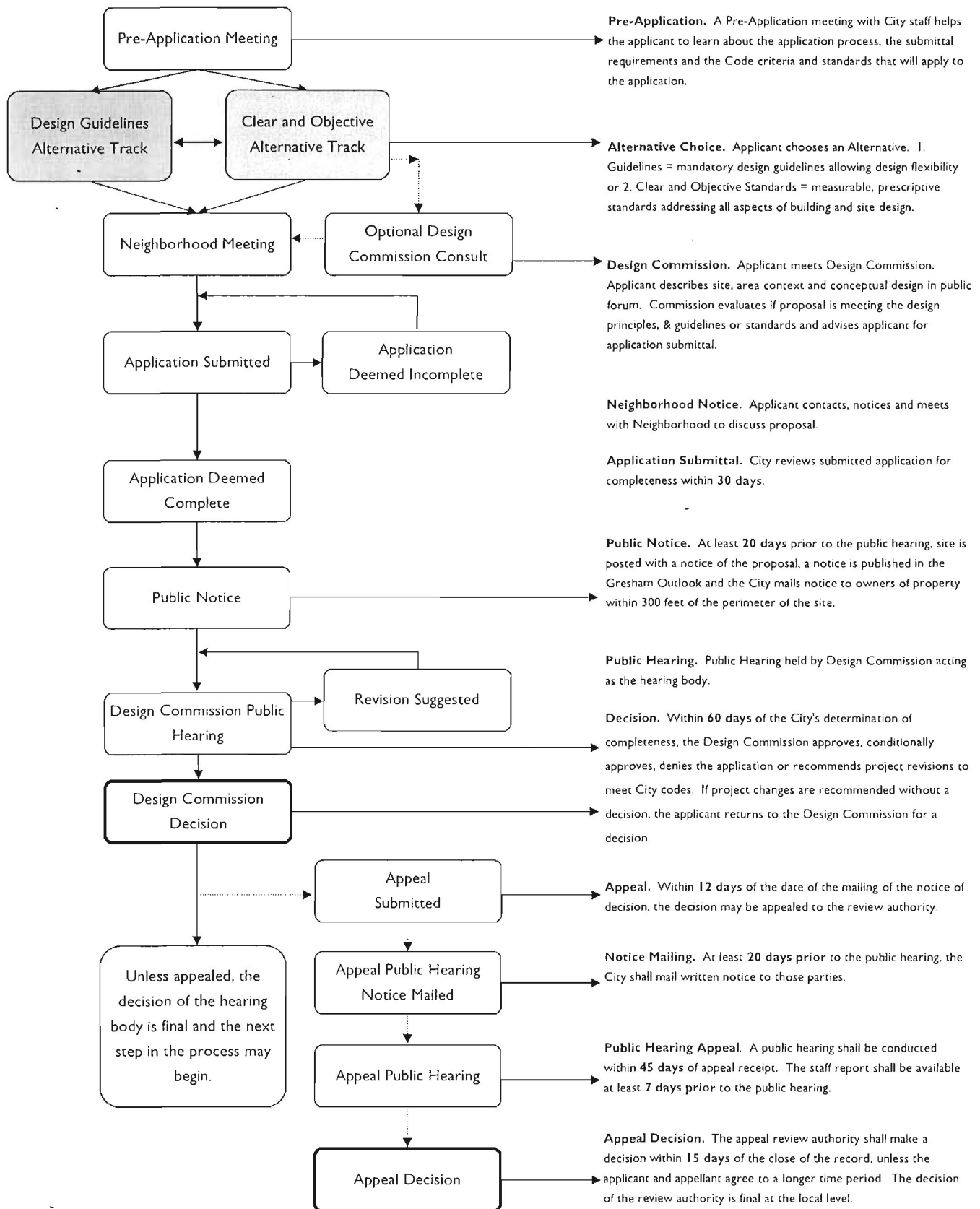


Figure: Design Commission process

The Design Standards are the objective requirements for development in design districts that are based on Design Principles. Design Standards provide a clear and objective way of evaluating the acceptability of a project's design.

a. Site Design

Site Design Guidelines and Standards primarily address the organization and arrangement of a project's components in two dimensions. They deal with the location of buildings and site features such as open space, landscaping, parking and service areas.

Good site planning can minimize a project's impacts on its neighbors, improve the quality of the streetscape, relate to or establish desirable development patterns, promote sustainability and make better connections.

b. Building Design

Guidelines and Standards in this section deal with the massing and exterior architectural elements of buildings – components which define the scale, quality and character of a building, such as roofs, entries, windows, materials and details.

Excellent building design contributes to improving the quality of life for residents by enhancing the appearance of the City, by establishing a sense of community pride and by improving the long-term economic value of the property.

5. District-Specific Guidelines and Standards

After consulting the Street Type Standards and Downtown-Wide Guidelines and Standards, an applicant must also comply with an additional set of Standards and/or Guidelines for a particular Downtown District. The District-Specific Guidelines and Standards recognize unique characteristics – either existing or envisioned by land use and planning studies – and help reinforce the desirable qualities and characteristics of the built environment within a particular land use district. The District-Specific Guidelines and Standards are intended to augment the Design Review process. They shall be used in conjunction with the Downtown-Wide Guidelines and Standards and the Street Type Standards, and those three sections are the basis for project review by the Design Commission or by staff within a particular district. Therefore, they are numbered and organized to correspond with the Downtown-Wide set of Guidelines and Standards.

In cases where there is conflicting guidance, the Sub-District-Specific Guidelines and Standard shall supersede the Downtown-Wide Guidelines and Standards.

6. Images

Most images, including photographs, illustrations, and maps, included in the Downtown Plan District Design Manual (4.1100) are not part of the Development Code and do not act as guidelines or standards. Images that are not part of the Development Code are labeled as figures. Images that are part of the Development Code will be labeled with a Development Code section number.

Approval Criteria and Standards

4.1102 Downtown Design Principles

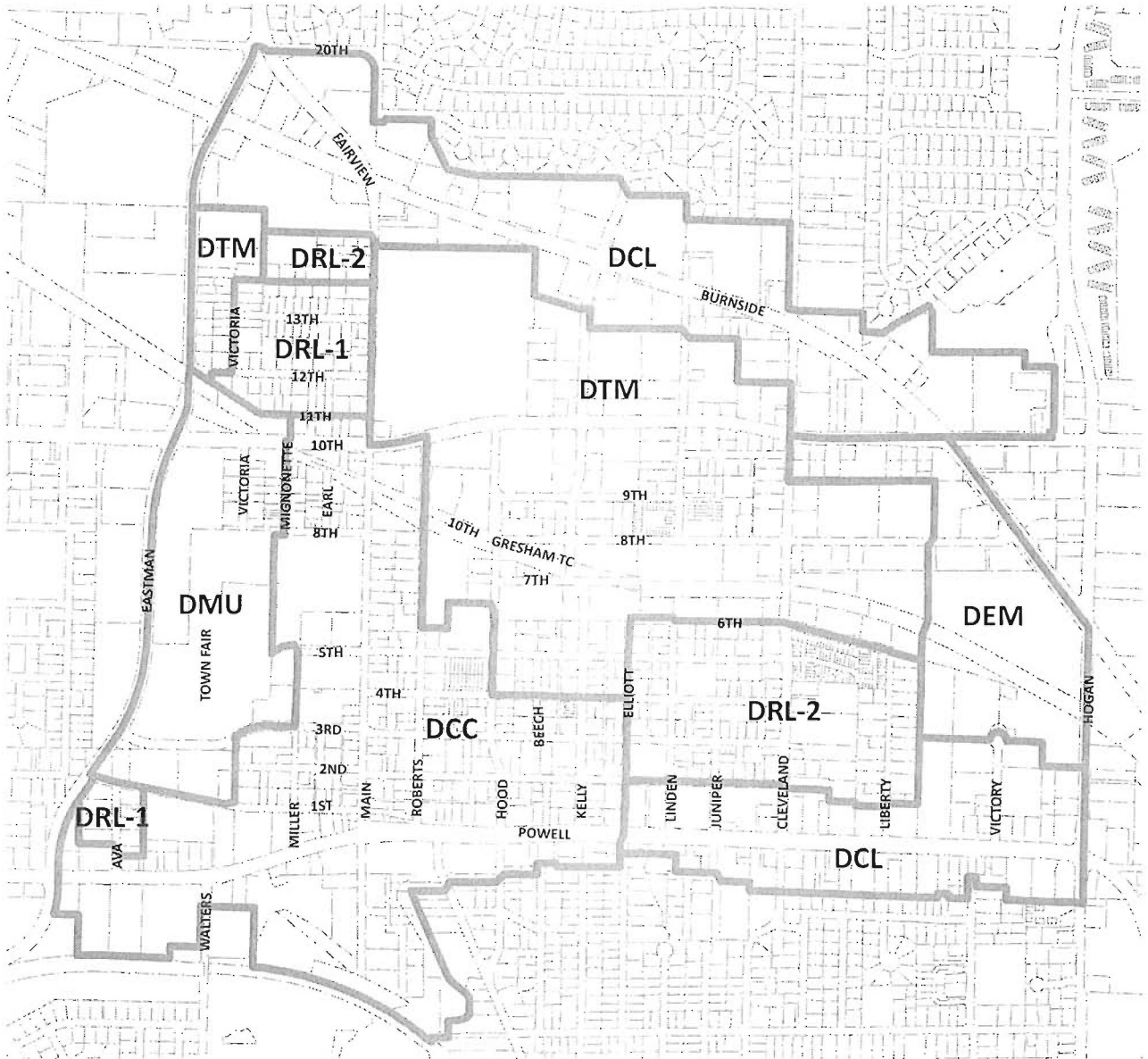
- A. **Offer a Vibrant Mix of Uses and a Variety of Housing Types.** A mix of uses and a variety of housing types shall be developed to support a more diverse, vibrant, 18-hour Downtown for a broad range of ages and backgrounds. Mixed-use development may be either horizontal or vertical, depending on the scale and intensity appropriate for a specific sub-area.
- B. **Promote Excellence in Design and Architectural Expression.** Each site, building and streetscape improvement must be treated as a long-term addition to Downtown. Exterior design and building materials shall exhibit both the permanence and quality appropriate to an urban district setting. Great visual interest and innovative design are critical elements of this principle, with buildings that assist in defining, enlivening the public realm and accentuating the main Gateways into the Downtown.
- C. **Create a Unique and Exciting Public Realm.** Emphasize building and site design elements that reinforce the experience of walking and biking, and promote active streets and lively public spaces. This helps achieve the Downtown Plan-area goal to create a cohesive, linked system of animated public and private open spaces, parks, plazas and pedestrian pathways.
- D. **Create strong connections between Sub-Districts and Plan Districts.** Each development shall contribute aspects of City-wide connectivity, whether through big moves like through-block connections, or small surprises like distinct lighting, to sustain attention and lead the eye down the street. The Downtown Sub-Districts shall have a strong connection to Civic Neighborhood.
- E. **Incorporate Sustainability.** A project's design approach to infrastructure and site development shall reflect a commitment to sustainable development that contributes to a healthier and greener community.
- F. **Provide Context Sensitivity.** The Downtown Plan District is composed of several sub-areas, each with their own unique characteristics and potential to enhance district identity. Context-sensitive redevelopment must take into account proximity to existing uses, height and massing relationships, surrounding building character, street widths and functions, open spaces, desired land uses and view corridors.
- G. **Preserve and Enhance Historic Character.** Where appropriate, new developments shall build on historic and other cultural assets by recognizing the smaller patterns of the townscape without being historicist.
- H. **Create Appropriate Transitions in Height, Bulk and Scale.** New development must strive to be compatible in scale with its surroundings. Elements which contribute to the perceived scale of new construction are addressed in the context of specific site conditions, as well as in relationships between Sub-Districts.
- I. **Increase Transportation Accessibility.** Building, site and street design shall increase accessibility to and within the Downtown Plan area by encouraging transportation by many modes including pedestrians, automobiles, bicycles, transit and other vehicles in a functional and visually attractive manner.

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Downtown Sub-Districts Characteristics

Development within each Sub-District is governed by a different combination of basic regulations such as uses, height limits, allowable floor area ratios and densities. In addition to Guidelines and Standards applicable to the Downtown as a whole, development within several Sub-Districts, such as the Downtown Commercial Core (DCC), the Downtown Mixed Use (DMU), the Downtown Transit Mid-Rise (DTM) and the Downtown Residential Low-Rise (DRL-1 and DRL-2), are subject to additional Guidelines and Standards specific to the given Sub-District.

Map 4.1109: Downtown Sub-Districts



4.1110 Downtown Commercial Core (DCC)

The DCC is the City's long-standing center and features unique local businesses, small-scale storefronts, and intimate sidewalks. Main Avenue has a small-scale, walkable quality appreciated by residents and visitors. This Sub-District is intended to preserve this small-scale character on Main Avenue while encouraging an active, engaging mix of old and new uses.

The DCC Sub-District allows a wide range of uses – retail, service, office and residential – that will help create a vibrant Sub-District that is active all day and much of the night.

4.1111 Downtown Mixed Use (DMU)

This Sub-District is intended to evolve over time from a shopping center largely organized around automobile trips and parking into a mixed-use Sub-District with jobs, housing and commercial opportunities. This full-service Sub-District will contain new shopping streets, public spaces and better pedestrian and automobile connections to the DCC Sub-District and to the Civic Neighborhood.

This DMU Sub-District allows a mixture of employment, retail, office and residential uses in a very dense, compact urban form by permitting the most intense, tallest development in Downtown.

4.1112 Downtown Transit Mid-Rise (DTM)

This mixed-use Sub-District provides a mid-rise, mixed-use character in the center of Downtown near the light-rail stations. Because of its proximity to transit, this Sub-District provides access opportunities for those who live Downtown to use buses and MAX light rail to get to jobs and other destinations. It also supports the creation of employment uses Downtown so those who live outside the Downtown have opportunities and easy access to work Downtown.

This Sub-District supports the continued presence of institutional uses, such as government offices. It also allows a mix of residential, commercial and employment uses at a mid-rise intensity.

4.1113 Downtown Employment Mid-Rise (DEM)

This mixed-use area is envisioned as one that could support significant employment, whether retail or office in nature. It has excellent access to light rail as well as several major streets – Hogan, Burnside, and Division. Buildings are allowed to have multiple stories with larger footprints here to accommodate market demand.

This Sub-District allows for a substantial amount of general office, financial, corporate and institutional uses that employ large numbers of people. It also allows a significant retail presence and residential uses.

4.1114 Downtown Residential Low-Rise-1 (DRL-1)

This mixed-use Sub-District will encourage some residential areas to gently transform into a broader mix of residential uses. This Sub-District is intended to create distinctive, walkable neighborhoods within a short distance of transit and the Downtown core.

The Sub-District encourages single-family homes to remain and allows duplexes but not townhomes or attached dwellings on a single lot. This Sub-District also will allow small-scale commercial uses only on certain streets where it is most appropriate.

4.1115 Downtown Residential Low-Rise-2 (DRL-2)

This mixed-use Sub-District will allow a gradual transformation into more varied and full-service residential neighborhoods that can take advantage of their proximity to transit and nearby shopping and job centers.

This predominantly residential Sub-District will allow single-family homes to remain while also allowing attached single-family houses, small-scale apartments and condominiums, and small-scale commercial activities.

4.1116 Downtown Commercial Low-Rise (DCL)

This Sub-District contains major corridors with the types of businesses, services, stores, and offices that demand a higher level of automobile access to employees and customers. Structures may be single use and aimed at regional traffic. This Sub-District will still serve this role, but the corridors will become more balanced over time to meet the needs of pedestrians as well as automobile traffic. The Sub-District's character will evolve as buildings and more walkable streets become prominent and parking is located to the side or rear of properties. This Sub-District allows commercial, residential, and employment uses, including auto-related uses such as service stations, auto repair, and car washes.

4.1117 Split-Zoned Development Sites

When a single development site in the Downtown Plan Sub-District is affected by two or more of the following Sub-Districts, the entire site may be developed in conformance with permitted uses and development standards of any of those Sub-Districts applying to the site: DCC, DMU, DTM, DEM, DRL-1, DRL-2, and DCL.

4.1120 Permitted Land Uses

The types of land uses that are permitted in the Downtown are listed in Table 4.1120. Permitted uses are designated with a "P". An "L" in this table indicates a use that may be permitted in that Sub-District, but is limited in the extent to which it may be permitted. Limitations are described in Table 4.1120 Notes that follow. An "NP" means that use is not permitted in the specified Sub-District(s). "NP" is only used if the use category is "P" or "L" in another Sub-District within the Development Code. Other categories not listed also are not permitted. Existing uses which are not permitted in a Downtown Sub-District may continue in existence, subject to provisions of Development Code Section 8.0200: Existing and Nonconforming Uses and Development. Community Service uses are permitted as indicated in Table 4.1120 and as provided in Section 8.0100: Community Services.

In addition to permitted commercial and employment land use types listed in Table 4.1120, the Manager may permit other commercial and employment uses found to be supportive of, and consistent with, the Downtown Plan District (Section 4.1100), with the findings and conclusions of the Gresham Downtown Plan, and with applicable land use policies and implementation strategies of the Community Development Code.

Table 4.1120: Downtown Permitted Land Uses

	DCC	DMU	DTM	DEM	DRL-1	DRL-2	DCL
Use Categories:							
COMMERCIAL							
Clinics	P	P	P	P	L ^A	L ^B	P
Retail Trade	P	P	P	P	L ^A	L ^B	P
Retail Services	P	P	P	P	L ^A	L ^B	P
Business Services	P	P	P	P	L ^A	L ^B	P
Auto-Dependent Use	NP	NP	NP	NP	NP	NP	P
Drive Through Use	L ^C	L ^C	NP	L ^C	NP	NP	P
Outdoor Commercial	NP	NP	NP	NP	NP	NP	L ^D
EMPLOYMENT							
Office	P	P	P	P	L ^A	L ^B	P
Manufacturing (4.0320)	L ^E	L ^E	L ^E	L ^E	NP	NP	L ^E
Information Services (4.0320)	P	P	P	P	NP	NP	P
RESIDENTIAL							
Attached Dwellings on a Single Lot	P	P	P	P	NP	P	P
Two-unit Attached Dwellings	NP	NP	NP	NP	P	P	NP
Single Family Attached Dwellings	P	L ^F	P	P	NP	P	P
Single Family Detached Dwelling	NP	NP	NP	NP	P	P	NP
Accessory Dwelling	P	P	P	P	P	P	P
Ancillary Dwellings	P	P	P	NP	P	P	NP
Residential Homes	NP	NP	NP	NP	P	P	NP
Residential Facilities	P	P	P	P	NP	P	P
OTHER							
Live/work	P	P	P	P	NP	P	P
Mixed Use	P	P	P	P	L ^H	P	P

Table 4.1120: Downtown Permitted Land Uses

	DCC	DMU	DTM	DEM	DRL-1	DRL-2	DCL
Use Categories:							
Temporary Uses	P	P	P	P	P	P	P
Home Occupations	P	P	P	P	P	P	P
Community Service Uses							
Type I	P	P	P	P	P	P	P
Type II	P	P	P	P	P	P	P
Type III	L ^G	L ^G	L ^G	L ^G	L ^G	L ^G	L ^G

4.1120 Table Notes

The following standards describe the limitations on use categories marked as limited in Table 4.1120.

- A. Clinics, retail trade, retail services, business services and office: The total floor area of all these uses combined is limited to 5,000 square feet per site in DRL-1 and these uses are limited to lots with frontage on Main Avenue or Ava Avenue. In a mixed-use building, the total square footage of commercial uses, clinics, retail trade, retail services, business services and offices cannot exceed 5,000 square feet in DRL-1.
- B. Clinics, retail trade, retail services, business services and office: The total floor area of all these uses combined is limited to 7,500 square feet per site in DRL-2. In a mixed-use building, the total square footage of commercial uses, clinics, retail trade, retail services, business services and offices cannot exceed 7,500 square feet in DRL-2.
- C. Drive-through use: New drive-through facilities are limited in DCC, DTM and DEM and allowed only under the following conditions:
 - 1. In DCC, no new drive-through facility can be constructed unless there was a legal drive-through on the site on July 16, 2009.
 - 2. In all Sub-Districts where drive-throughs are limited:
 - a. Direct drive-through lane access onto an Urban Boulevard street type is prohibited; and
 - b. The drive-through shall be part of a development that meets the minimum FAR requirements of the Sub-District; and
 - c. Drive-through stacking lanes and service areas shall not be located between the street and the building and shall be located to the rear of the property; and
 - d. Goods and services provided to drive-through customers also must be available to pedestrian customers inside a building on the same site.
- D. Outdoor Commercial is limited to DCL and allowed only if:
 - 1. The site has frontage on Burnside Road or Hogan Road; and
 - 2. The amount of total site area covered by buildings shall amount to no less than 25% of the amount of site area used for outdoor storage or display; and
 - 3. Screening shall be provided along any portion of the site's frontage which is not occupied by a building or parking area, in a manner which satisfies standards for Landscape/Screening along a Public Right-of-Way, as contained in Section 9.0823(C)(3).
- E. Manufacturing uses are defined in Community Development Code Table 4.0320 but shall be compatible with other Downtown uses and are allowed only if all of the following conditions are met:
 - 1. Manufacturing uses are subject to all standards in Section 4.0331(F): External Effects; and
 - 2. The manufacturing component must be accessory to another permitted, non-residential use on the same site; and
 - 3. In DCC, DMU, and DTM, the manufacturing component shall occupy no more than 10,000 square feet of floor area per site. In DEM and DCL, the manufacturing component shall occupy no more than 20,000 square feet of floor area per site.
- F. Single-family attached residential uses are prohibited in DMU within 320 feet of the centerline of Eastman Parkway.

- G. Community Services, Type III: Solid waste transfer stations, solid waste landfills, campgrounds, golf courses, recycling facilities (including drop-box transfer stations), recycling collection sites and recyclables recovery facilities, are prohibited.
- H. Mixed-use developments in DRL-1 are limited to those locations where commercial uses are allowed.

4.1121 Prohibited Uses in Downtown Sub-Districts

- A. In addition to the use categories that are not permitted because they are listed as “NP” or not listed in Table 4.1120, the following categories are specifically prohibited from Downtown Sub-Districts: mini-storage facilities and the following uses defined in 4.0320: transportation/distribution (trucking and rail), exclusive heavy industrial uses, warehouse and storage, wholesale trade, construction contractors, transport and distribution.
- B. The prohibition on warehousing and storage as a stand-alone use does not preclude the on-site storage of materials associated with another use Downtown, such as the storage of goods and supplies as an accessory use to another use allowed in Downtown Sub-Districts.
- C. The prohibition on distribution does not preclude the distribution of goods produced on-site to locations off-site as an accessory use to another use allowed in the Downtown Sub-Districts or the delivery of goods to an allowed use.

4.1130 Downtown Plan Sub-District Standards

Table 4.1130 summarizes development standards that apply in the Downtown Plan District. The standards contained in this Table are supplemented by referenced sub-sections of 4.1100 which provide additional clarification and guidance. Existing developments that do not meet the standards specified for a particular Sub-District may continue in existence and be altered, subject to provisions of Section 8.0200, Existing and Nonconforming Uses and Development. There are two key factors of development potentially allowable on parcels and development sites for each district with the Downtown: building height and floor area ratio. To determine the development potential of a property, the FAR number is multiplied by the site area. For example: A site area of 20,000 square feet multiplied by an FAR of 3.0 yields 60,000 square feet of floor area that can be built on that site.

- A. Development on a site must achieve at least the Minimum Floor Area Ratio and can build up to the Maximum Floor Area Ratio.
- B. Additional FAR, shown in the Maximum Floor Area Ratio with Bonuses column, can be achieved by including bonus elements and attributes (described in 4.1131). Multiple bonus elements can be used in combination to achieve the maximum FARs listed in the Maximum Floor Area Ratio with Bonuses column in Table 4.1130. Each bonus element can be used only once, unless otherwise indicated in the bonus element descriptions. This method allows for many combinations of uses, intensities and different features within each development.
 - 1. With bonuses, residential-only buildings can achieve a maximum FAR up to what is shown in the Residential column under Maximum Floor Area Ratio with Bonuses. Maximum heights cannot be exceeded in the DRL-1 and DRL-2 districts.
 - 2. With bonuses, buildings that do not include a residential use can achieve a maximum FAR up to what is shown in the Non-residential column under Maximum Floor Area Ratio with Bonuses.
 - 3. With bonuses, buildings that combine both residential and non-residential uses can achieve a maximum FAR up to what is shown in the combined column under Maximum Floor Area Ratio with Bonuses.
- C. For purposes of floor-area-ratio calculations in DCC, DMU and DTM, applicants may include the following in the ground-level floor area up to a total of 1,000 square feet or 10% of the site, whichever is larger:
 - 1. Publicly accessible open space.
 - 2. Walkways that include landscaping and provide access to publicly accessible open spaces or provide a through-block connection.
 - 3. Required minimum setbacks.
 - 4. Area removed from the first-floor building footprint to create corner features to comply with 4.1151(B) (8). For example, a beveled corner will have a slightly smaller footprint than a 90-degree corner because of the bevel.
 - 5. Public space required for gateway locations.

		DCC		DMU	DTM	DEM	DRL-1	DRL-2	DCL
			Main see 4.1130(A)						
Maximum Height ^{A-D} (feet)		85	45	125	85	70	35	50	65
Minimum Floor Area Ratio (base) (see 4.1145)		1.0	1.0	1.0	1.0	0.5	0	0.5	0.5
Maximum Floor Area Ratio		3.0	1.5	3.5	3.0	2.5	1.0	1.0	1.0
Maximum Floor Area Ratio with bonuses (see 4.1131)	Residential	3.5	2.0	5.0	3.5	3.0	2.0	2.0	2.0
	Non Residential	4.5	3.0	6.0	4.5	3.0	3.0	3.0	2.5
	Combined	5.0	4.0	7.0	5.0	5.0	4.0	4.0	3.0
Minimum Residential Density for all-residential projects (not mixed use) (units per acre)		17		20	20	17	8.71	8.71	17
Maximum residential density (units per acre)		No maximum		No maximum	No maximum	No maximum	12.45	No maximum	60

Table 4.1130 (continued)

Street types	Urban Boulevard (in feet)	Urban Commercial (in feet)	Urban Residential (in feet)	Green/Shared (in feet)	Main (in feet)	Stanley (in feet)	Third – north side (in feet)	Third – south side (in feet)	Cleveland (in feet)
Commercial, employment, live/work, mixed-use									
Minimum Building Setbacks	Front: 0 Side: 0 ^G Rear: 0 ^G Alley: 3	Front: 5 Side: 0 ^G Rear: 0 ^G Alley: 3	Front: 5 Side: 0 ^{G&K} Rear: 5 ^G Alley: 3	Front: 0 Side: 0 ^{H&K} Rear: 0 ^G Alley: 3	Front: 0 Side: 0 ^K Rear: 0 Alley: 3	Front: 0 Side: 0 ^K Rear: Alley: 3	Front: 6 on first floor; zero above Side: 0 ^{G&K} Rear: 0 ^G Alley: 3	Front: 0 Side: 0 ^{G&K} Rear: 0 ^G Alley: 3	Front: 5 Side: 0 ^{G&K} Rear: 0 ^G Alley: 3
Maximum Building Setbacks	Front: 5 ^M	Front: 10 ^M	Front: 10 ^M	Front: 5 ^M	Front: 5 ^M	Front: 5 ^M	Front: 10 ^M	Front: 10 ^M	Front: 10 ^M

Table 4.1130 (continued)

Street types	Urban Boulevard (in feet)	Urban Commercial (in feet)	Urban Residential (in feet)	Green/ Shared (in feet)	Main (in feet)	Stanley (in feet)	Third – north side (in feet)	Third – south side (in feet)	Cleveland (in feet)
Attached dwellings on a single lot									
Minimum Building Setbacks	Front: 10 Side: 0 ^G Rear: 5 ^G Alley: 3	Front: 5 Side: 0 ^G Rear: 5 ^G Alley: 3	Front: 5 Side: 0 ^{G&K} Rear: 5 ^G Alley: 3	Front: 5 Side: 0 ^{H&K} Rear: 5 Alley: 3	Front: 0 Side: 0 ^K Rear: 0 Alley: 3	Front: 0 Side: 0 ^K Rear: 0 Alley: 3	Front: 6 on first floor; zero above Side: 0 ^{G & K} Rear: 0 ^G Alley: 3	Front: 0 Side: 0 ^{G & K} Rear: 0 ^G Alley: 3	Front: 5 Side: 0 ^{G&K} Rear: 5 ^G Alley: 3
Maximum Building Setbacks	Front: 20 ^{I&M}	Front: 10 ^{I&M}	Front: 15 ^{I&M}	Front: 10 ^M	Front: 5 ^M	Front: 10 ^{I&M}	Front: 10 ^M	Front: 5 ^M	Front: 20 ^M
Single-family attached									
Minimum Building Setbacks ^N	FRONT Façade: 10 Porch: 6 Garage: 10 ^L SIDE^I Interior: 5 Common: 0 Street porch: 5 REAR No alley: 10 Alley: 3	FRONT Façade: 5 Porch: 3 Garage: 5 ^L SIDE^I Interior: 5 Common: 0 Street porch: 5 REAR No alley: 10 Alley: 3	FRONT Façade: 5 Porch: 3 Garage: 5 ^L SIDE^I Interior: 5 Common: 0 Street porch: 5 REAR No alley: 10 Alley: 3	FRONT Façade: 5 Porch: 3 Garage: 5 ^L SIDE^I Interior: 5 Common: 0 Street porch: 5 REAR No alley: 10 Alley: 3	Not allowed on this street type.	Not allowed on this street type.	Not allowed on this street type.	Not allowed on this street type.	FRONT Façade: 5 Porch: 3 Garage: 5 ^L SIDE^I Interior: 5 Common: 0 Street porch: 5 REAR No alley: 10 Alley: 3
Maximum Building Setbacks	Front: 20 ^{I&M} Garage: 5	Front: 20 ^{I&M} Garage: 5	Front: 20 ^{I&M} Garage: 5	Front: 15 ^{I&M} Garage: 5					Front: 20 ^{I&M} Garage: 5

Table 4.1130 (continued)

Street types	Urban Boulevard (in feet)	Urban Commercial (in feet)	Urban Residential (in feet)	Green/ Shared (in feet)	Main (in feet)	Stanley (in feet)	Third – north side (in feet)	Third – south side (in feet)	Cleveland (in feet)
Single-family detached^o and two-unit attached dwellings^p									
Minimum Building Setbacks ⁿ	FRONT Façade: 10 Porch: 6 Garage: 20 SIDE Interior: 10 Streetside: 10 Street porch: 6 Garage: 5 REAR No alley: 10 Alley: 6	FRONT Façade: 10 Porch: 6 Garage: 20 SIDE Interior: 10 Streetside: 10 Street porch: 6 Garage: 5 REAR No alley: 10 Alley: 6	FRONT Façade: 10 Porch: 6 Garage: 20 SIDE Interior: 10 Streetside: 10 Street porch: 6 Garage: 5 REAR No alley: 10 Alley: 6	FRONT Façade: 10 Porch: 6 Garage: 20 SIDE Interior: 10 Streetside: 10 Street porch: 6 Garage: 5 REAR No alley: 10 Alley: 6	N/A	N/A	N/A	N/A	FRONT Façade: 10 Porch: 6 Garage: 20 SIDE Interior: 10 Streetside: 10 Street porch: 6 Garage: 5 REAR No alley: 10 Alley: 6
Maximum Building Setbacks	Front: 20'	Front: 20'	Front: 20'	Front: 15'	Front: 15'	Front: 10'	Front: 20'	Front: 5'	Front: 20'

4.1130 Table Notes:

- A. The maximum height and FAR for the portion of development sites within 110 feet of the centerline of Main Avenue between Powell Boulevard and Fifth Street are limited to those shown in Table 4.1130 for DCC: Main Street. A 10-foot building setback on Main Street is required above the 3rd story.
- B. Floor heights by building type
 - 1. Commercial/employment uses: 13 feet minimum height to 20 feet maximum height.
 - 2. Residential uses: 10 feet minimum height.
- C. Building heights in all Downtown Sub-Districts except DRL-1 and DRL-2 shall be allowed to exceed the maximum up to 8 feet to allow for architectural elements not suitable for occupancy, such as parapet walls and pitched roofs.
- D. For developments abutting a land-use district where the maximum building height is 40 feet or less, the maximum building height within 50 feet of that low-height-limit district or Sub-District shall be 45 feet. Maximum building height shall be 50 feet in DCC along the north side of Division Street.
- E. Setbacks
 - 1. Setbacks in Table 7.0201 (M)(3)(a) of the Gresham Development Code do not apply.
 - 2. Refer to Section 9.0900 for a list of structural elements and decorative features that may encroach up to a maximum of 2 feet into required setbacks
 - 3. Encroachments may project into the public right-of-way provided that the encroachments are between 9 and 12 feet above the sidewalk height, subject to City approval. They must not obstruct or prevent the placement of street trees or other improvements within the public right-of-way.
- F. Flag pole portions of lots, which allow access to the portions of the lot away from streets, are not counted as part of the site size in floor-area ratio calculations. The remainder of the lot not needed for access would be required to meet FAR requirements.
- G. Side setback is 10 feet when side yard abuts DRL-1, DRL-2, LDR-5, LDR-7, TR or TLDR; rear setback is 10 feet when rear yard abuts DRL-1, DRL-2, LDR-5, LDR-7, TR or TLDR.
- H. Side setback is 5 feet when in the DRL-2 Sub-District or when the side yard abuts the DRL-2 Sub-District.
- I. Courtyard-type developments are exempted from maximum setbacks for that portion of the building or buildings having a courtyard area between it and the street.
- J. For attached single-family dwellings, the street façade and garage side setbacks shall be equal to the front façade setback on the street type that applies to the side street.
- K. For commercial, employment, live/work, mixed-use, and attached dwellings on a single lot, the streetside setbacks shall be equal to the front façade setback on the street type that applies to the side street.
- L. Single-family attached dwellings are encouraged to be accessed from a rear alleyway on all street types where single-family attached dwellings are allowed. Parking on driveways in front of single-family attached dwellings is only permitted on the Urban Residential street type in Sub-Districts DRL-1 and DRL-2. If parking is to be provided in front of these units on the Urban Residential street type in DRL-1 and DRL-2, the minimum front setback for the garage is 20 feet.
- M. Maximum front setbacks do not apply on a site once all minimum building frontage requirements of the street type have been satisfied. For example, if a street type requires that 75% of the frontage must be occupied by a building and that requirement has been satisfied, other buildings can be constructed behind that building beyond the maximum setback.
- N. For purposes of Table 4.1130 side setbacks:
 - 1. Interior means interior side (not common wall)
 - 2. Common means interior side with common wall
 - 3. Street façade means streetside façade
 - 4. Street porch means streetside porch
 - 5. Garage means streetside garage face

O. Single-family detached dwellings shall comply with the following:

1. A minimum lot size of 4,000 square feet.
2. 4.0130 standards for minimum lot dimensions and minimum street frontage for single-family detached dwellings in TR; and
3. 4.0132 Additional Standards, except (A)(1) and (B); and
4. 4.0136 Manufactured Dwelling Requirements; and
5. Standards for maximum height and setbacks in 4.1130; and
6. Standards for parking in 9.0800.

P. Two-unit attached dwellings shall comply with the following:

1. A minimum lot size of 7,000 square feet.
2. 4.0130 standards for TR minimum lot dimensions for duplexes and minimum street frontage; and
3. 4.0132(D) Safe Neighborhood Design Performance Standards; and
4. Standards for maximum height and setbacks in 4.1130; and
5. Standards for parking in 9.0800; and
6. For developments with more than one two-unit attached dwelling on a site, the applicable design review standards in 7.0201.
7. Two-unit attached dwellings may be reviewed under the discretionary review process described in 4.1101(A)(1)(a) using the following guidelines:
 - a. 4.1151(A)(1); and
 - b. 4.1151(B)(2)(c)(1) and (6) through (9); and
 - c. 4.1155(A)(2)(c)(1) and (2); and
 - d. 4.1155(A)(4)(c)(1) and (2).

4.1131. Bonus Elements

The future vision for Downtown Gresham calls for a series of amenities that will lead to a more livable and complete community. As a means of encouraging new development to incorporate these amenities, a series of development bonuses are included in the Code that offer additional development potential in the form of floor area. Most projects will require a mixture of various elements.

A. For each of the following, an FAR increase of .5 shall be granted:

1. LEED Silver Certification

A development shall be LEED Silver Certified, according to criteria established by the U.S. Green Building Council (USGBC). The applicant is responsible for providing LEED Silver Pre-Certification documentation and annotated checklist to the City to receive this bonus. The City will review and approve the documentation and apply this implementation of the LEED Silver elements as a condition of approval for the subsequent building permit.

2. Exterior Art Elements

Exterior art element(s) shall be equivalent to at least 1% of the total value of the project's construction cost. Such elements include but are not limited to free-standing sculptures, bas-reliefs on walls, metalwork on walls and murals. Documentation shall be provided of the construction value and the value of the art as appraised by an art appraiser. Art elements shall be visible to the public at all times and will be reviewed and approved for artistic merit by an arts review body designated by the City.

3. Water Feature(s)

Any decorative water feature(s) shall be equivalent to at least 1% of the total value of the project's construction costs. Documentation shall be provided of the construction value and the cost of the water feature. Water features shall be directly accessible by the public or shall be visible to the public by being adjacent to a sidewalk or through-block connection.

4. Alley Enhancements

Enhancements to an alley shall include elements that create a pedestrian-friendly setting such as stone, brick, unit pavers or textured paving; pedestrian-scaled lighting that could be wall-mounted; and landscape plantings such as vines splayed on walls, trees, and irrigated planters that do not obstruct the movement of vehicles. The result shall be a place that appears safe, inviting and allow for public

enjoyment. Features such as entrances with canopies or stoops shall also qualify so long as vehicular movement is maintained. Entrances to uses are permitted and encouraged.

5. Canopy Over Sidewalks Where Not Otherwise Required

A canopy shall be a permanent structure of steel and glass that projects over a public sidewalk from a building face at least 5 feet in horizontal distance. The height of a canopy above the sidewalk shall be between 8 and 12 feet. Canopies may be intermittent or continuous provided the total length is at least 75% of the frontage along the public sidewalk.

6. Through-Block Connection

A through-block connection is a pedestrian walkway and accompanying landscaping that shall be at least 15 feet wide and extend along a property line or through a site to allow the public to pass from one street to either another street or an alley that is either parallel or perpendicular. The surface shall be accessible and consist of stone, unit pavers or textured concrete. The connection shall include pedestrian scaled lighting along the length at intervals no less than 50 feet, that can either be wall-mounted or freestanding.

7. Public Meeting Room

This is a room that shall be available to the community for meetings and events. The size shall allow for at least 30 people to occupy it (300 square feet) and it shall be accessible either directly from the outside or from a controlled lobby that allows public access. There shall be no fees imposed on user groups, other than for basic maintenance. The room shall have windows on at least one side.

8. Multi-Modal Pathway

A multi-modal pathway shall be a pathway for the movement of pedestrians and bicyclists that is consistent with the City's Parks and Recreation Master Plan and approved by the City staff.

9. MAX Pathway Landscaping

MAX pathway landscaping shall include landscape elements such as trees, shrubs, groundcovers and perennials that are consistent with the City's Parks and Recreation Master Plan and approved by City staff. The entire development frontage on the Pathway shall be landscaped.

B. For each of the following, an FAR increase of 1.0 shall be granted:

1. LEED Gold Certification

A development shall be LEED Gold Certified, according to criteria established by the U.S. Green Building Council (USGBC). The applicant is responsible for providing LEED Gold Pre-Certification documentation and annotated checklist to the City to receive this bonus. The City will review and approve the documentation and apply this implementation of the LEED Gold elements as a condition of approval for the subsequent building permit.

2. Structured Parking, On-Grade or Above-Grade

At least 80% of the parking shall be contained within a structure. The structure may be part of the building or a separate structure. The structure shall be designed so that no parked cars are visible from the street and active commercial, employment or residential uses occupy the street level if the structure is on a street frontage. Any wall openings necessary for ventilation shall be screened with decorative metalwork or irrigated planters. Interior lighting shall be shielded to obscure visibility from outside.

3. Public Plaza

This is an outdoor space available to the public at all times. It shall be equivalent to at least 2% of the interior floor area but shall not be less than 400 square feet. No dimension shall be less than 10 feet. At least 70% of the area shall be paved with stone, brick, unit pavers or textured paving. One tree (with a minimum caliper of 2 ½ inches) shall be provided for every 600 square feet of area. A quantity of one bench or seating unit shall be provided for each 100 square feet of area. Seating placement shall be artfully designed. If a project includes more than one Public Plaza that meets these standards, each can generate bonus FAR.

4. Public Greenspace

This is an outdoor space available to the public at all times. It shall be visible and accessible from a public sidewalk. It shall be equivalent to at least 2% of the interior floor area but shall not be less than 400 square feet. No dimension shall be less than 10 feet. At least 70% of the space shall be planted with grass or walkable ground cover. One tree (with a minimum caliper of 2 ½") shall be provided for every 600

square feet of area. One seating unit, which may be in the form of benches or ledges, shall be provided for each 100 square feet of area. If a project includes more than one Public Greenspace that meets these design standards, each can generate bonus FAR.

5. Public Indoor Wintergarden

This is an interior space available to the public during normal business hours. It shall be equivalent to at least 2% of the interior floor area but shall not be less than 1000 square feet. No dimension shall be less than 20 feet. At least 50% of the ceiling and at least one wall of the wintergarden shall be glass to allow for day lighting. One seating unit, which may be in the form of benches or ledges, shall be provided for each 100 square feet of area.

C. For each of the following, an FAR increase of 1.5 shall be granted:

1. LEED Platinum Certification

A development shall be LEED Platinum Certified, according to criteria established by the U.S. Green Building Council (USGBC). The applicant is responsible for providing LEED Platinum Pre-Certification submittal documentation and annotated checklist to the City to receive this bonus. The City will review and approve the documentation and apply this implementation of the LEED Platinum elements as a condition of approval for the subsequent building permit.

2. Roof Garden

Roof gardens shall provide usable outdoor space and gardening opportunities to the building residents, tenants and/or visitors. The roof garden shall occupy a minimum of 50% of the roof surface and result in an accessible rooftop terrace with defined growing areas, seating and additional landscaping.

3. Eco (Green) Roof

A roof garden shall be a combination of landscape plantings and a growing medium installed on a roof deck that performs functions of collecting, absorbing and filtering rain water and shall occupy a minimum of 50% of the roof surface. The result shall be visually interesting and it shall accomplish environmental objectives such as reducing the amount of run-off that reaches the ground, reducing heat gains and losses from the building, and recycling gray water.

4. Underground Parking

At least 80% of the parking shall be contained within a structure that is below grade with an energy efficient system developed to maintain the parking underground. Any parking stalls not located in the below-grade structure shall be within a structure that is on-grade or above-grade that shall meet the design standards for the bonus feature of Structured Parking, On-Grade or Above-Grade.

4.1132 Building Placement

The Downtown Sub-Districts will be the most active, intense, and urban areas in the city. Consistent building edges out to or near the sidewalk help contribute to these qualities.

4.1133 Parking

See Section 9.0800 for general parking lot design standards and parking ratios.

4.1134 Buffering

New development in the Downtown Plan District is exempt from the provisions of Section 9.0100 - Buffering and Screening Requirements, except where the proposed development abuts a lot that is outside the Downtown Plan District.

4.1135 Utilities

Undergrounding of utilities in the Downtown Plan District shall be in accordance with Section A5.510 - Underground Utilities.

4.1136 Clear Vision Area

New development in the Downtown Plan District shall be exempt from the provisions of Section 9.0200 – Clear Vision Area, except for developments with frontage on an Urban Boulevard Street Type.

4.1137 Limitations on Outdoor Commercial Activity

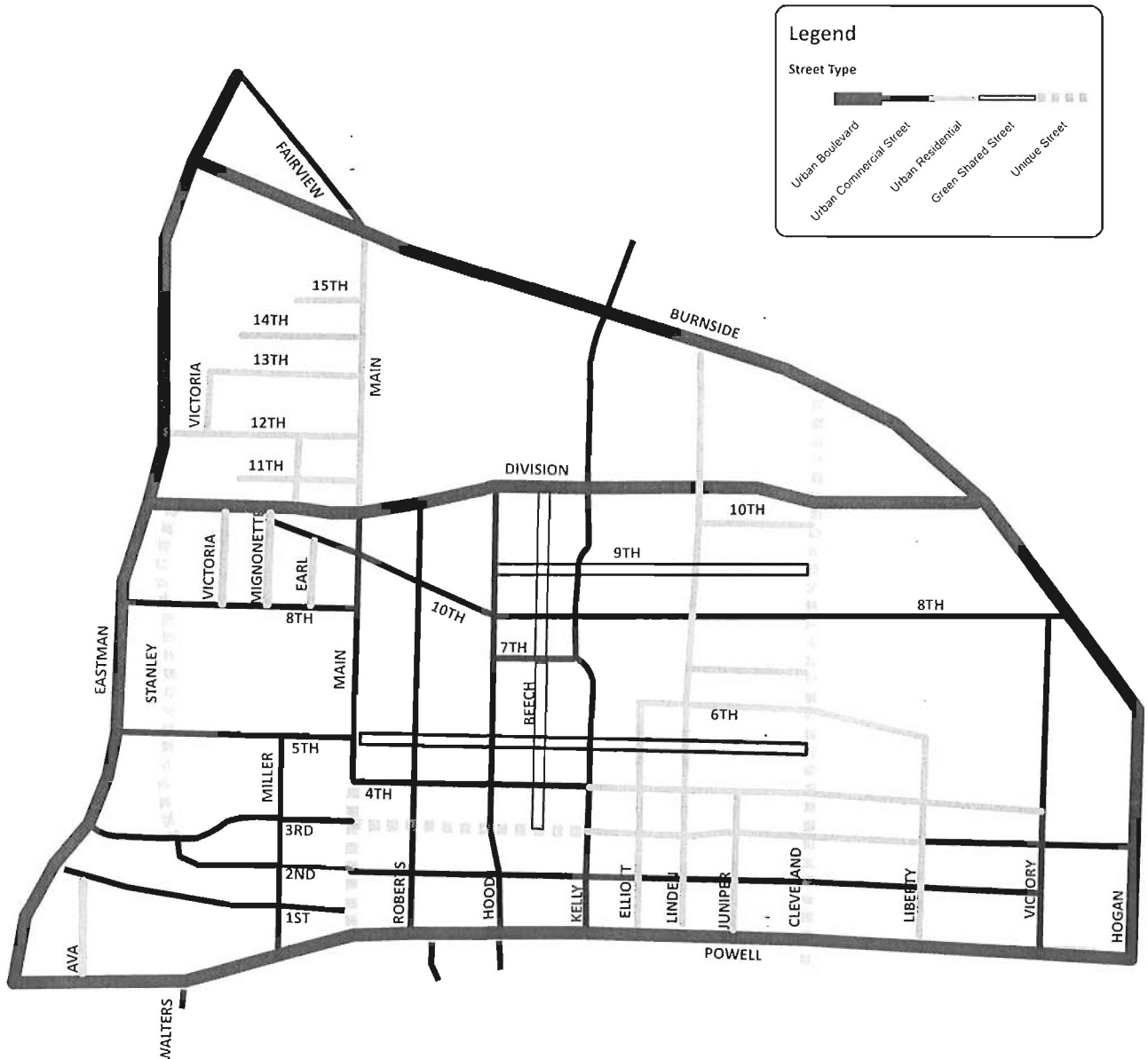
In all Downtown Sub-Districts except the DCL Sub-District, the amount of site area for outdoor display of materials for retail sales shall not exceed 15% of the ground-floor area of buildings on the site with which the display is associated or 1,000 square feet, whichever is less.

Downtown Street Types

4.1140 Street Type Purpose

The Street Types sections contain non-discretionary standards that applicants must meet as part of the design review process to ensure positive building-to-sidewalk relationships and well-designed streetscapes. The life of Downtown shall be closely tied to the character of its public space, and it is especially important to acknowledge the importance of street as public space. Street design features, and building-to-street relationships will support the development of the Downtown area as a focus of the community and as pedestrian-oriented, transit-supportive Sub-Districts. Special features have been incorporated into several street design classifications included in this document. This section includes several different street types applied to streets throughout Downtown, recognizing that street design and how buildings interface with the street varies based on function, location, land uses and multi-modal capacity.

Downtown Street Types



Map 4.1140: Downtown Street Types

These Street Type requirements will ensure consistent application of appropriate treatments based on mixes of uses and levels of intensity throughout the Sub-Districts. This section includes standards for the following Street Type designations (refer to Figure 4.1140 for specific designations of each Street Type Downtown):

- Urban Boulevard
- Urban Commercial Street
- Urban Residential Street
- Green/Shared Street
- Unique Streets (Main Avenue, Stanley Avenue, Third Street, Cleveland Avenue)

As new streets are constructed or existing streets are redeveloped, environmental friendly features for stormwater management are required. See Street Types and the Gresham's Public Works Standards for requirements.

4.1141 Public Works Standards Coordination

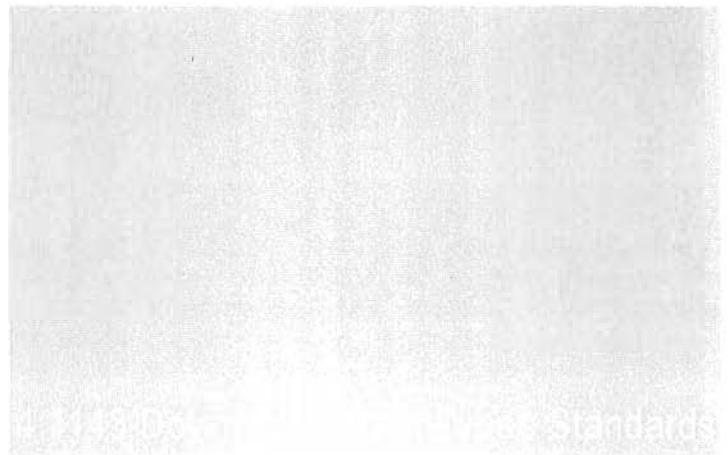
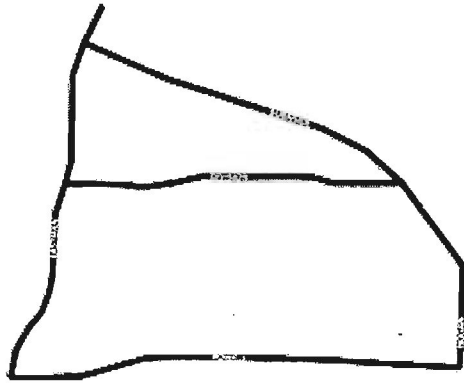
Applicants shall consult specific street engineering construction standards in the Public Works Standards and the Green Streets Standards in combination with these Street Type Standards and the street standards included elsewhere in the Gresham Community Development Code. If compliance with the standards results in a conflict between the Community Development Code and Public Works Standards, the Public Works Standards govern the street design. The Department of Environmental Services may grant exceptions to the Public Works Standards through the Design Modification Process if the City receives benefit from the suggested standard deviation.

Where existing public right-of-way is too narrow to accommodate required Street Type improvements, the applicants shall dedicate the required right-of-way. On Third Street, an applicant may choose to dedicate the required right-of-way or provide a sidewalk easement for the sidewalk portion under the colonnade on the north side of the street.

4.1142 Street Type General Provisions

- A. For the purposes of the minimum frontage percentage required to be occupied by a building on a street type, the building facade can be placed anywhere between the minimum and maximum setback.
- B. If the development site has frontage on more than one street type, the minimum percentage of street frontage that must be occupied by a building facade may be reduced in one of the following ways:
 1. Except for the Main, Third, Stanley and Boulevard street type frontages, the minimum percentage of street frontage that must be occupied by a building facade may be reduced on one street type frontage by the Design Commission or Manager up to 25% of the total frontage along that street type as part of the clear-and-objective track. For example, a requirement that 75% of the frontage must be occupied by a building facade could be reduced to 50% of the frontage on that one street type; or
 2. The minimum percentage of street frontage that must be occupied by a building facade may be reduced through the discretionary review process. The applicant must demonstrate that the application complies with applicable site-design Design Guidelines in 4.1151 and 4.1152.
- C. The minimum amount of building frontage required on the front setback line may be reduced by the Design Commission or Manager if an adjustment is required to allow a driveway with the required minimum width, on street types where driveways are allowed.
- D. The minimum amount of building frontage required does not apply for single-family detached and two-unit attached dwellings.
- E. For courtyard developments involving attached dwellings on a single lot or single-family attached dwellings, the minimum amount of building required on the frontage is 40%.
- F. Lots whose frontage on a street includes only vehicular access, such as a flag pole, are not required to meet the requirements for minimum building on the frontage.
- G. The following community service uses are exempt from requirements for the minimum amount of building frontage required on a street type:
 1. Public urban plazas and public walking/hiking trail with associated trail access points and trailheads.
 2. Public neighborhood parks.
 3. Cemeteries.
 4. Public community parks.
 5. Public multi-use paths with associated trail access points and trailheads.
- H. The Design Commission may reduce the transparency requirements in the Street Type standards if the Design Commission determines transparency guidelines in 4.1151(B) have been met.

APPLIES TO ALL DEVELOPMENT ON STREETS:

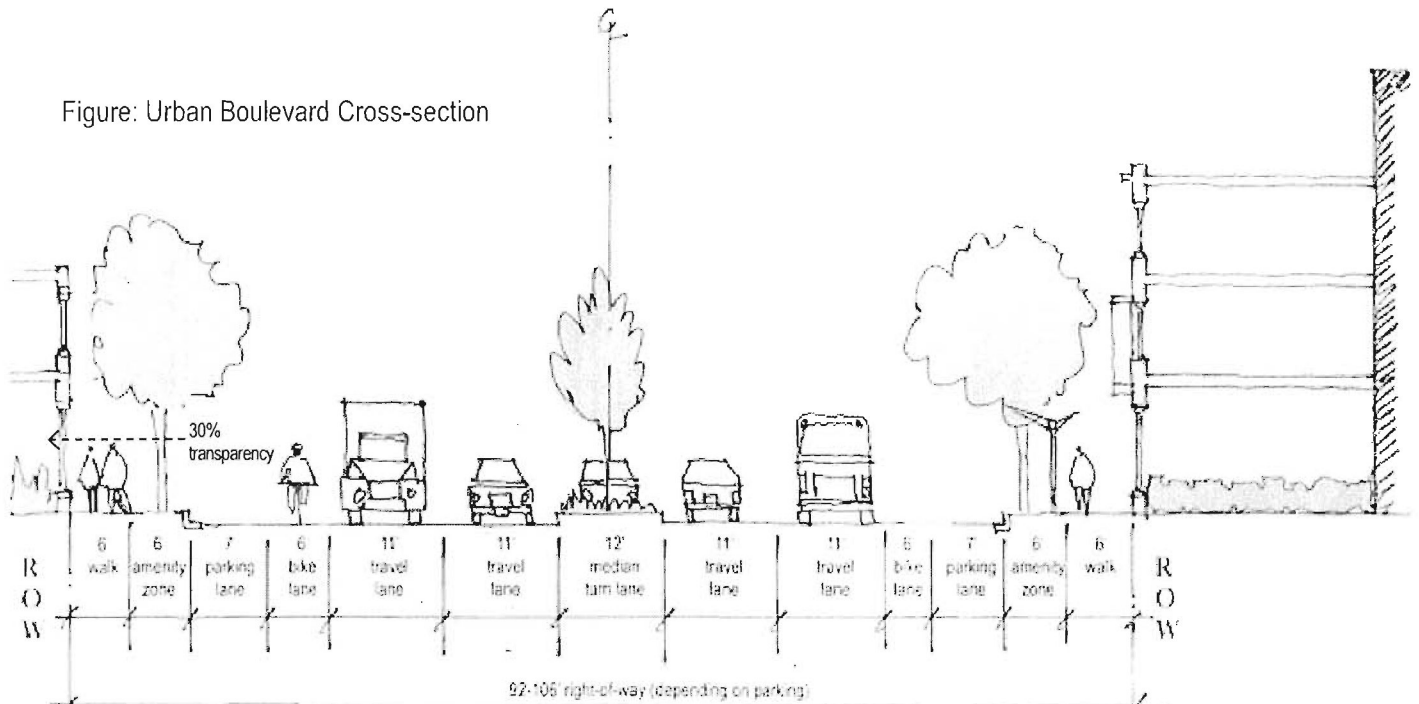


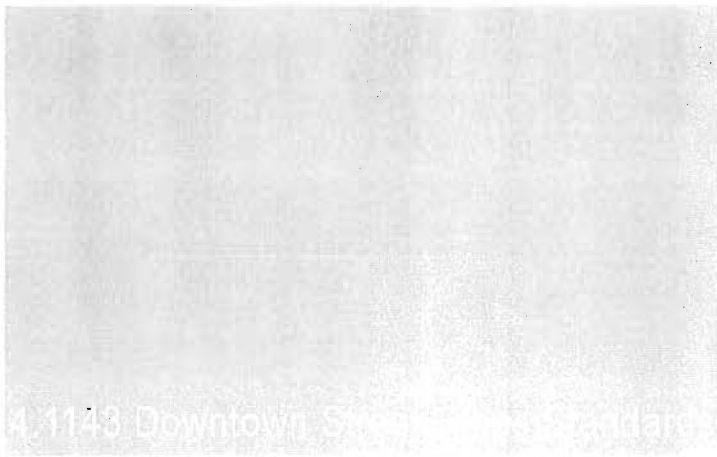
A. Urban Boulevard

For the Urban Boulevard Street Type, all the following Street Type Standards shall be met:

1. Applicants shall consult specific street engineering construction standards in the Public Works Standards and the Green Streets Standards in combination with these Street Type Standards and street standards included elsewhere in the Gresham Community Development Code.
2. There shall be a clear accessible walking route of 5 feet provided in a walk of 6 feet width. Appurtenances can protrude one foot into the walk width.
3. There shall be a 6-foot amenity zone provided. This amenity zone may consist of street trees, street lighting, landscaping and/or seating.
4. At least 50% of the site's frontage shall be occupied by a building, except for the north side of Powell Boulevard between Miller and Roberts, which shall have a building facade on 60% of the frontage.
5. A minimum of 30% of the ground-floor level shall be transparent with visibility into and out of the building, except for the north side of Powell between Miller and Roberts, where a minimum of 75% of the ground-floor level shall be transparent with visibility in and out of the building.
6. There is no first-floor commercial requirement on this Street Type, except for the north side of Powell between Miller and Roberts, which shall be required to have first-floor commercial uses.
7. No parking shall be permitted on any corners facing public streets.

Figure: Urban Boulevard Cross-section





APPLIES TO ALL DEVELOPMENT ON STREETS:



Fig. A planted median

Figure: Urban Boulevard Plan View

- 8. The Gresham Standard 16-foot height Downtown Pedestrian Light is required as detailed in the Public Works Standards.
- 9. If on-street parking is included, curbed bulb-outs at the street intersections are required.
- 10. Comply with Section 9.0200 – Clear Vision Area.
- 11. Access to single-family attached dwellings is prohibited directly from Urban Boulevard streets. Access from rear alleys and side streets is allowed.

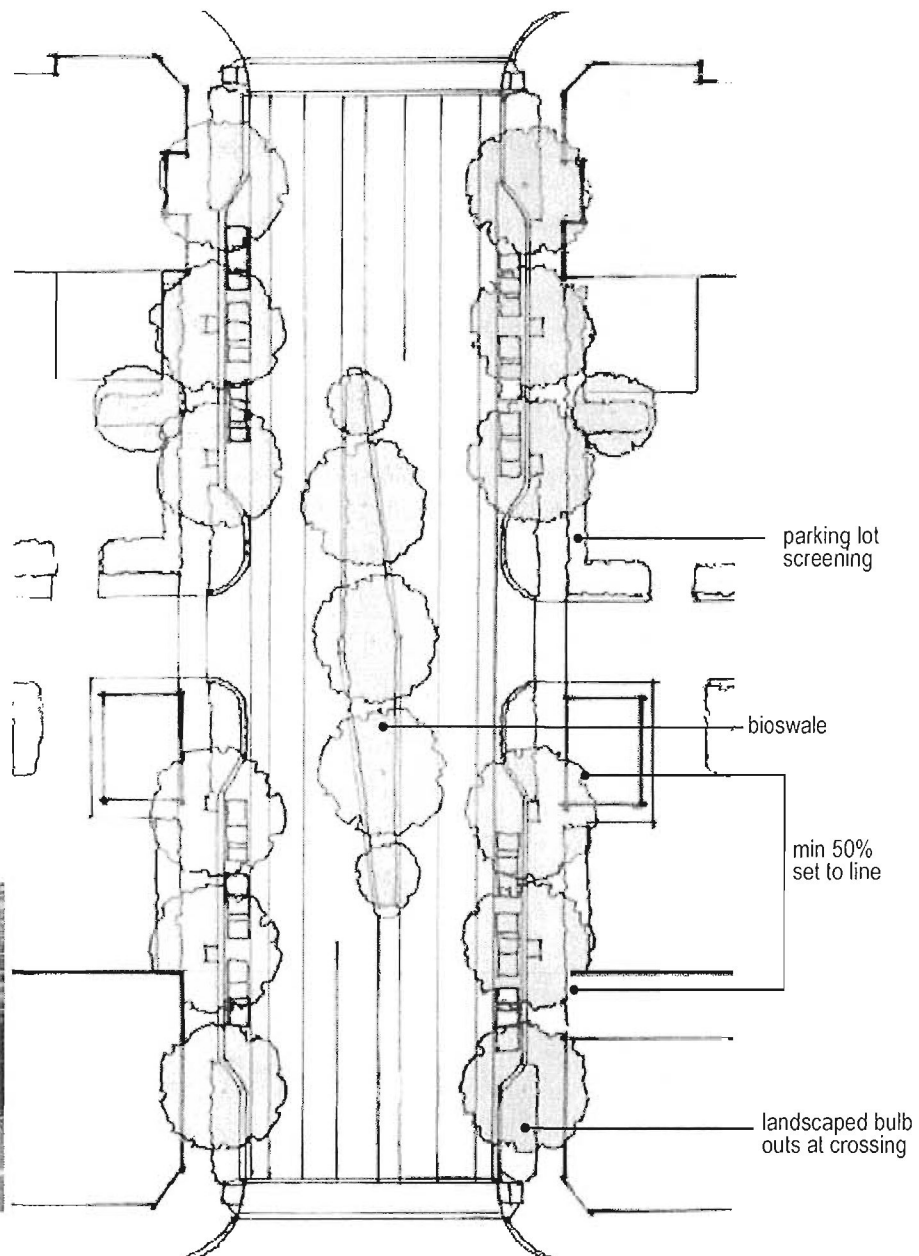
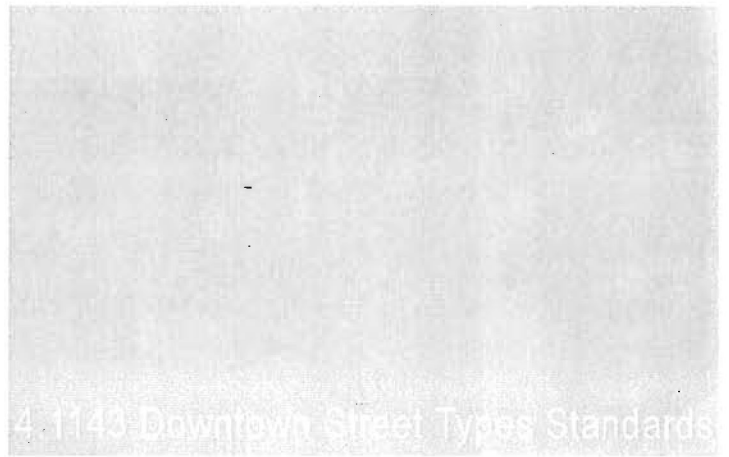
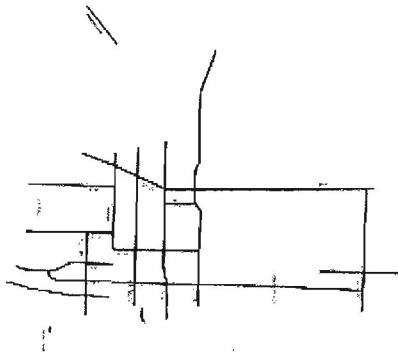


Fig. 1. A large tree canopies and distinctive lighting

APPLIES TO ALL DEVELOPMENT ON STREETS:

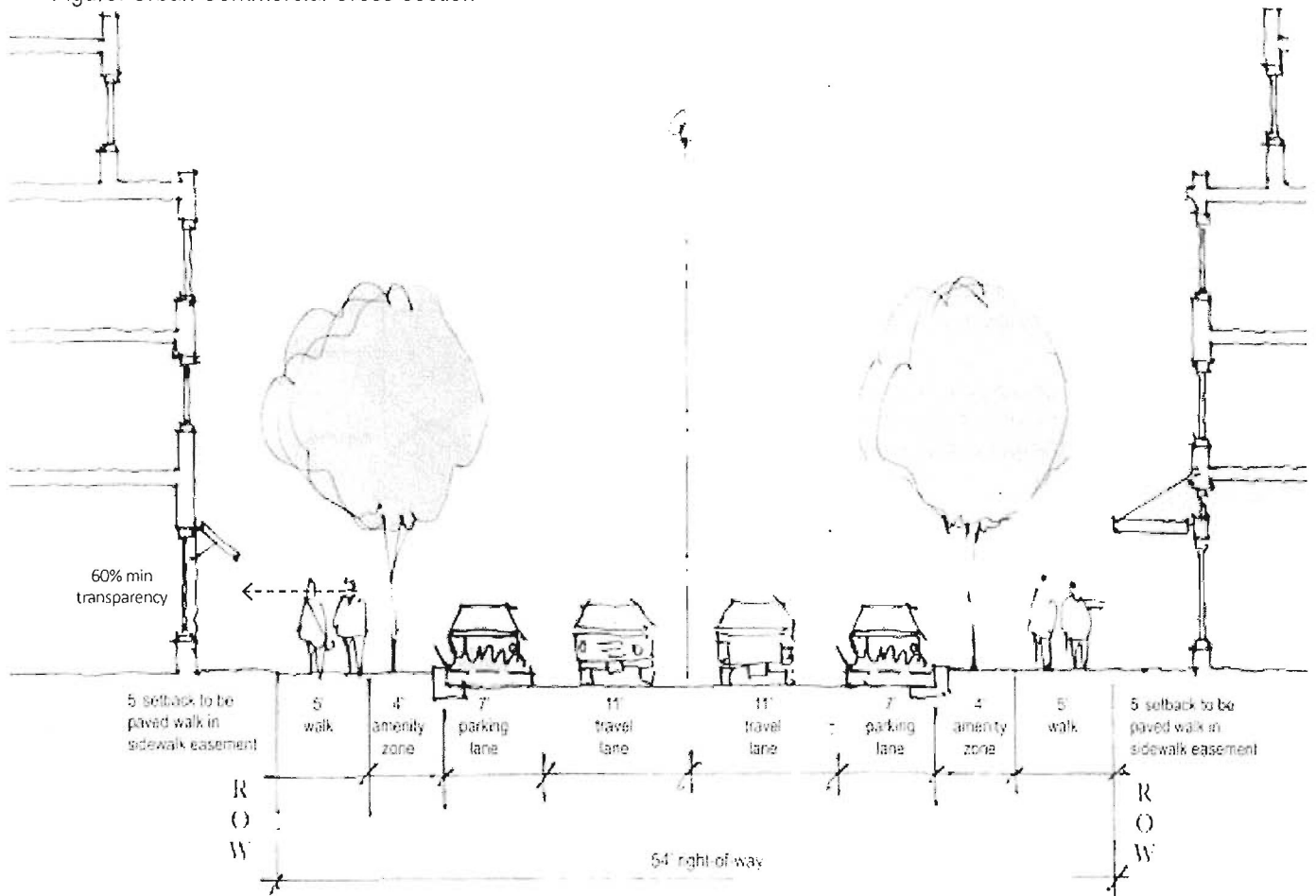


B. Urban Commercial Street

For the Urban Commercial Street Type, all the following Street Type Standards shall be met:

1. Applicants shall consult specific street engineering construction standards in the Public Works Standards and the Green Streets Standards in combination with these Street Type Standards and street standards included elsewhere in the Gresham Community Development Code.
2. There shall be a clear accessible walking route of 5 feet provided in a walk of 10 feet width. Five feet of the 10-foot walk shall be dedicated within a sidewalk easement on private property. Appurtenances can protrude 1 foot into the walk width.
3. There shall be a 4-foot amenity zone provided. This amenity zone may consist of street trees, street lighting, landscaping and/or seating.
4. At least 75% of the site's frontage shall be occupied by a building.

Figure: Urban Commercial Cross-section



APPLIES TO ALL DEVELOPMENT ON STREETS:



Fig. B furnishing zone with trees, lights, benches, trash receptacles and decorative paving



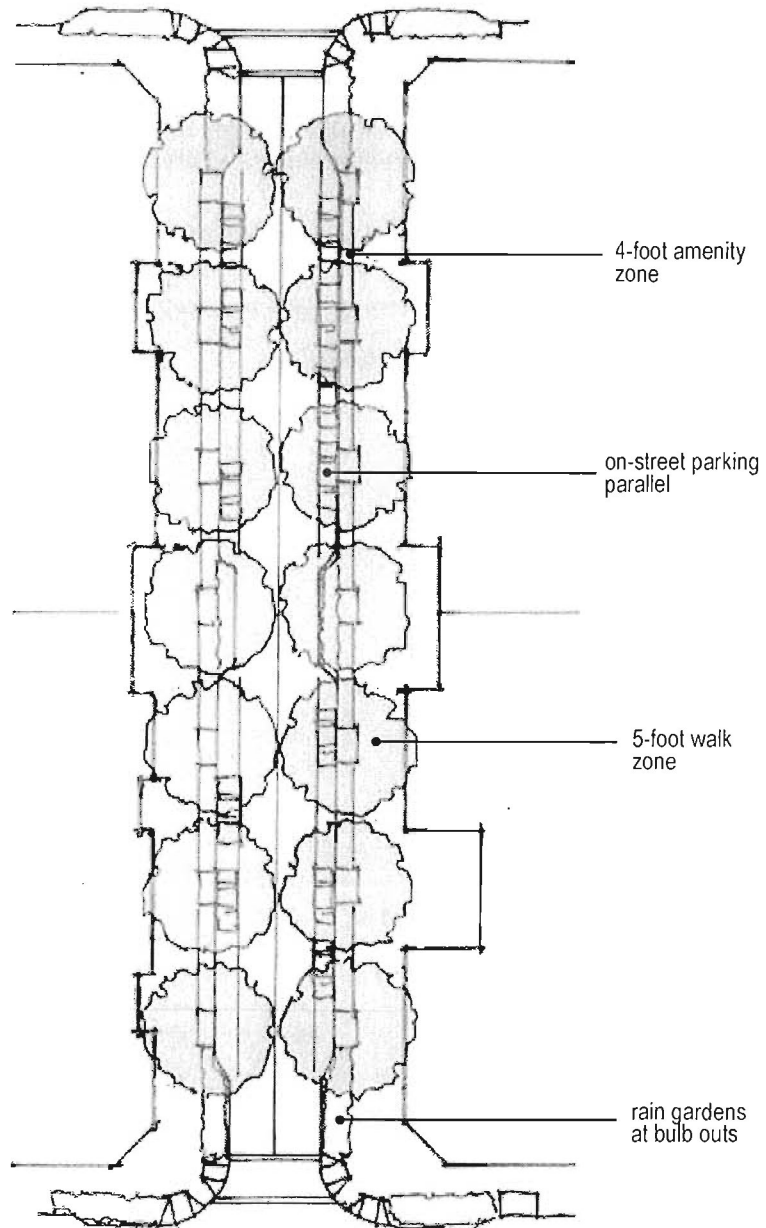
Fig. B.5 generous transparency and overhead weather protection



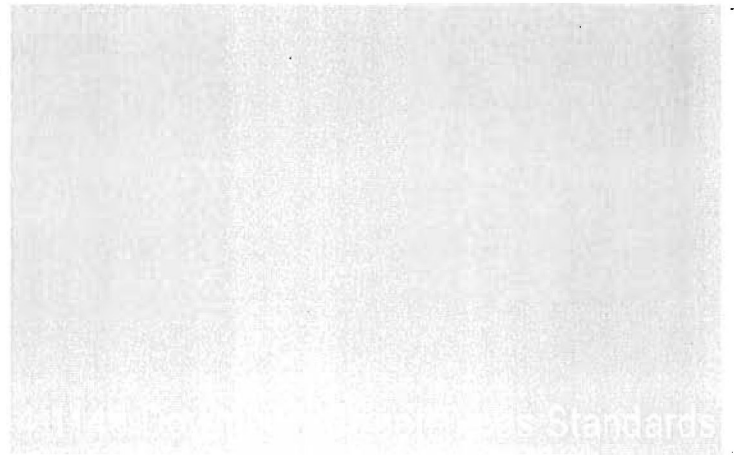
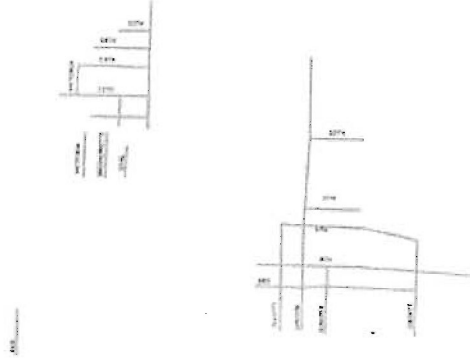
Fig. B.2 with 5-foot walk zone

5. A minimum of 60% of the ground-floor level shall be transparent with visibility into and out of the building for commercial uses.
6. A minimum of 40% of the ground-floor level shall be transparent with visibility into and out of the building for residential uses.
7. No parking shall be permitted on any corners facing public streets.
8. The Gresham Standard 14-foot height Downtown Pedestrian Street Light is required as detailed in the Public Works Standards.
9. If on street parking is included, curbed bulb-outs at the street intersections are required.
10. Overhead weather protection shall be provided that is at least 4 feet in depth, that is a minimum of 9 feet above grade and that corresponds to the first floor height.
11. No curb cuts are permitted without permission of the City and shall be allowed only in instances that permit no other options.

Figure: Urban Commercial Plan View



APPLIES TO ALL DEVELOPMENT ON STREETS:

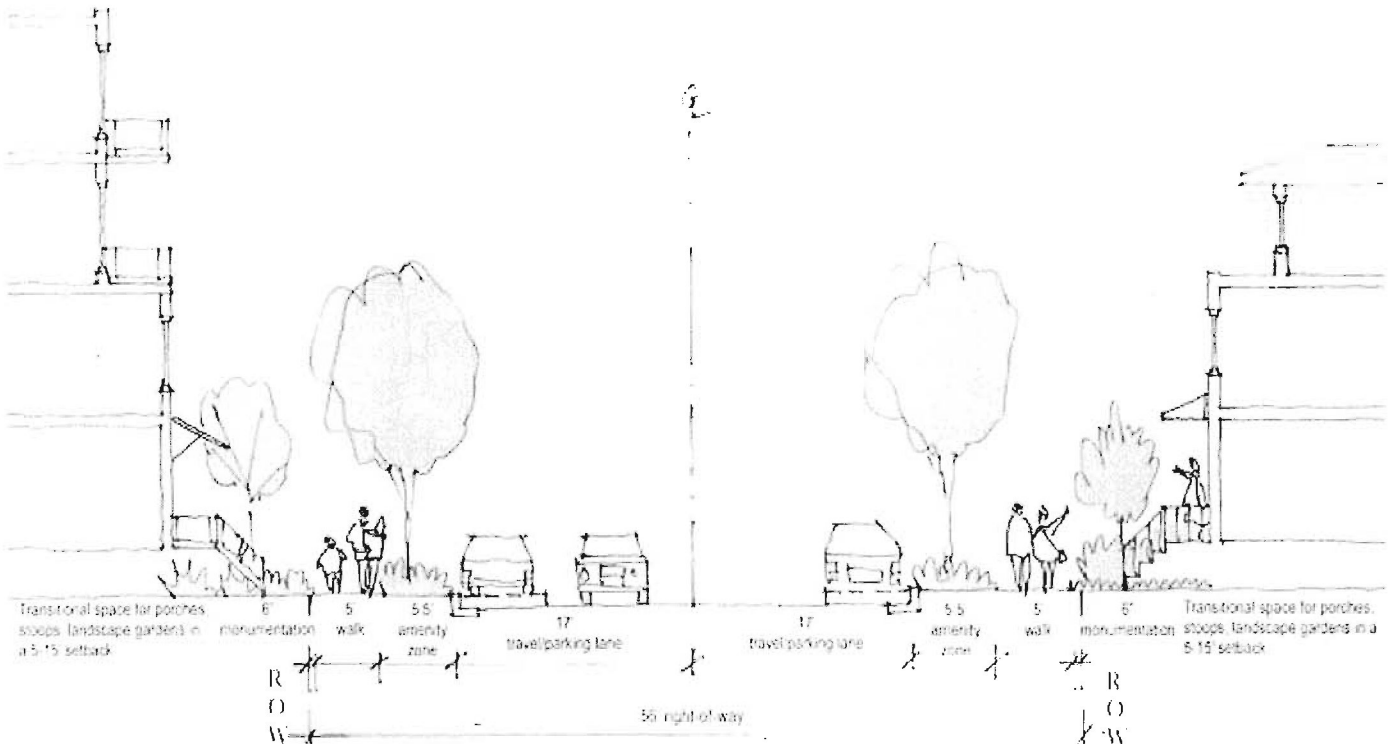


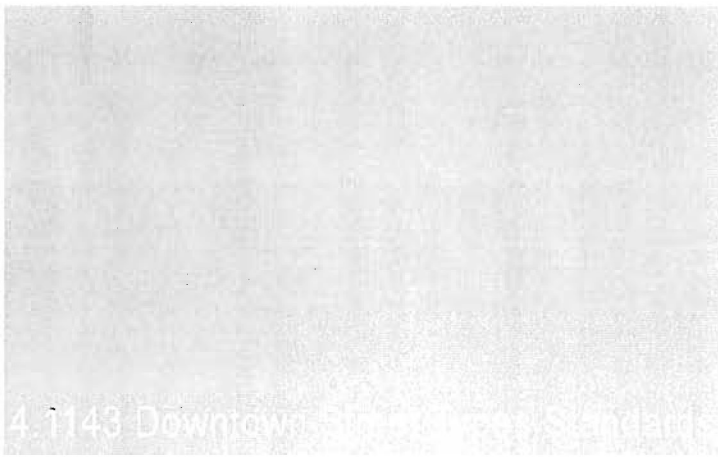
C. Urban Residential Street

For the Urban Residential Street Type, all the following Street Type Standards shall be met:

1. Applicants shall consult specific street engineering construction standards in the Public Works Standards and the Green Streets Standards in combination with these Street Type Standards and street standards included elsewhere in the Gresham Community Development Code.
2. There shall be a clear accessible walking route of 5 feet provided.
3. There shall be a 5.5-foot amenity zone provided with planted understory landscaping. This amenity zone may consist of street trees, street lighting, landscaping and/or seating.
4. At least 75% of the site's frontage shall be occupied by a building.
5. A minimum of 40% of the ground-floor level shall be transparent with visibility into and out of the building.
6. No parking shall be permitted on any corners facing public streets.

Figure: Urban Residential Cross-section





APPLIES TO ALL DEVELOPMENT ON STREETS:



Fig. C.3 transitional stoops and planting

- 7. Transitional setback spaces shall allow porches, stoops, small front yards, landscaping and gardens.
- 8. The Gresham Standard 14-foot height Downtown Pedestrian Light is required as detailed in the Public Works Standards.

Figure: Urban Residential Plan View

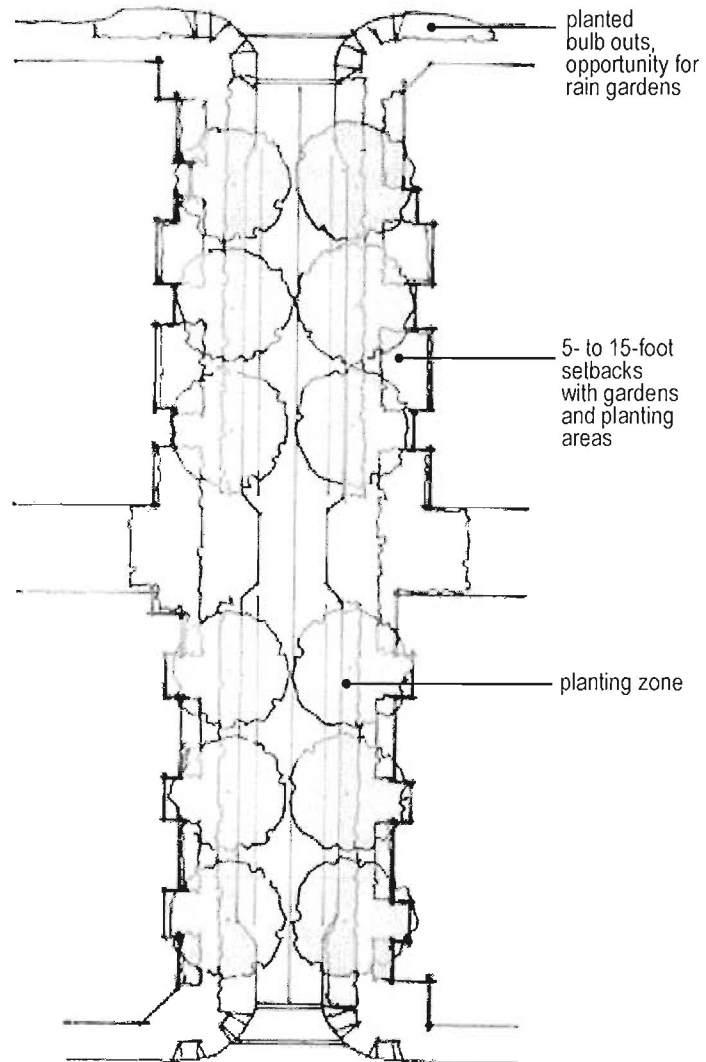
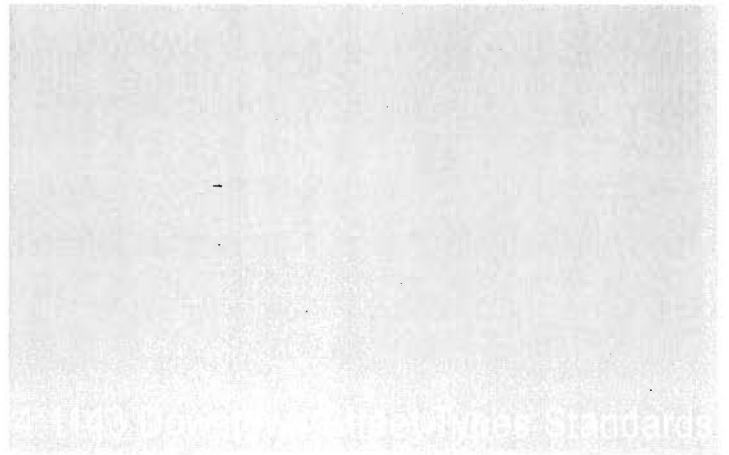
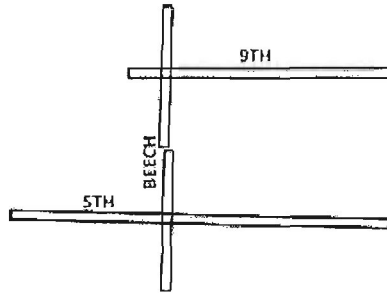


Fig. C residential setback

APPLIES TO ALL DEVELOPMENT ON STREETS:

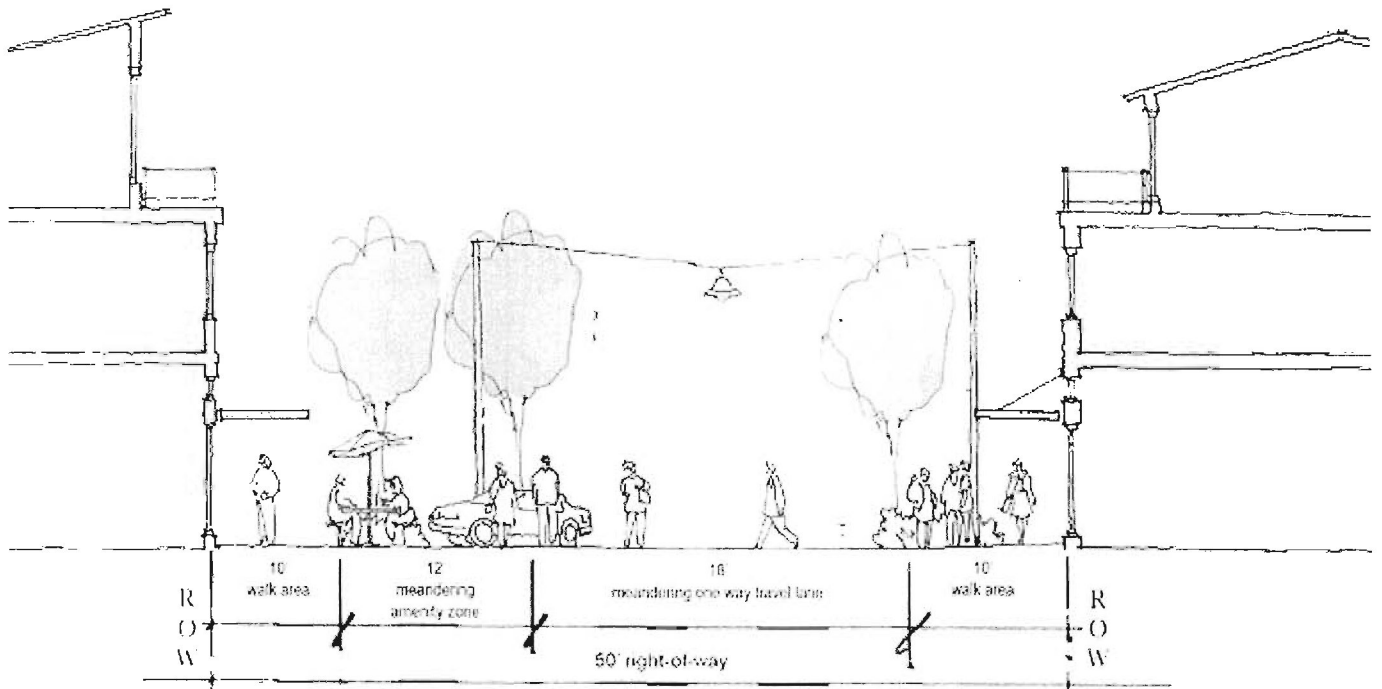


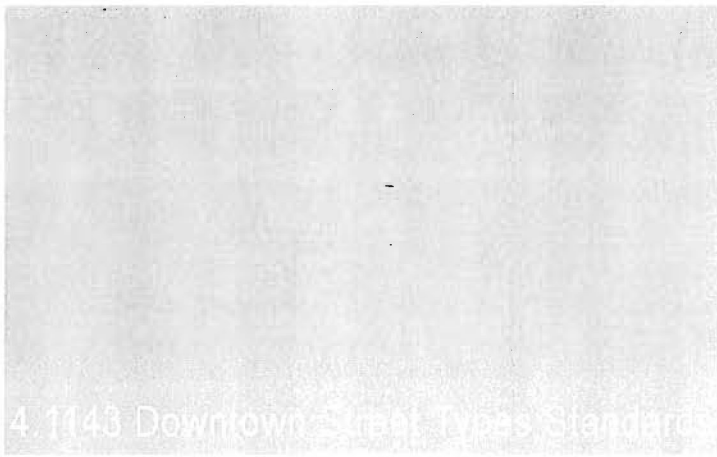
D. Green/Shared Street

For the Green/Shared Street Type, all the following Street Type Standards shall be met:

1. Applicants shall consult specific street engineering construction standards in the Public Works Standards and the Green Streets Standards in combination with these Street Type Standards and street standards included elsewhere in the Gresham Community Development Code.
2. There shall be a clear accessible walking route of 9 feet provided. The pedestrian walk width shall be 10 feet. Appurtenances can protrude 1 foot into the walk width.
3. There shall be a 12-foot meandering amenity zone provided. This amenity zone may consist of large, heavily planted street trees, street lighting, landscaping and/or seating.
4. A minimum of 30% of the ground-floor level shall be transparent with visibility into and out of the building.
6. At least 75% of the site's frontage shall be occupied by a building.
7. No parking shall be permitted on any corners facing public streets.
8. The Gresham Standard 14-foot height Downtown Pedestrian Light is required as detailed in the Public Works Standards. Special additional street lighting shall be approved by the City Department of Environmental Services.

Figure: Green/Shared Cross-section





APPLIES TO ALL DEVELOPMENT ON STREETS:



Fig. D.3 large planted trees

- 9. Curbs shall be de-emphasized or eliminated.
- 10. Special paving materials shall be provided throughout the street as approved by the Department of Environmental Services.
- 11. Rain gardens, valley gutters or appropriate storm water treatment facilities shall be provided.
- 12. Access to single-family attached dwellings is prohibited directly from Green/Shared streets. Access from rear alleys or side streets is allowed.

Figure: Green/Shared Plan View

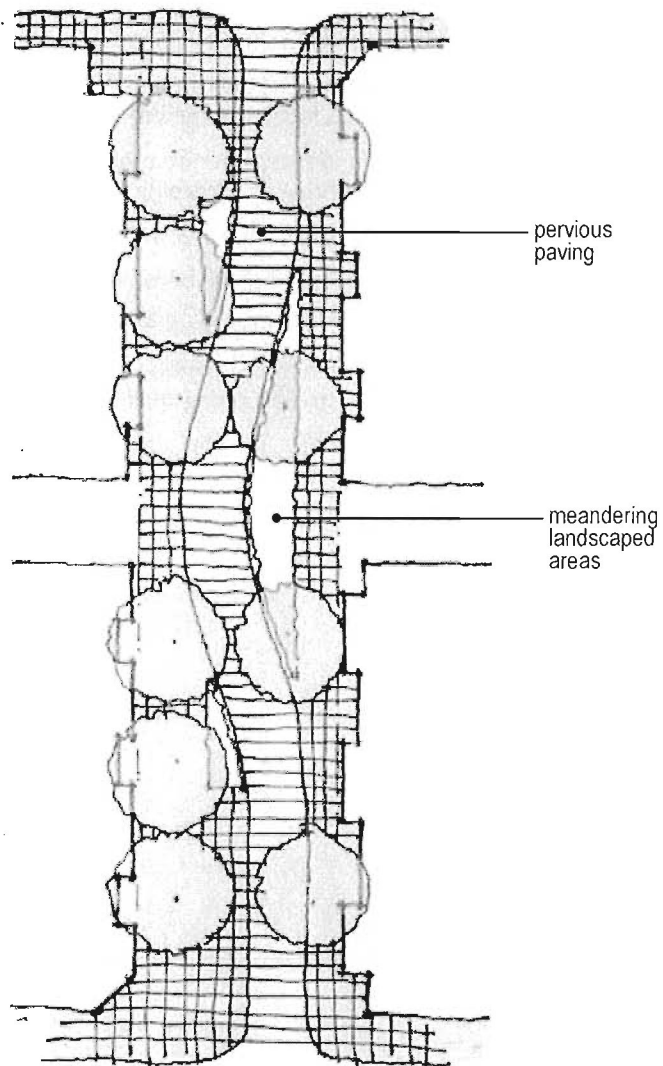
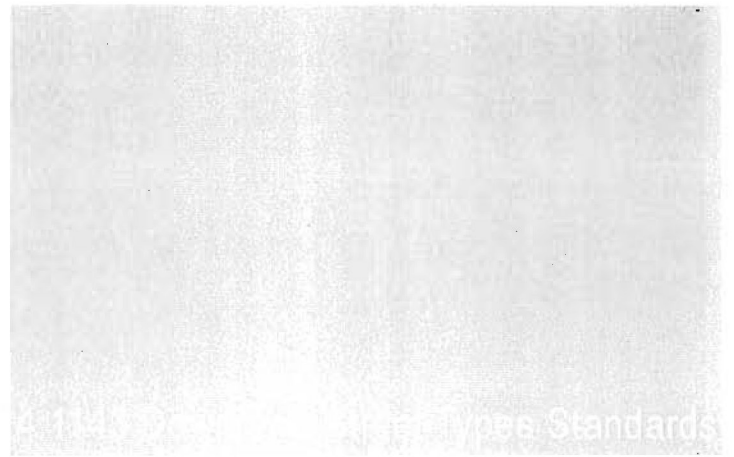
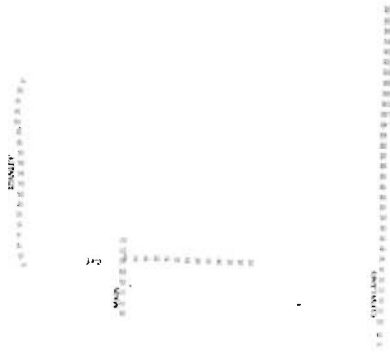


Fig. D.11 street runnels and special paving



Fig. D.9 curbless street

APPLIES TO ALL DEVELOPMENT ON STREETS:



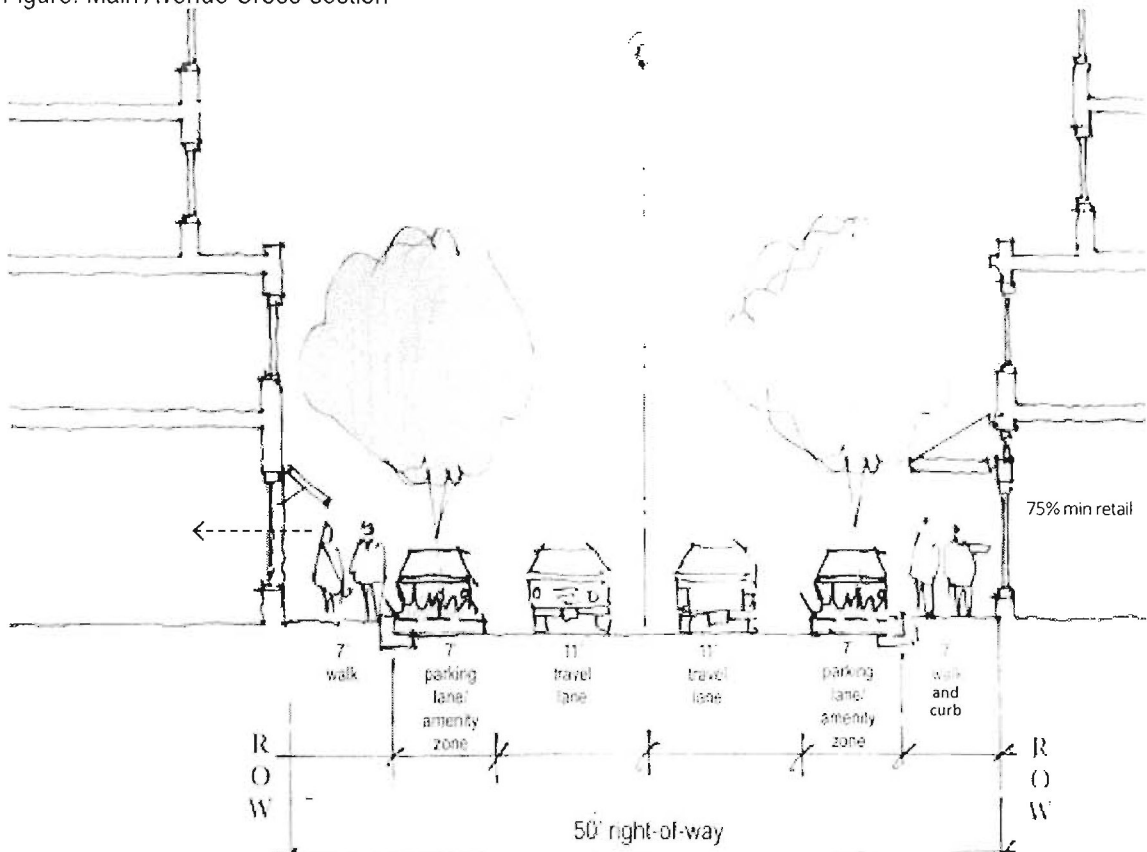
E. Main Avenue

(Powell Boulevard north to 4th Street)

For the Main Avenue Street Type, all the following Street Type Standards shall be met:

1. Applicants shall consult specific street engineering construction standards in the Public Works Standards and the Green Streets Standards in combination with these Street Type Standards and street standards included elsewhere in the Gresham Community Development Code.
2. There shall be a clear minimum accessible walking route width of 4.5 feet. The pedestrian walk width shall be 6.5 feet. Appurtenances can protrude 1 foot into the walk width.
3. At least 90% of the site's frontage shall be occupied by a building.
4. A minimum of 75% of the ground-floor level shall be transparent with visibility into and out of the building and 75% of the ground floor level shall include street-level commercial.
5. No parking shall be permitted on any corners facing public streets. No parking access or loading access shall be provided directly from this street. No parking is allowed between the building and the street.

Figure: Main Avenue Cross-section



4.1143 Downtown Street Design Standards

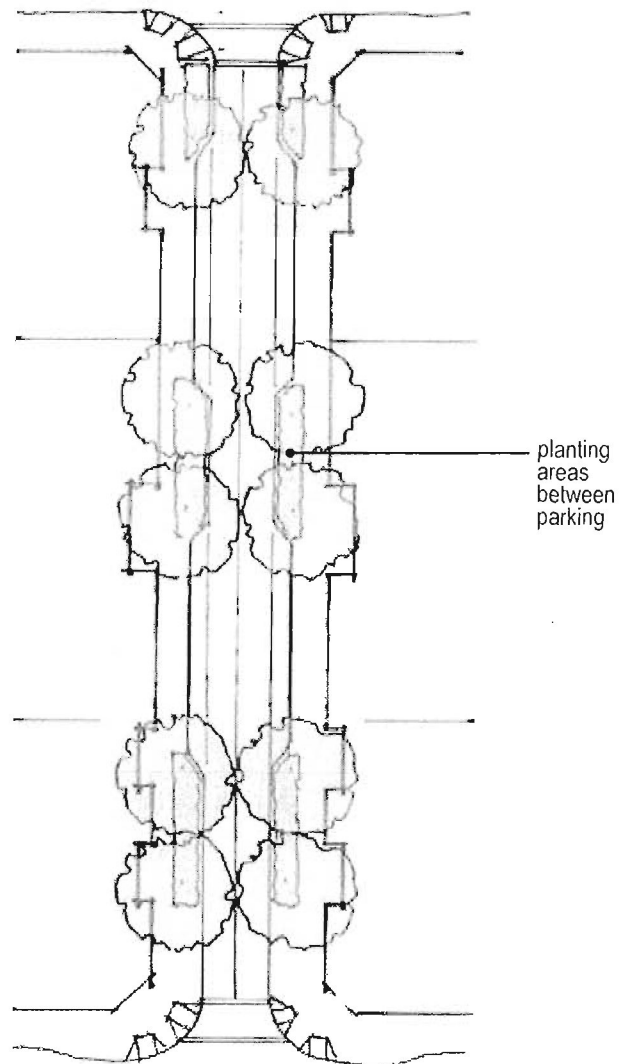
APPLIES TO ALL DEVELOPMENT ON STREETS:



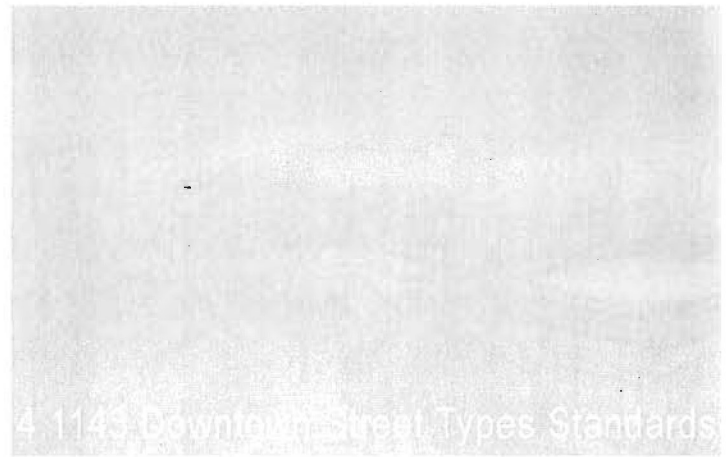
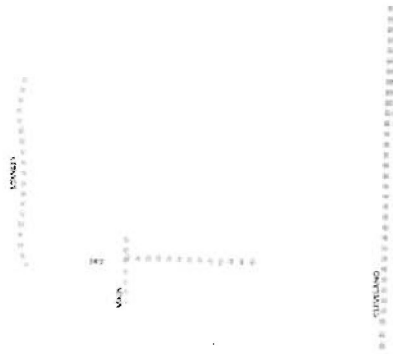
Fig. E. Main Ave

7. The Gresham Standard 14-foot height Downtown Pedestrian Light is required as detailed in the Public Works Standards.
8. Amenities, including street trees, will be located outside of the accessible route in the required bulb-out areas near the street intersections.
9. Overhead weather protection shall be provided that is at least 4 feet in depth, that is a minimum of 9 feet above grade and that corresponds to the first-floor height.

Figure: Main Avenue Plan View



APPLIES TO ALL DEVELOPMENT ON STREETS:



F. Stanley Avenue:

For the Stanley Shopping Street Type, all the following Street Type Standards shall be met:

1. Applicants shall consult specific street engineering construction standards in the Public Works Standards and the Green Streets Standards in combination with these Street Type Standards and street standards included elsewhere in the Gresham Community Development Code.
2. There shall be a clear accessible walking route of 9 feet provided. The pedestrian walk width shall be 10 feet. Appurtenances can protrude one foot into the walk width.
3. There shall be a 6-foot amenity zone provided. This amenity zone may consist of street trees, street lighting, landscaping and/or seating.
4. A minimum of 90% of the building shall be placed on the minimum required front setback line.
5. A minimum of 75% of the ground-floor level shall be transparent with visibility into and out of the building and 75% of the ground-floor level shall include street-level commercial.
6. No parking shall be permitted on any corners facing public streets. No parking access or loading access shall be provided directly from this street.

Figure: Stanley Cross-section

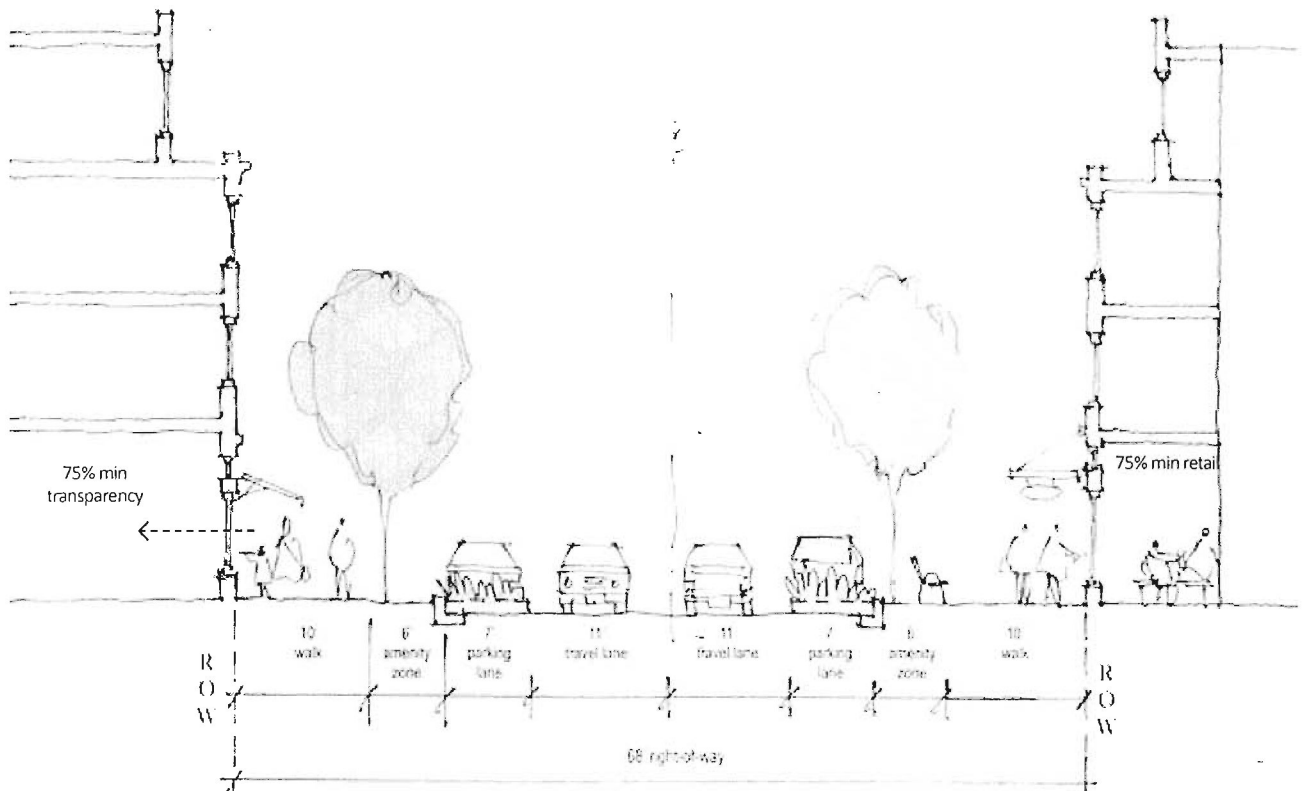
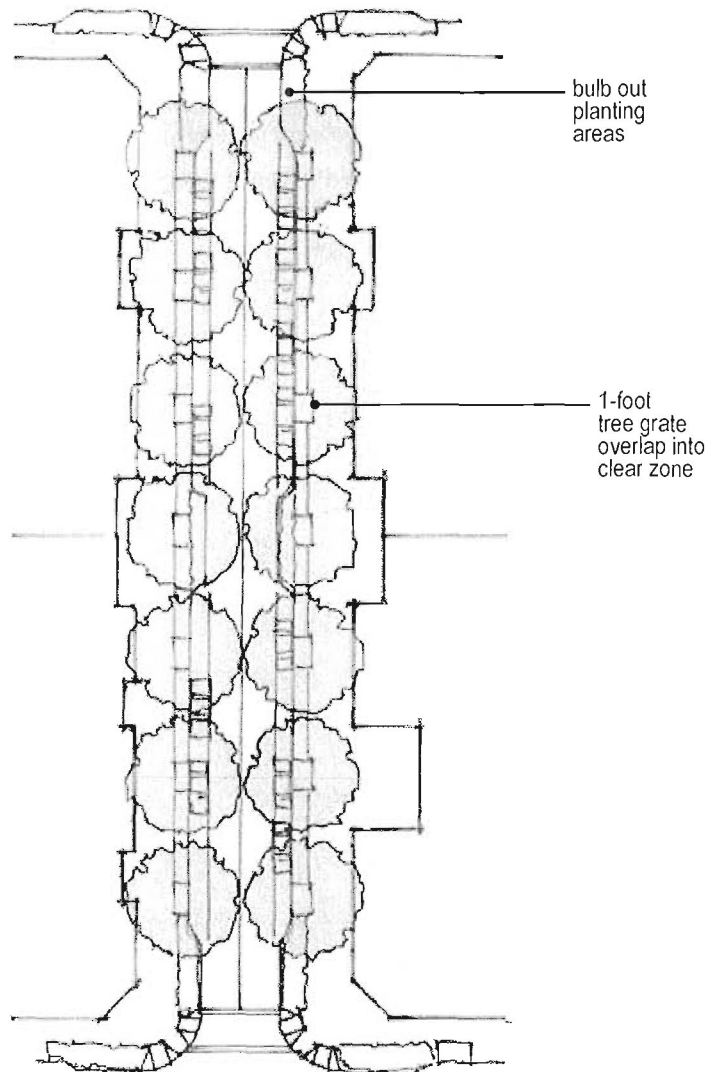




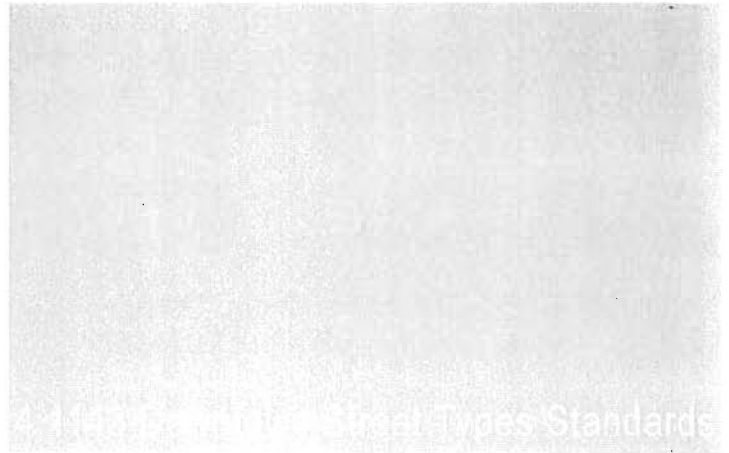
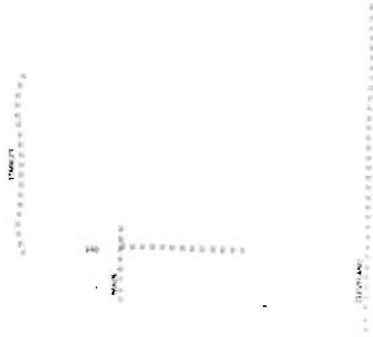
Fig. E.2 envisioned new Stanley Street in Town Fair District

Figure: Stanley Plan View

7. The Gresham Standard 14-foot height Downtown Pedestrian Light is required as detailed in the Public Works Standards.
8. If on street parking is included, curbed bulb-outs at the street intersections are required.
9. Overhead weather protection shall be provided that is at least 4 feet in depth, that is a minimum of 9 feet above grade and that corresponds to the first-floor height.



APPLIES TO ALL DEVELOPMENT ON STREETS:

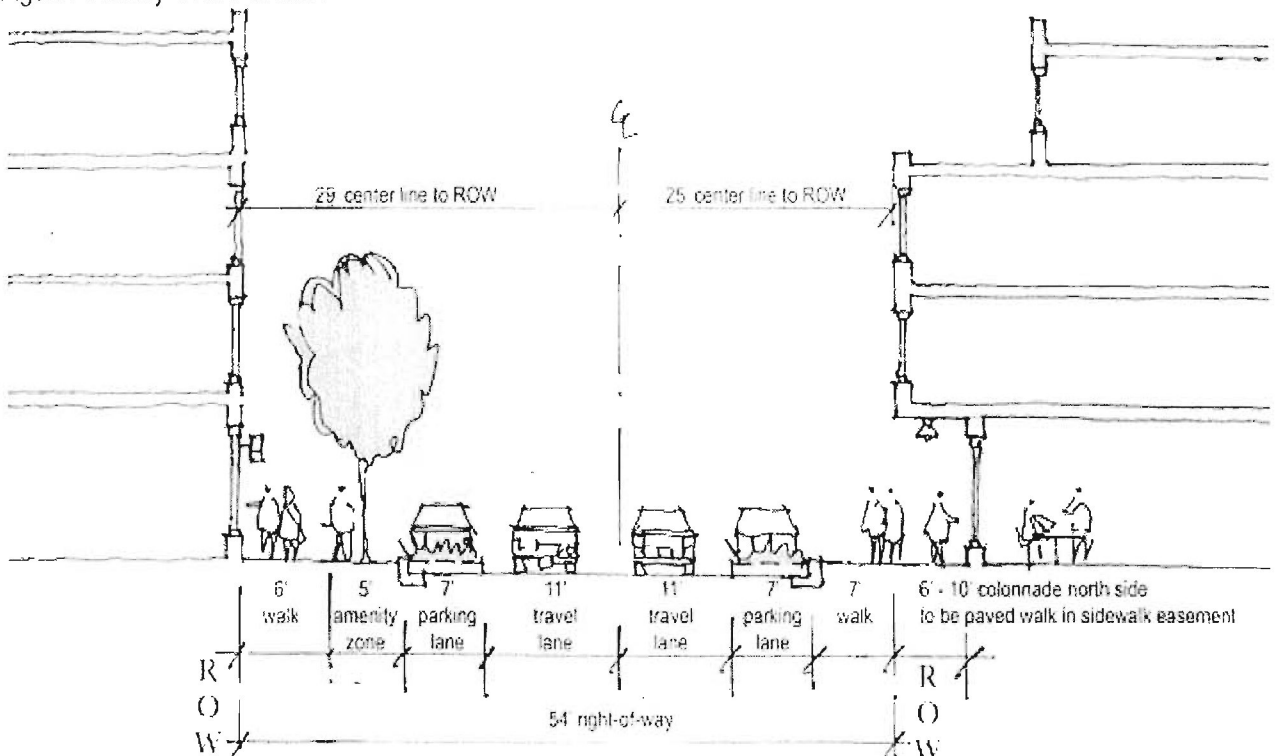


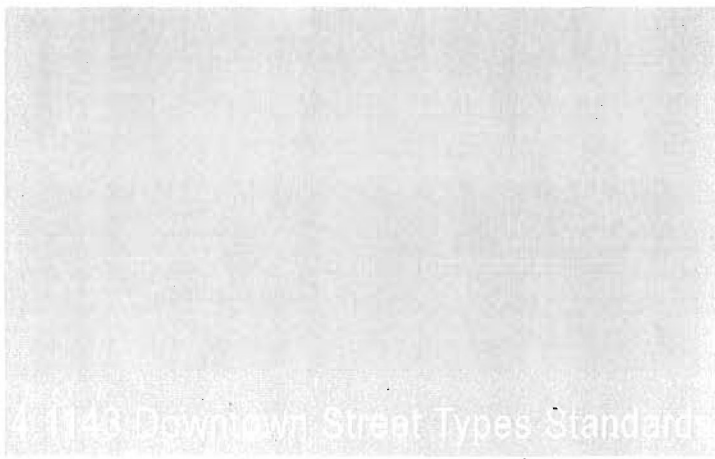
G. Third Street

For the Third Street Type, all the following Street Type Standards shall be met:

1. Applicants shall consult specific street engineering construction standards in the Public Works Standards and the Green Streets Standards in combination with these Street Type Standards and street standards included elsewhere in the Gresham Community Development Code.
2. There shall be a minimum clear accessible walking route of 5 feet provided on the north and south sides of the street. The pedestrian walk width shall be 6 feet on the south side of the street and 7 feet on the north side. Appurtenances can protrude 1-foot into the walk width. The area under the colonnade shall also be paved and shall be 6 feet to 10 feet wide.
3. There shall be a 4-foot amenity zone provided on the south side of the street. This amenity zone may consist of street trees, street lighting, landscaping and/or seating.
4. A colonnade is required on the north side of the street. The colonnade shall be a minimum depth of 6 feet with a maximum depth of 10 feet. The colonnade height shall be 16 feet. Decorative pedestrian-scale lighting is required under the overhang of the colonnade.
5. At least 90% of the site's frontage shall be occupied by a building.
6. A minimum of 75% of the ground-floor level shall be transparent with visibility into and out of the building and 75% of the ground-floor level shall include street level commercial.

Figure: Stanley Cross-section





APPLIES TO ALL DEVELOPMENT ON STREETS:

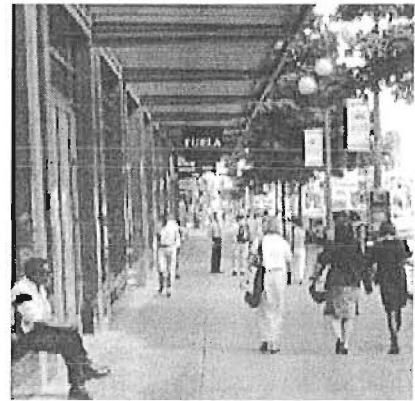


Fig. E.3 (3) continuous overhead weather protection

7. No parking shall be permitted on any corners facing public streets. No parking access or loading access shall be provided directly from this street.
8. The commercial ground-floor height shall be a minimum height of 12 feet to a maximum height of 20 feet on the south side of the street. The north side commercial ground-floor height shall be a minimum height of 16 feet with a maximum of 20 feet.
9. The Gresham Standard 14-foot height Downtown Pedestrian Light is required as detailed in the Public Works Standards.
10. If on street parking is included, curbed bulb-outs at the street intersections are required.
11. Overhead weather protection shall be provided that is at least 4 feet in depth, that is a minimum of 9 feet above grade and that corresponds to the first-floor height.

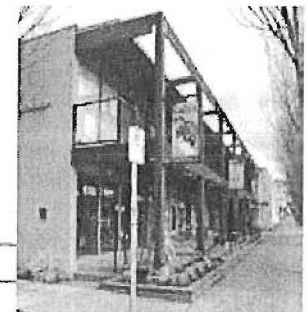


Fig. E.3 (2) Cor-ten steel portico adds to the three-dimensional quality of this commercial building

Figure: Stanley Plan View

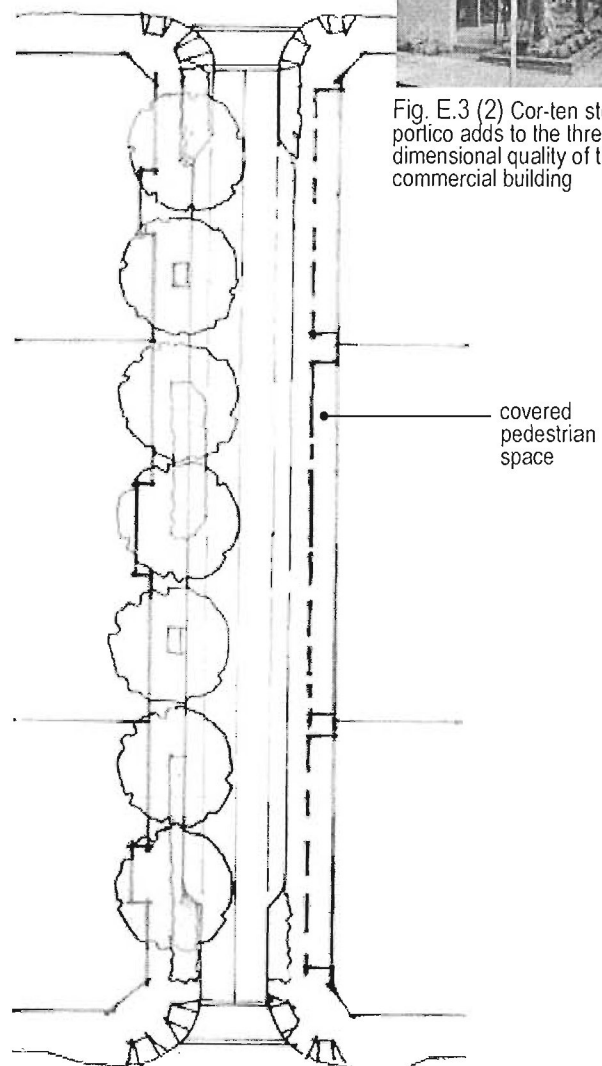
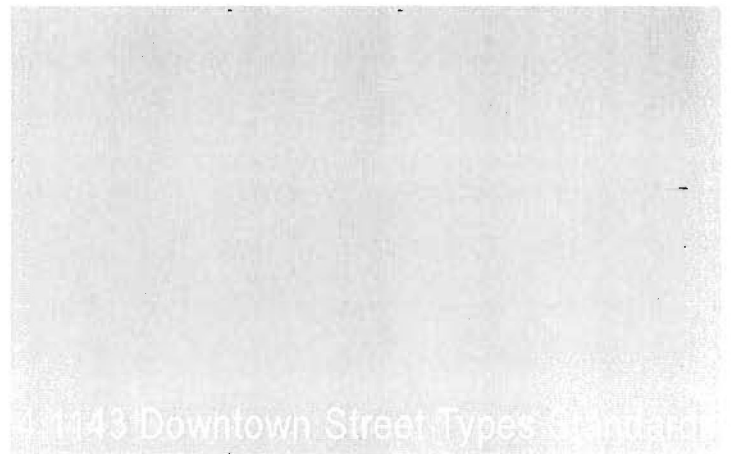
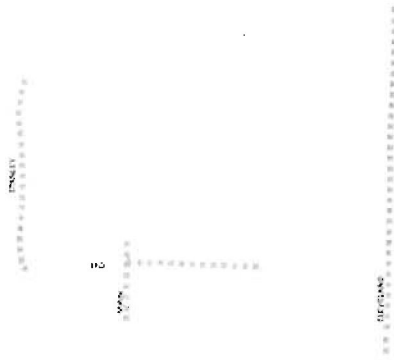


Fig. E.3 (1) colonnade lined with shops

APPLIES TO ALL DEVELOPMENT ON STREETS:

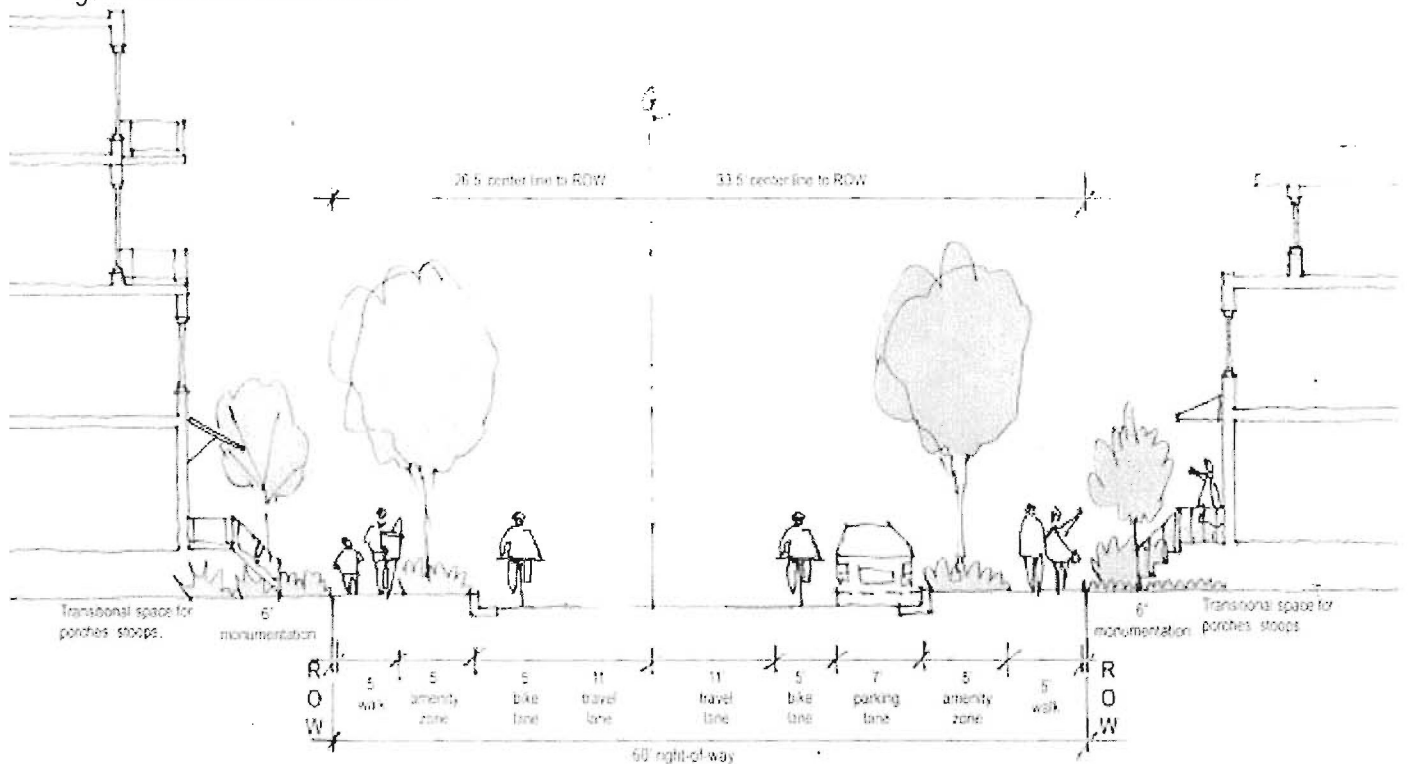


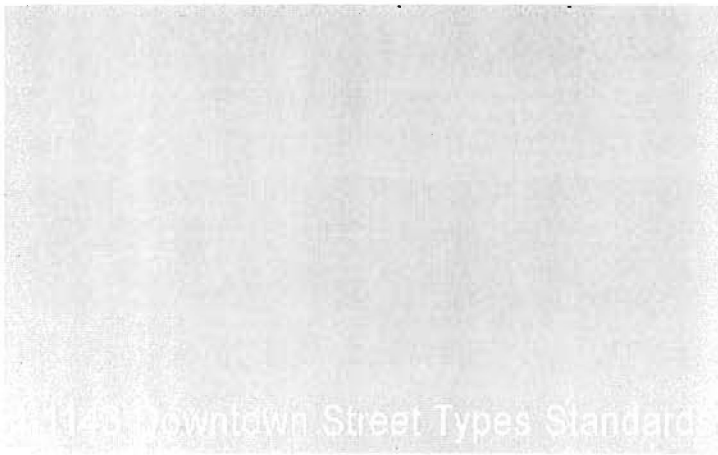
H. Cleveland Avenue:

For the Cleveland St. Street Type, all the following Street Type Standards shall be met:

1. Applicants shall consult specific street engineering construction standards in the Public Works Standards and the Green Streets Standards in combination with these Street Type Standards and street standards included elsewhere in the Gresham Community Development Code.
2. There shall be a minimum clear accessible walking route of 5 feet provided.
3. There shall be a 5-foot amenity zone provided. This amenity zone may consist of street trees, street lighting, landscaping and/or seating.
4. A 5-foot bike lane shall be provided on both sides of the street.
5. The cross-section does not apply to the section of Cleveland Avenue from Powell north to 2nd Street and the MAX tracks north to Burnside. In these areas, a 12-foot left-turn lane will be required with an additional right-of-way dedication on each side of 2.5 feet. There will be no parking lane in these areas.
6. At least 75% of the site's frontage shall be occupied by a building.
7. A minimum of 40% of the ground-floor level shall be transparent with visibility into and out of the building.
8. Transitional setback spaces shall allow porches, stoops, small front yards, landscaping and gardens.

Figure: Cleveland Cross-section





APPLIES TO ALL DEVELOPMENT ON STREETS:



Fig. E.4.h residential transitional spaces

9. The Gresham Standard 14-foot height Downtown Pedestrian Light is required as detailed in the Public Works Standards.
10. If on street parking is included, curbed bulb-outs at the street intersections are required. Rain gardens are required in bulb-outs.
11. This Street Type without a center turn lane applies from 2nd Street north to the MAX transit line. A center turn lane of 12 feet will be required from Powell Boulevard-north to 2nd Street and from the MAX Transit line north to Burnside. Where the turn lane is required, an additional right-of-way dedication of 2.5 feet is required and there will be no parking lane.

Figure: Cleveland Plan View

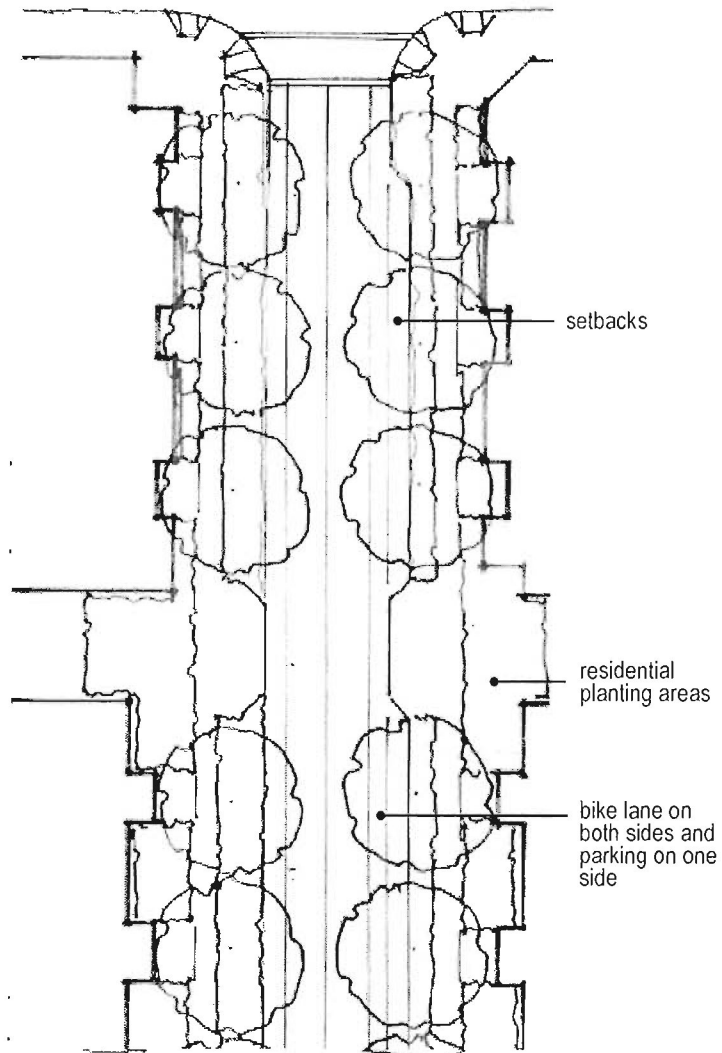


Fig. E.4.j rain gardens in planting area

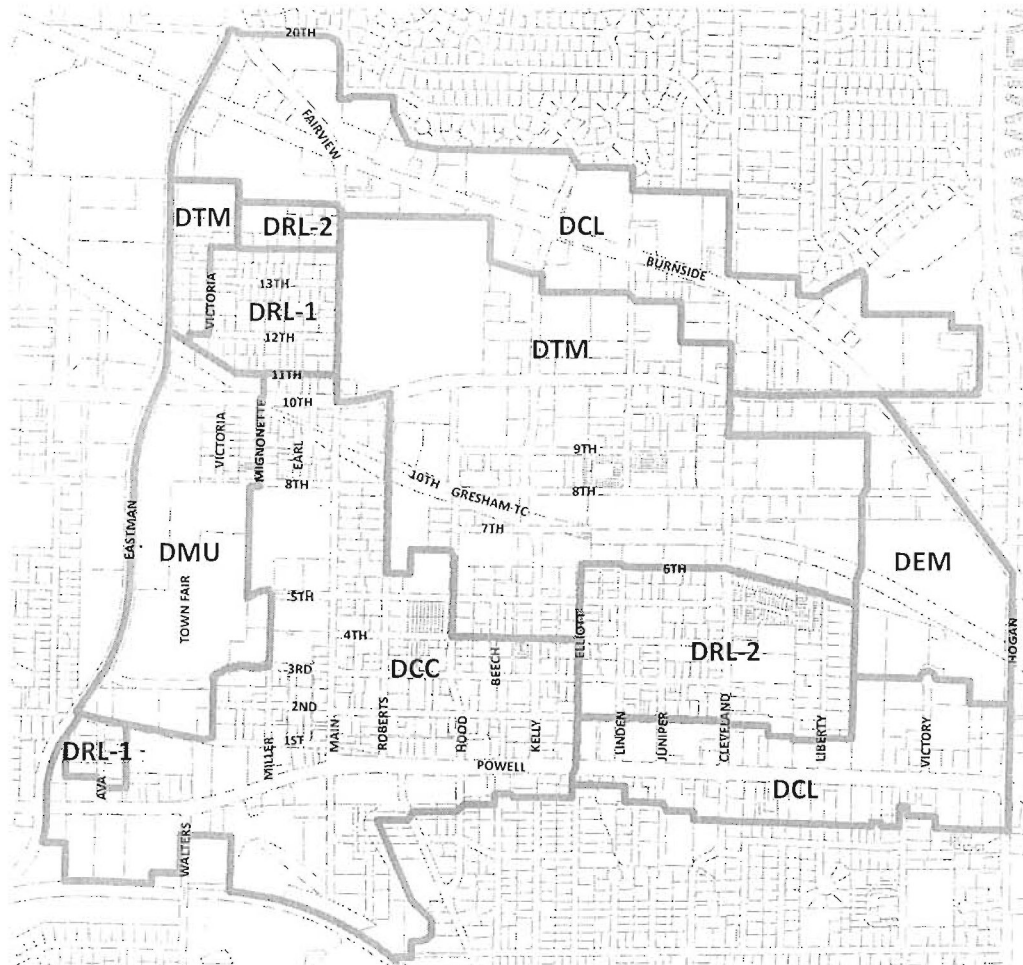
Design Guidelines and Standards: All Downtown Sub-Districts

4.1151. Design Guidelines and Standards: All Downtown Sub-Districts Purpose

Each project is subject to either the Downtown-Wide Design Standards or Guidelines, depending on the project's review track. The Downtown-Wide Guidelines and Standards set the criteria and provide measurable or descriptive ways depending on the review process chosen for evaluating how well the project meets the design principles established for Downtown.

The Guidelines and Standards are organized under two principal categories, site design and building design. Each general category includes basic topics, such as Integrated Site Planning or Building Massing. Each of these includes the design principles applicable to the specific topic, and a list of guidelines (descriptive methods and techniques to meet the intent of the design principles) and standards (clear and objective, measurable requirements to carry out the design principles).

This section does not apply to single-family detached or two-unit attached dwellings, unless specified in 4.1130. Guidelines and standards under the "All Development" heading or without a heading apply to all developments except single-family detached, two-unit attached, and single-family attached dwellings. Those under the "Non-Residential" heading apply to all development that are not residential, such as commercial, mixed-use, live-work, and employment uses. Those under the "Residential" heading apply only to attached dwellings on a single lot, unless otherwise specified.



1. Integrated Site Planning

- a. **Intent:** To ensure that the placement of buildings, parking, service access, plazas and courtyards recognize the important relationship between public and private space in creating a walkable and pedestrian-friendly environment.
- b. **Applicable Downtown Design Principles:**
 - A. Provide a Vibrant Mix of Uses.
 - C. Create a Vibrant Public Realm.
 - D. Create strong connections between Plan Districts and Sub-Districts.
 - F. Context Sensitivity.
 - G. Preserve and Enhance Historic Character.

c. Guidelines

All Development

1. Buildings shall be located with the principal façade oriented to the sidewalk.
2. Site parking and loading services shall be located so as to allow desired uses and activities to face the street and to support pedestrian-oriented streets.
3. The majority of parking shall be located to the rear or in structures to support pedestrian-oriented streets.
4. The pedestrian shall have principal access into the building from the sidewalk. Clear pedestrian connections shall be established on-site that are well-marked and gracious; for example, direct pedestrians from surface or structure parking to buildings, streets, and public spaces.
5. Service items such as loading docks, mechanical equipment, and garbage dumpsters shall be buffered from pedestrian areas. Enclosing and integrating these items into the building is strongly encouraged.



Fig. A.1.1 & 4 principal facade set on the back of the sidewalk

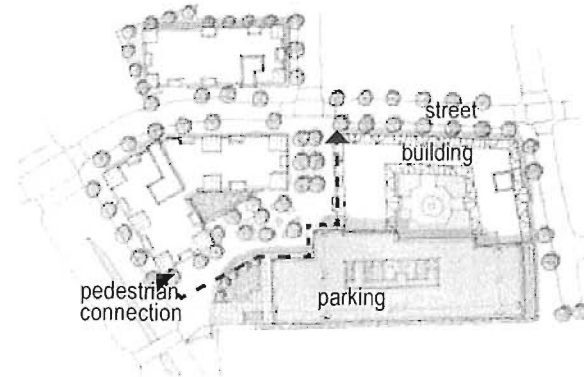


Fig. A.1.c.2 & 4 site plan with access to parking behind buildings fronting the street and connections through the site to surrounding streets



Fig. A.1.c.4 & 5 principal building entrance from sidewalk with a gracious entry court

APPLIES TO ALL DOWNTOWN DEVELOPMENT

4.1151 Design Guidelines and Standards: All Downtown Sub-Districts
A. Site Design



Credit: Nancy Smith

Fig. A.1.c.6.a. Consider how the arrangement of buildings, access, parking, open spaces and landscaping on a site can take advantage of special views

1. Integrated Site Planning, cont'd

6. View Opportunities. Views of nearby natural features such as Mount Hood to the east and surrounding bluffs and buttes, contribute to Gresham's identity. New buildings will inevitably limit views from other buildings and surrounding sites. The purpose of this Guideline is to provide site planning methods and techniques to ensure that reasonable care has been taken to avoid obstructing valued views, where applicable.
 - a. Orient buildings, landscaping and other site features in ways that help maintain existing views along public rights-of-way and within the Downtown Plan District area.
 - b. Take advantage of opportunities to enhance open spaces on a site, such as plazas and terraces oriented to maximize views of significant features. Site design shall take into account the relationship between the development and natural features and points of interest. Place buildings in ways that incorporate visual connections to adjacent public spaces, and physical and natural landmarks.

1. Integrated Site Planning, cont'd

Residential & Single-family Attached

7. A shallow setback and minor grade separation between the first floor and the street level where residential uses occupy the first floor should be considered to create layers (such as gardens, stoops, and porch railings) that mark the transition between public and private spaces.

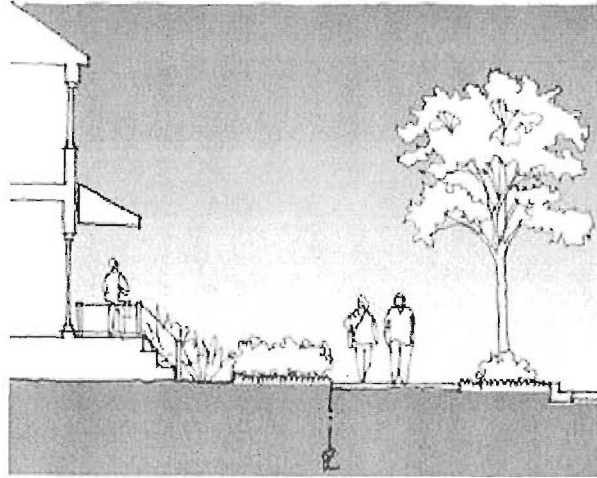


Fig. A.1.c.7 layers of landscaping, stoops, and railing creating an appropriate transition from residence to the street

APPLIES TO ALL DOWNTOWN DEVELOPMENT

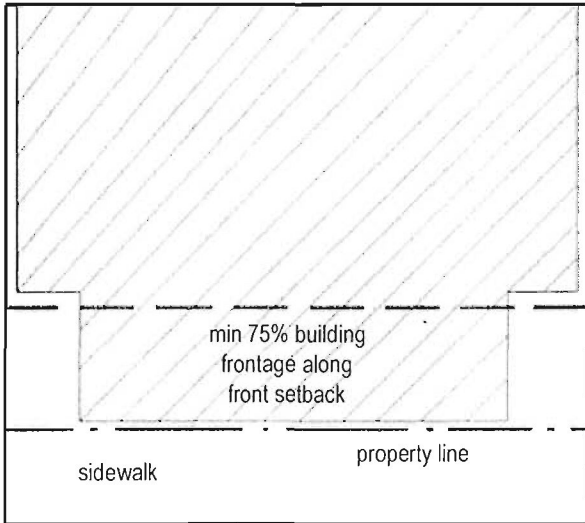


Fig. A.1.d.1 Commercial building placement along primary street

1. Integrated Site Planning, cont'd

d. Standards

All Development

1. In each development at least 75% of the minimum front setback shall be lined with a building face, unless otherwise specified in the Street Type. Refer to setback and frontage standards in Section 4.1130 Downtown Plan Sub-District Standards and Section 4.1140 Street Types for additional building frontage requirements on specific streets.
2. Parking and loading services shall be located to the side, interior or rear of the site or within or to the rear of the building. Parking located to the side of the building shall be limited to 25% of the frontage, except in DCL where parking can occupy 50% of the frontage.
3. Pedestrians shall be separated from nuisances such as noise and odors. Loading docks, mechanical equipment, garbage dumpsters and other such items shall be enclosed and located away from pedestrian areas and kept to service areas or alleys.

Non-Residential

4. In commercial development, building faces shall directly abut the sidewalk, with no intervening landscaping.

All Development Except Single-family Attached

5. Principal entrances to buildings shall be directly from the public, street side adjacent sidewalk.

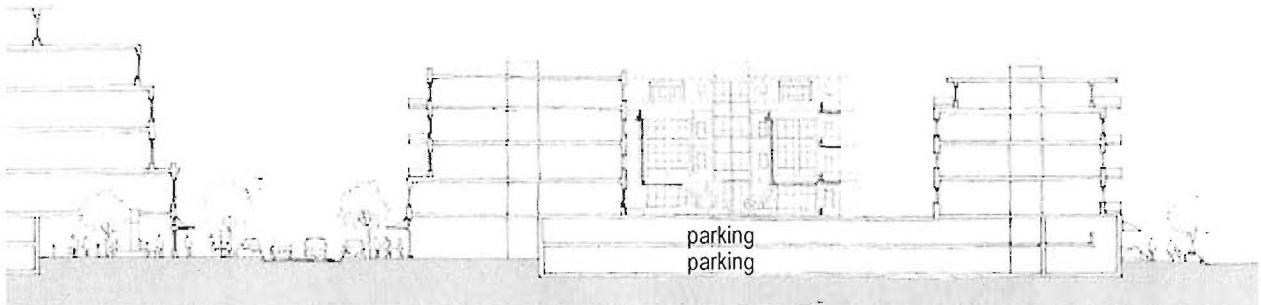


Fig. A.1.d.2. Structured parking within the building is the preferred location for parking on a site

1. Integrated Site Planning, cont'd

All Developments

6. View Opportunities. Views of nearby natural features such as Mount Hood to the east and surrounding bluffs and buttes, contribute to Gresham's identity. As the Downtown redevelops, there may be valued views to significant buildings. New buildings will inevitably limit views from other buildings and surrounding sites. The purpose of the following Standards is to provide site planning methods and techniques to ensure that reasonable care has been taken to avoid obstructing valued views of significant natural or man-made views.
 - a. A view corridor study shall be provided at the Manager's discretion in instances where significant views are present.
 - b. Developments shall be designed to preserve significant views of existing natural or man-made features or enhance new views to significant Downtown features. Developments shall not obstruct all views to significant natural features. This site design requirement to address view corridors includes the following:
 - i. Building placement on site;
 - ii. Site landscaping height and placement on site; and
 - iii. Open space placement on site.
 - c. If potential views to noteworthy natural features and points of interest exist (either nearby or in the distance) from the development site, entrances and publicly accessible open spaces shall be located and oriented to take advantage of this view.

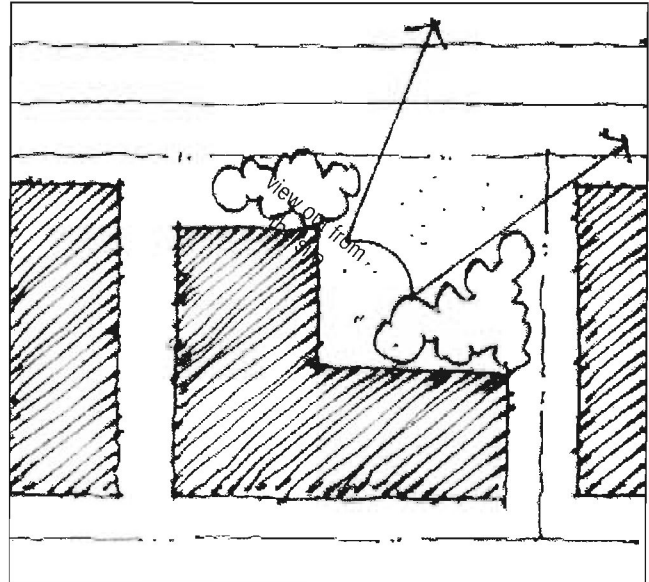


Fig. A.1.d.6.b. arrange buildings, landscaping and open space on the site to enhance views

APPLIES TO ALL DOWNTOWN DEVELOPMENT

1. Integrated Site Planning, cont'd

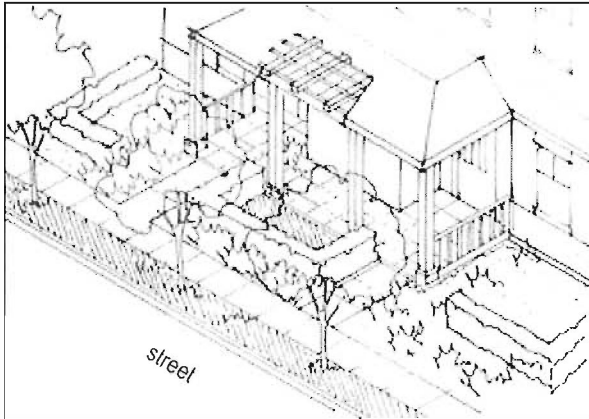


Fig. A.1.d.7. Residential development with a shallow setback from the street for multiple entries and small gardens

Residential

7. Building faces may be located up to 15 feet behind the property line to allow for gardens, stoops or steps, where allowed. Fully 75% of the required front setback shall be lined with a building face (excluding single-family detached development, two-unit attached development, and single-family attached development).

Residential & Single-Family Attached

8. All provisions of 7.0200 Site Design Review Standards and Criteria shall apply, except:
 - 7.0201 (E) (1) and (2); and
 - 7.0201 (G)(6); and
 - 7.0201 (H)(7)
 - 7.0201 (J); and
 - 7.0201 (L)(4); and
 - 7.0201 (K); and
 - 7.0202 (B); and
 - 7.0210(B); and
 - all of 7.0211
9. Deviations from the Downtown Plan District standards or applicable standards of Section 7.0200 may be allowed through the discretionary process as described in 4.1101(A)(1)(a).

2. Publicly Accessible Open Spaces

- a. **Intent:** To provide an enhanced pedestrian experience by creating a network of privately provided and maintained public spaces, courtyards, and other gathering spaces.
- b. **Applicable Downtown Design Principles:**
 - A. Provide a Vibrant Mix of Uses.
 - C. Create a Vibrant Public Realm.
 - D. Create strong connections between Plan Districts and Sub-Districts.
 - E. Incorporate Sustainability.
 - F. Context Sensitivity.

c. Guidelines

All Developments

1. For development on sites larger than 40,000 square feet, provide publicly accessible open space that include a variety of public spaces, both hardscaped and landscaped, such as on-site plazas, interior courtyards, patios, terraces and gardens.
2. Public spaces shall incorporate features that advance sustainable principles, such a use of gray water, solar collection for powering pumps or lighting, rain gardens, pervious paving, containers for recycling, and benches made from recycled materials.
3. Public spaces intended for public use shall be located at/or near street grade to promote physical and visual connection to the street.
4. Public spaces shall include focal points which could consist of art sculpture, water, pavilions, seating areas, specimen plants, unique paving, or unusual lighting.

Non-Residential

5. Public spaces can be located between a building and the sidewalk, so long as active ground floor uses are accessible to and from the space and provided the desired street wall and/or retail continuity is not adversely impacted by the presence of this space. Locate plazas intended for public use at/or near street grade to promote physical and visual connection to the street.
6. Public spaces shall be flanked by uses that activate the space and complement street activity with elements such as shops, outdoor cafes and vendors within the space.



Fig. A.2.c.1 & 5 public on-site plaza incorporating a variety of hardscaped and landscaped treatments to create an engaging space

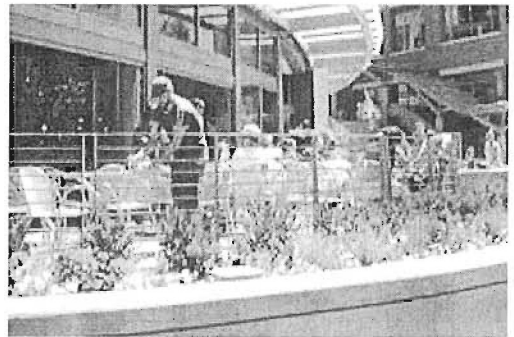


Fig. A.2.c.6 public space lined by outdoor cafe



Fig. A.2.d.4 water feature

APPLIES TO ALL DOWNTOWN DEVELOPMENT

4.1151 Design Guidelines and Standards: All Downtown Sub-Districts A. Site Design

2. Publicly Accessible Open Spaces, cont'd

d. Standards

All Developments

1. For sites larger than 20,000 square feet that are providing public open space, at least 3% of a development site greater than 20,000 square feet shall be allocated and designed as publicly accessible open space, such as a plaza. Public spaces shall be placed at high pedestrian activity areas, with western or southern exposure.
2. Public spaces shall incorporate the following:
 - a. At least 30% of the area shall be planted with trees, shrubs, groundcover and perennial landscape plantings.
 - b. At least 30% of the area shall be hardscaped with decorative paving
 - c. There shall be at least one bench or seating unit for each 200 square feet of area, (seating may be group into benches or ledges)
 - d. Pedestrian-scaled lighting fixtures
 - e. One element with sustainability attributes (rain garden, solar powered lights or equipment, pervious paving, container for recycling, or benches made from recycled materials.)
 - f. Artistic design elements such as decorative paving patterns, ornamental art features, creative lighting elements, etc.
3. Public spaces shall be accessible at grade adjacent to the sidewalk to promote physical and visual connection to the street. Portions of public spaces such as plazas may be above or below grade to accommodate a variety of outdoor gathering spaces.

Non-Residential

4. Street facing public spaces shall not exceed 20% of the total building footprint or 2000 square feet, whichever is less. Such space shall have a minimum width of 30 feet and a minimum depth of 20 feet, unless otherwise approved by the Manager due to site constraints.
5. Public spaces located between a building and a sidewalk shall be abutted on at least two sides by retail shops, restaurants, primary residential or office entrances or services with their windows and doors fronting on the space.

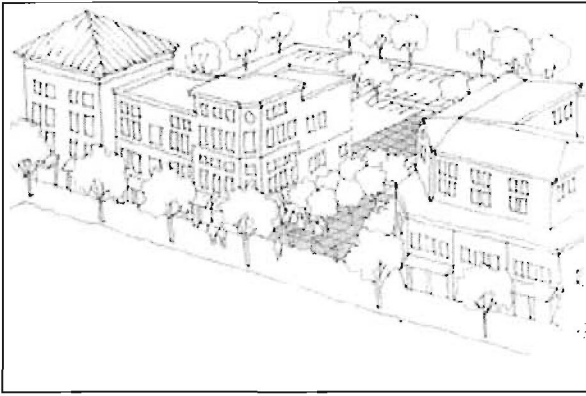


Fig. A.2.d.1 public plaza directly facing principal street

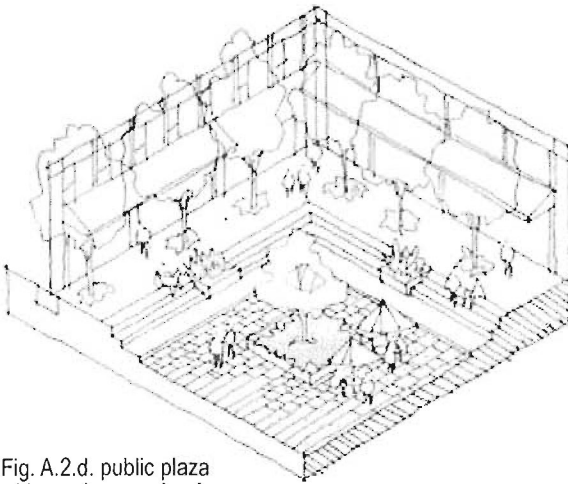
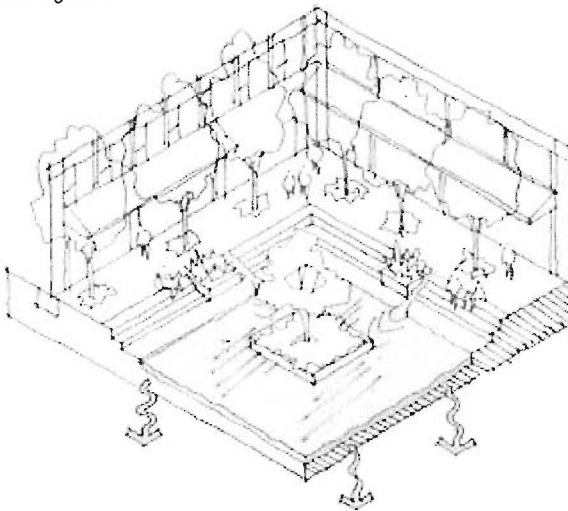


Fig. A.2.d. public plaza with pervious paving for improved stormwater management



3. Walkways and Pedestrian Circulation

a. **Intent:** To improve the pedestrian environment by making it easier, safer, and more comfortable to walk among residences, businesses, open space areas, the street sidewalk, transit stops, through parking areas, adjacent properties, and connections throughout Downtown.

b. Applicable Downtown Design Principles:

- A. Provide a Vibrant Mix of Uses.
- C. Create a Vibrant Public Realm.
- D. Create strong connections between Plan Districts and Sub-Districts.
- E. Incorporate Sustainability.
- I. Increase Transportation Accessibility.

c. Guidelines

All Development

1. Walkways shall be provided through larger development projects to promote walking through the site and in particular through parking lots so that pedestrians may walk without winding through parked cars.
2. Walkways shall incorporate sustainability features such as pervious paving or french drains to direct run-off. Private developments are encouraged to use a variety of stormwater infiltration and conveyance systems, such as french drains and valley drains, provided maintenance is ensured.
3. Walkways shall connect various destinations and be denoted with architectural features so that pedestrian routes are safe, clear, and distinct from vehicular areas. Integration with pedestrian connections in surrounding areas is especially encouraged.
4. Walkways shall accommodate at least two people side by side. Amenities that enhance the pedestrian experience such as planters, benches or other seating opportunities, lighting such as bollards and pedestrian-scale lighting and overhead weather protection are encouraged, and can be used as unifying design elements for the site.

Non-Residential

5. Provide signage scaled to the pedestrian that identifies uses, shops and attractions nearby.

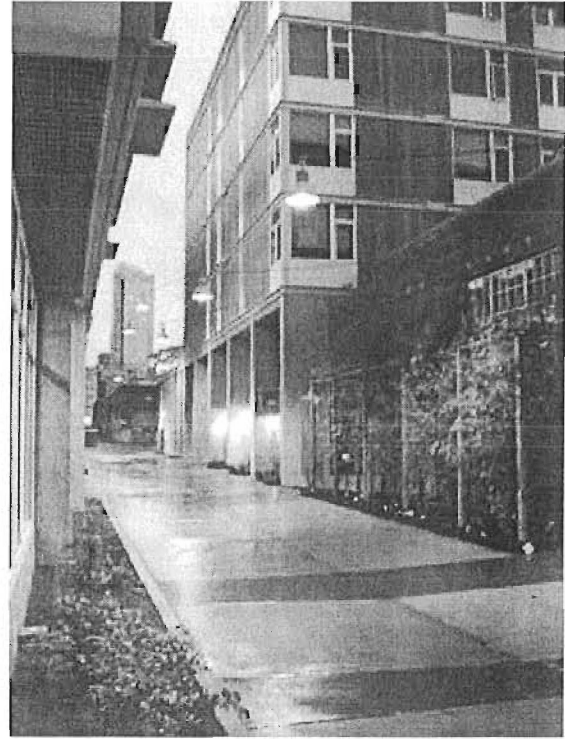


Fig. A.3.c.1 pedestrian walkway through development site provides generous width, landscaping, special lighting, and active facades to create a safe, stimulating and appealing pedestrian environment.



Fig. A.3.c.2 detail of stormwater valley drain

APPLIES TO ALL DOWNTOWN DEVELOPMENT

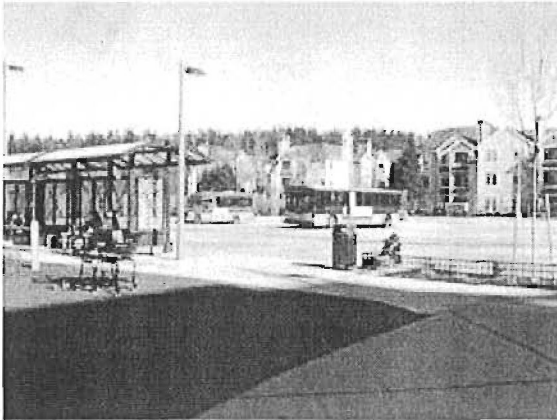


Fig. A.3.c.3 walkway connecting to a major transit stop



Fig. A.3.c.4 gracious and landscaped walkway



Fig. A.3.c.5 way-finding signage integrated into the design of a site

4.1151 Design Guidelines and Standards: All Downtown Sub-Districts

A. Site Design

3. Walkways and Pedestrian Circulation, cont'd

All Development

6. Where a building is close to a transit stop and adequate shelter does not exist, consider including a covered and lighted entrance where people can wait for transit out of the weather. If the development includes a retail use, locate the storefront and weather protection close to the transit stop.

Residential & Single-Family Attached

7. Under Section 7.0201 the following Site Design Criteria and Standards apply. A departure from a referenced Code Section requires demonstration of meeting the intent statement and approval by the Design Commission or Manager.
 - a. 7.0201(I)(1) Pedestrian Circulation (a),(b),(c), and (d), modification as follows: "The on-site pedestrian circulation system shall be continuous and connect the following: streets abutting the site; ground level units entrances and common parking areas; shared open space and children playground areas; abutting transit facility; and any pedestrian amenities such as plazas, resting areas and viewpoints."
 - b. 7.0201 (I)(2) "The on-site shared pedestrian circulation system for all developments, including single-family attached dwellings, shall be designed to meet the accessibility standards of Chapter 11 of the International Building Code (IBC)."

3. Walkways and Pedestrian Circulation, cont'd

d. Standards

1. Walkways shall connect public sidewalks with building entrances not fronting on public streets.
2. Walkways shall connect building entries to other entries within the development.
3. Walkways shall incorporate at least one of the following elements:
 - a. pervious paving for at least 50% of surface area, including sand set decorative paving; stone; porous concrete; or interlocking concrete paver system with gaps to allow water infiltration.
 - b. valley drains to direct water run-off to planted areas
 - c. high-quality, durable seating every 50 feet.
4. Walkways shall be distinctively paved and marked so that the public has a clear sense of where they can travel on foot. Walkways shall be located to provide an obvious connection to transit stops. Acceptable pavement treatments include:
 - a. scored or sawcut concrete in a grid pattern
 - b. decorative troweling patterns
 - c. colored surfaces such as Lithocrete
 - d. concrete with inset art objects
5. Walkways having a width of at least 5 feet shall be provided in all parking lots with more than 60 stalls or development sites greater than 40,000 square feet. Walkways shall be located to provide convenient access for the greatest amount of users.
6. Pedestrian circulation plans shall incorporate all of the following techniques to increase the personal safety of pedestrians:
 - a. Provide adequate lighting; and
 - b. Eliminate sharp "blind" corners; and
 - c. Eliminate tunnels and passages where pedestrians cannot see activity on the far side

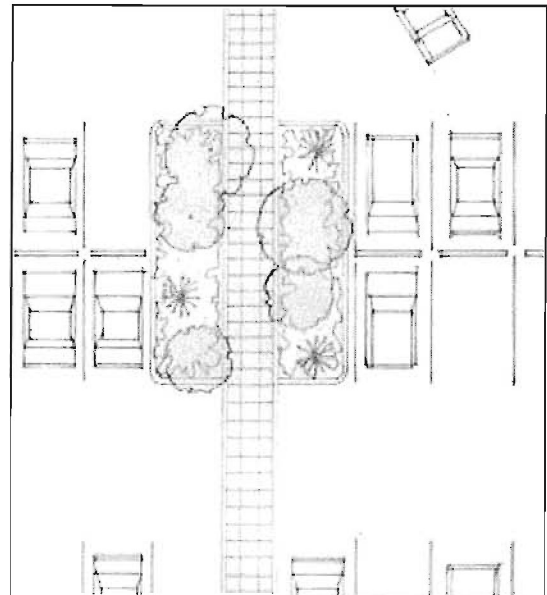


Fig. A.3.d.5 minimum 5ft walkway with scoring



Fig. A.3.d.3 porous walkway



Fig. A.3.d.2 walkway through residential development with benches

APPLIES TO ALL DOWNTOWN DEVELOPMENT

4.1151 Design Guidelines and Standards: All Downtown Sub-Districts
A. Site Design

3. Walkways and Pedestrian Circulation, cont'd

7. If a transit stop is adjacent to the development site, pedestrian connections to the stop shall be provided in an efficient manner.

Residential & Single-Family Attached

8. Under Section 7.0201 the following Site Design Criteria and Standards apply.
 - a. 7.0201(l)(1) Pedestrian Circulation (a),(b),(c), and (d), modification as follows: "The on-site pedestrian circulation system shall be continuous and connect the following: streets abutting the site; ground level units entrances and common parking areas; shared open space and children playground areas; abutting transit facility; and any pedestrian amenities such as plazas, resting areas and viewpoints."
 - b. 7.0201 (l)(2) "The on-site shared pedestrian circulation system for all developments, including single-family attached dwellings, shall be designed to meet the accessibility standards of Chapter 11 of the International Building Code (IBC)."



Fig. A.3.d.7 covered walkway connecting to the street and transit stop

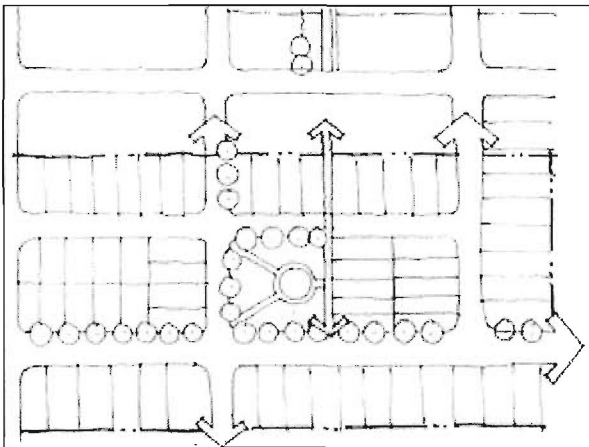


Fig. A.3.d.1 pedestrian network

4. Parking Lot/ Parking Garage Screening

a. **Intent:** To reduce the visual impact of surface lots and parking structures

b. **Applicable Downtown Design Principles:**

- B. Promote Excellence in Design.
- C. Create a Vibrant Public Realm.
- E. Incorporate Sustainability.
- F. Context Sensitivity.
- H. Create Appropriate Transitions in Height, Bulk and Scale

c. **Guidelines**

1. Section 9.0823 Landscaping of Parking Lots (C)(4) Perimeter Screening applies to all new developments. The following additional guidelines supplement this Section. A departure from the referenced Code Section requires demonstration of meeting the intent statement and approval by the Design Commission or Manager.
2. Although parking lots are to be located behind buildings or within structures, there may be conditions where parking lots are visible from public streets, alleys or walkways. Screening shall be used to obscure this visibility and soften the relationship between public places and large paved areas for vehicles.
3. Screening methods for parking lots shall use vegetation and or walls, along with landscaping.
4. On the ground floor level facing street frontages, garages shall provide a perimeter of active use spaces on the ground floor level such as commercial, retail, office or residential uses unless substitutions are approved by the decision authority, either the Design Commission or the Manager. Substitutions for active uses may include trees and other vegetation or artfully designed walls or grillwork.
5. Parking structures shall be viewed not a merely as utilitarian but as contributions to the architectural quality and character of Downtown, with materials and details that reflect the composition of the building and the surrounding buildings.

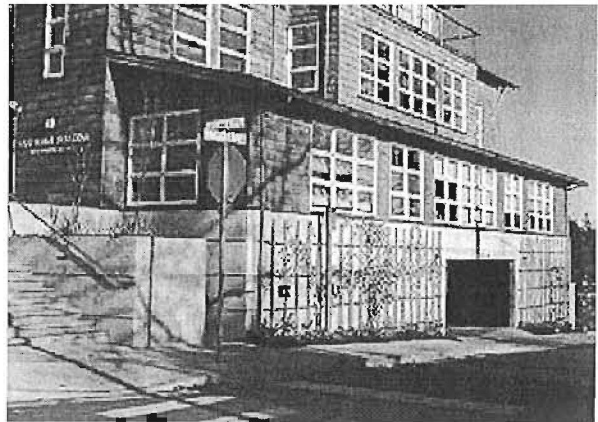


Fig. A.4.c.2 & 3 parking structure softened with lattice and vegetation

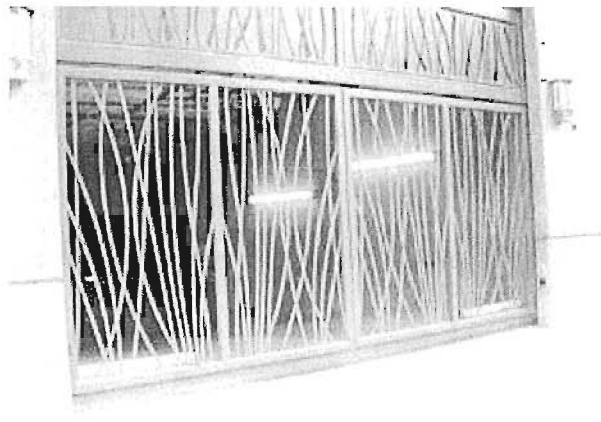


Fig. A.4.c.4 parking structure with decorative metal screening



Fig. A.4.c.5 parking structure designed with architectural facades on all sides and ground level retail

APPLIES TO ALL DOWNTOWN DEVELOPMENT

4.1151 Design Guidelines and Standards: All Downtown Sub-Districts A. Site Design

4. Parking Lot/ Parking Garage Screening, cont'd

d. Standards

1. Section 9.0823 Landscaping of Parking Lots (C)(4) Perimeter Screening applies to all new developments. The following additional standards supplement this Section.
2. Surface parking lots shall be screened to a height of 3' using at least one of the following:
 - a. A combination of evergreen and deciduous shrubs spaced no more than 3 feet apart planted in an area at least 5 feet wide. The height shall be met at installation, not at maturity.
 - b. Ornamental masonry wall clad with brick or tile
 - c. Ornamental metal fence
 - d. A combination of a masonry wall and ornamental metal fencing
 - e. Other elements that meet the intent, as approved by the decision authority, whether the Design Commission or the Manager.
3. Parking structures visible from the street shall be screened using at least one of the following:
 - a. A permitted use such as commercial, retail or residential use with a minimum depth of 20 feet
 - b. An ornamental screen made of metal work
 - c. Trees with a minimum caliper of 2 inches at 6 feet above grade and maximum spacing of 25 feet, along with shrubs, and understory planting
 - d. Bas-relief sculpture
 - e. Another approved device that meets the intent
4. Rain gardens shall be incorporated into planting areas.
5. Parking entrances/exits shall be limited to one per each street face.
6. Chain link fencing, plain metal bars, concrete block or plywood are not acceptable for screening materials.
7. Parking structures must have flat floors on street frontages. Ramps and slanted floors may be used in areas that have less visibility from the street, such as the center of the structure or at the rear away from street frontages.

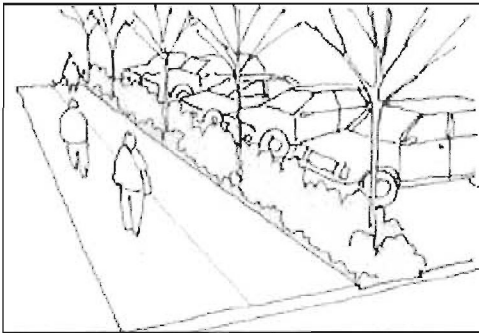


Fig. A.4.d.2 (1) parking lot landscaping screen

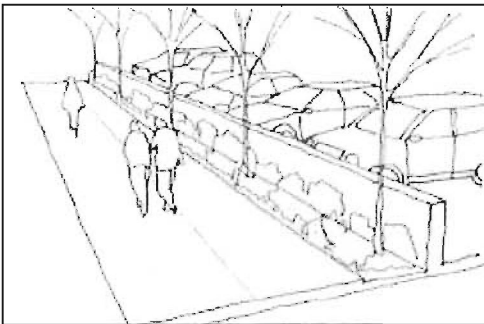


Fig. A.4.d.2 (2) parking lot landscaping and masonry wall screen

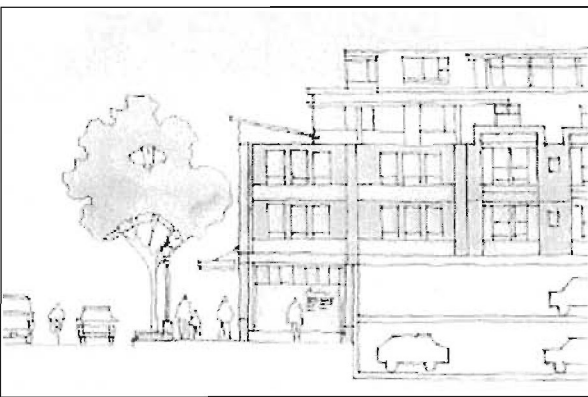


Fig. A.4.d.3 active street level use lining parking structure

5. Landscaping

a. Intent: To integrate landscaping into open spaces, parking areas, and general site design to contribute to an attractive and sustainable development that respects and enhances the landscape character of Downtown.

b. Applicable Downtown Design Principles:

- A. Provide a Vibrant Mix of Uses.
- B. Promote Excellence in Design.
- C. Create a Vibrant Public Realm.
- E. Incorporate Sustainability.
- F. Context Sensitivity.

c. Guidelines:

Parking Lot Landscaping and Lighting

All Development

Intent: To facilitate infiltration of water run-off, offer shade to otherwise barren paved areas, and visually soften expanses of parked vehicles.

1. Section 9.0823 Landscaping of Parking Lots applies to all new developments. The following additional guidelines supplement this Section. A departure from the referenced Code Section requires demonstration of meeting the intent statement and approval by the Design Commission or Manager.
2. Within parking lots, parking shall be divided into parking islands containing one tree per nine stalls and understory.
3. Lighting fixtures shall be pedestrian-scaled and designed to prevent glare.
4. Stormwater management techniques shall be incorporated into the design of parking lots.
5. Parking lot landscaping and decorative paving should be used to soften the visual impact on adjacent buildings and public spaces.



Fig. A.5.c.5 (1) naturalistic grouping of plantings



Fig. A.5.c.4 landscaped swale through parking lot provides low impact stormwater infiltration



Fig. A.5.c.5 (2) decorative paving, bollards and trees in parking areas

APPLIES TO ALL DOWNTOWN DEVELOPMENT

5. Landscaping, cont'd

d. Standards:

Parking Lot Landscaping and Lighting

All Development

1. Section 9.0823 Landscaping of Parking Lots applies to all new developments. The following additional standards supplement this Section.
2. Parking lots shall include fully shielded and full cut-off light fixtures between 18 feet and 24 feet in height.
3. Stormwater management, including rain gardens, stormwater planters, and swales for sites larger than 20,000 square feet shall be applied to all landscaped areas so that run-off is captured and filtered into the ground.
4. Any planting areas within parking lots that are not part of a parking island shall have a minimum depth of 5 feet.

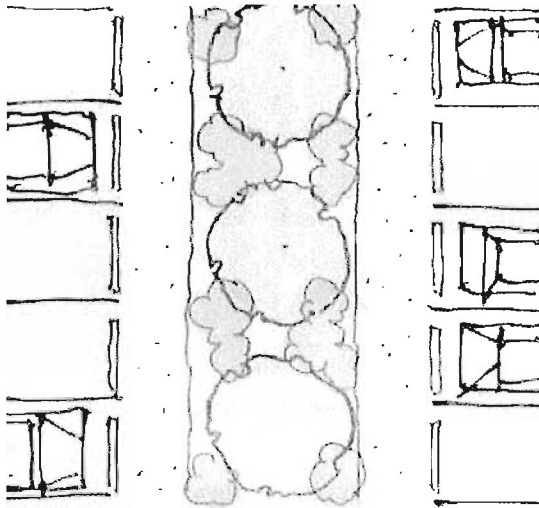


Fig. A.5.d.1 a range of required parking lot landscaping as a site amenity

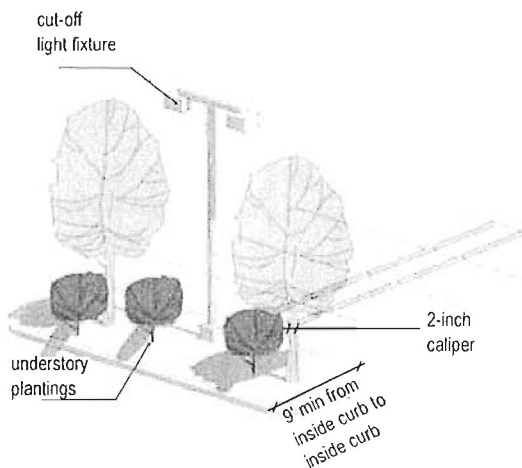


Fig. A.5.d.2 & 3 & 4 landscaped island

5. Landscaping, cont'd

e. Guidelines: Site Landscaping

All Development

Intent: To reinforce or establish the character of the site, surrounding community and the natural environment through the treatment of landscape areas.

1. Use a rich, yet coordinated palette of landscape materials throughout the site to provide scale, texture and color that supports the design intentions of the building architecture.
2. For an entry way, planting should be massed and scaled as appropriate to demarcate the entrance.
3. Enhance the front of the building with ornamental plant material, such as ornamental trees, flowering shrubs and perennials and ground covers.
4. Extend the landscape character of the area into the site; in particular, include mature shade trees where appropriate.
5. Automatic watering systems and drought tolerant species are encouraged to promote water conservation and reduce maintenance concerns.



Fig. A.5.e.3 a rich array of plantings creates an appealing residential street scene



Fig. A.5.e.1 & 3 landscaping in the front yard softens the building edge along the street

APPLIES TO ALL DOWNTOWN DEVELOPMENT

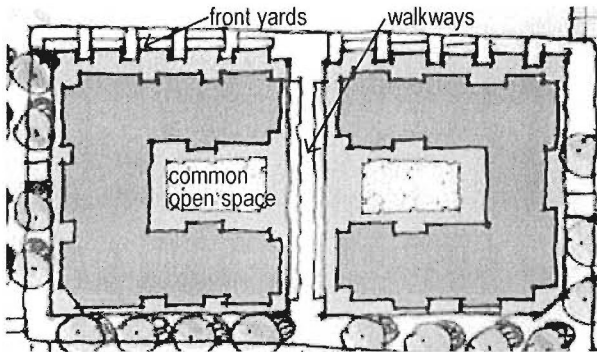


Fig. A.5.f.3 meeting the landscaping requirement can be accomplished through a number of site features



Fig. A.5.f.4 a variety of plantings enhances the walkway through the site



Fig. A.5.f.5 generous landscaping in the landscaped strip

5. Landscaping, cont'd

f. Standards: Site Landscaping

All Development

1. At least 15% of the site area shall be landscaped.
2. At least 70% of the front yard setback area shall be landscaped in meeting the 15% site area total. Use of ornamental grass lawns shall be limited to 20% of this requirement in the front yard.
3. Landscaping as defined in 3.0010, all landscaped setback areas, landscaped common open space, eco-roofs, preserved natural areas, planter areas, landscaped screening areas, as well as landscaped strips located between the curb and sidewalk can be included to meet the 15% landscaping requirement. In addition, shared walkways within landscaped areas may count for up to 66% of the total required landscape area.
4. A minimum of 20% of plant varieties used within landscape areas shall be perennial and/or annual varieties to provide color, texture, and other special interest.
5. In addition to required street trees, site trees shall be required at a rate of one tree per 3,000 square feet of gross site area.
 - a. New site trees must be capable of growing to a height of 25 feet. However, when accompanied by an arborist's report, ornamental, dwarf, and other smaller species may be permitted in locations where larger sized trees are not appropriate.
 - b. New deciduous site trees shall be a minimum of 1.5 inch caliper at the time of planting.
 - c. Evergreen trees shall be a minimum 6 foot height at the time of planting.
 - d. As much as possible, new site trees shall be distributed throughout the project rather than clumping them in one location.
 - e. New site trees shall be staked for support for at least one year.
6. All landscaped areas shall be irrigated with an in-ground irrigation system, unless a licensed landscape architect submits written verification that the proposed planting materials do not require irrigation.

5. Landscaping, cont'd

g. Guidelines: Shared Open Space

Intent: To create comfortable places for residents, amenable to social activity, through the use of plazas, courtyards, and other outdoor spaces.

Residential

1. Section 7.0201(M)(5) Shared Open Space Standards for Complexes applies to new stacked unit developments of 20 or more units. A departure from the referenced Code Section requires demonstration of meeting the intent statement and approval by the Design Commission or Manager. The following additional guidelines supplement this Section.
2. Make plazas, courtyards, patios and lawns comfortable for human activity and social interaction - standing, sitting, talking, eating and playing.
3. Create a pleasant climate, including shade from summer sun and access to winter sunlight. Locate seating with consideration to sun and shade.
4. Use plants, furniture and lighting to shape, enliven, and give purpose to outdoor space.
5. Consider adding a focal element, such as a sculpture or fountain to outdoor space.
6. Design outdoor space with safety in mind. On-site plazas and courtyards should promote visibility from the street, and lighting to enhance nighttime security.



Fig. A.5.g.2 (1) courtyard

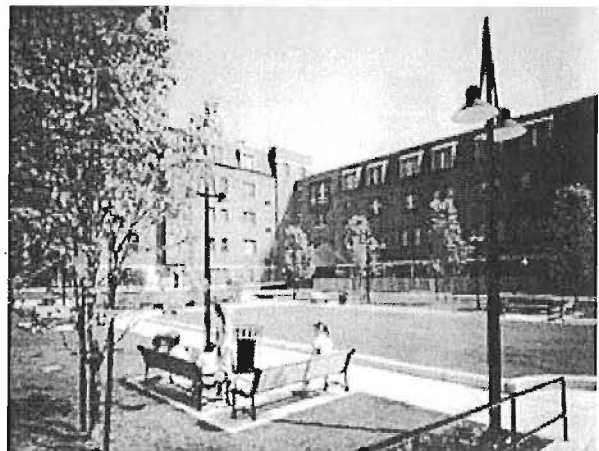


Fig. A.5.g.3 lawn with seating and lighting



Fig. A.5.g.4 seating and tables

APPLIES TO ALL DOWNTOWN DEVELOPMENT

4.1151 Design Guidelines and Standards: All Downtown Sub-Districts
A. Site Design



Fig. A.5.h.2 (1) courtyard with entry trellis, seating, and fountain as a focal point



Fig. A.5.h.2 (2) lawn surrounded by units provides surveillance



Fig. A.5.h.2 (3) tot lot

5. Landscaping, cont'd

h. Standards: Shared Open Space

Residential

1. Section 7.0201(M)(5) Shared Open Space Standards for Complexes applies to new stacked unit developments of 20 or more units. The following additional standards supplement this Section.
2. Shared open spaces shall provide at least three of the following amenities to accommodate a variety of ages and activities. Amenities include:
 - a. Site furnishings (seating areas, tables)
 - b. Patios, plazas, or courtyards
 - c. Tot lots
 - d. Gardens
 - e. Open lawn
 - f. Play fields
 - g. Sports courts, such as tennis or basketball courts
 - h. Architectural lighting
 - i. Fountain
 - j. Art piece

1. Massing

a. **Intent:** To break down the apparent scale and massing of larger buildings.

b. **Applicable Downtown Design Principles:**

- E. Incorporate Sustainability.
- F. Context Sensitivity.
- G. Preserve and Enhance Historic Character.
- H. Create Appropriate Transitions in Height, Bulk and Scale

c. **Guidelines**

1. Respect Surrounding Character:
 - a. Design building volumes to maintain a compatible scale with their surroundings.
 - b. Rely on building massing and orientation to place strong visual emphasis on the street and other important civic spaces.
 - c. Upper stories may be stepped back, treated with different exterior finish materials, or incorporated into the roof line to reduce the apparent mass of the building.
2. Consider day-lighting, sun shading, natural ventilation and views in the massing and orientation of the building.
3. Use articulated, smaller building volumes within larger developments as a transition in size to adjacent historic or residential structures that are smaller in scale.
4. Vary the three dimensional character as a building rises; in general, differentiate between the base, middle and top.



Fig. B.1.c.1 (1) orientation and massing of a tall building's base places strong emphasis on the street and creates a humane street environment



Fig. B.1.c.1 (2) Upper story step backs create a transition in height, bulk and scale

APPLIES TO ALL DOWNTOWN DEVELOPMENT



Fig. B.1.c.5 building massing can frame views within and through a development site



Fig. B.1.c.5 building massing can maintain public views of Downtown landmarks

1. Massing, cont'd

5. View Opportunities. Views of nearby natural features such as Mount Hood to the east and surrounding bluffs and buttes, contribute to Gresham's identity. New buildings will inevitably limit views from other buildings and surrounding sites. The purpose of the following Guidelines is to provide building massing methods and techniques ensuring reasonable care have been taken to avoid obstructing valued views where applicable.
 - a. Configure the building shape and the roof form to maximize scenic views, especially from public places such as parks and streets.
 - b. Framing of important views of natural and man-made landmarks opportunities to open new views of landmark features through the site shall be considered.

APPLIES TO ALL DOWNTOWN DEVELOPMENT

1. Massing, cont'd

d. Standards

1. Massing Requirement

Building size	Footprint	Required Massing
Small	< 10,000 sf	one building mass
Medium	> 10,000 sf < 20,000 sf	two building masses (Fig. a and b)
Large	> 20,000 sf	three building masses (Fig. c and d)

* Multiple buildings on a site will be calculated individually and follow the guidelines for their size building

- a. The minimum mass or volume change shall be at least 25% of the total volume of the building as calculated by FAR.
- b. Building size limitations shall be adhered to (see Table).
- c. Elements being used to count as massing shall not be used as wall modulation elements.
- d. Upper-level building step backs. This is one option in satisfying the building massing requirement. Provide a minimum 6-foot step back above the third floor of a maximum five-story tall building or above the fourth floor of a building taller than five stories to help create human scaled environments and prevent over-shading the street.

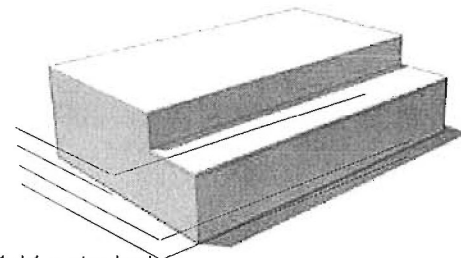
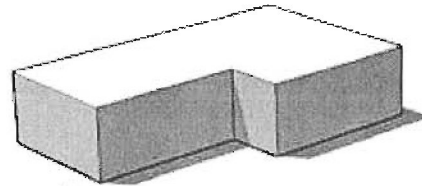
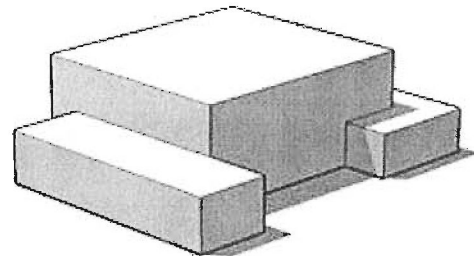


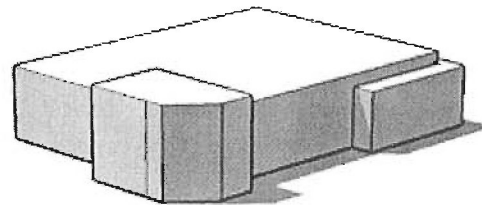
Fig. B.1.d.1 a. step back



b. building jog



c. separate volumes



d. separate volumes

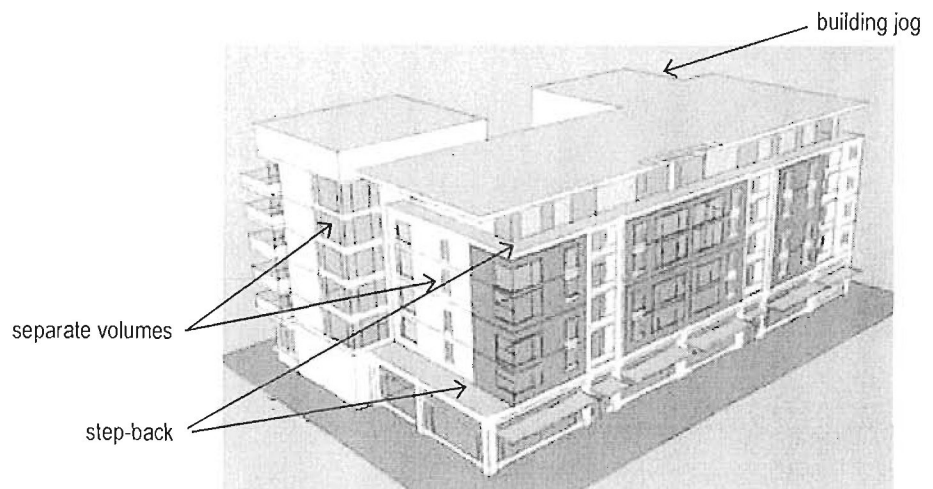


Fig. B.1.c.1 (2) Example of a large building (>20,000 sf footprint) employing three building masses to satisfy the requirement

APPLIES TO ALL DOWNTOWN DEVELOPMENT

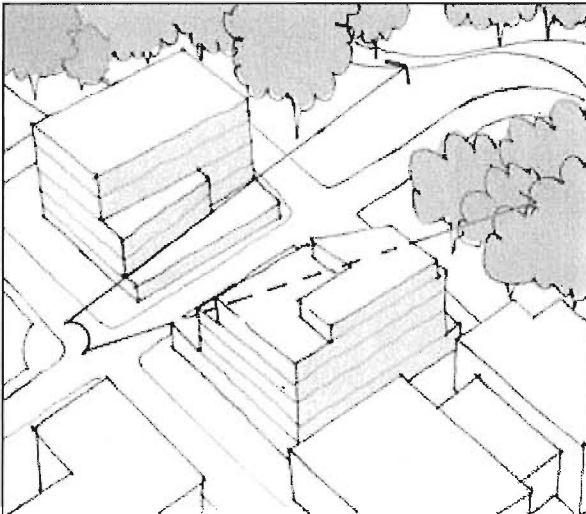


Fig. B.1.d.3.b step buildings back to enhance views from public spaces

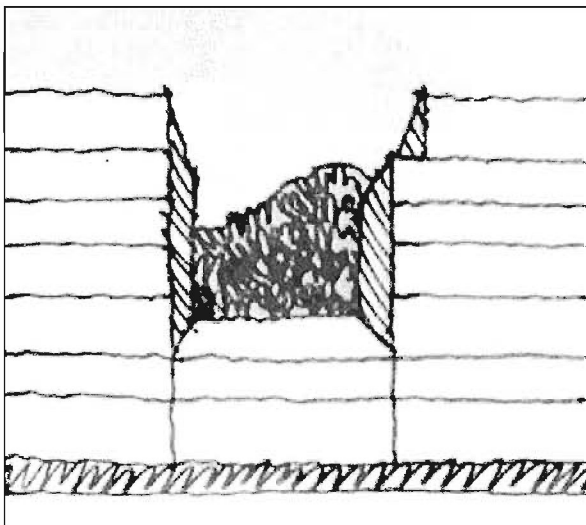


Fig. B.1.d.3.a building massing frames a view of a natural landmark by creating separate building volumes

1. Massing, cont'd

View Opportunities

2. Alterations to the height and length shall be incorporated into portions of the building to maintain public and private views through the site. This is not intended to significantly reduce the maximum FAR allowed for the site.
3. The project's massing requirements (upper level step backs, building jogs, and separate volumes) shall address the view focused on the portion of a site affected by the view. Ways to address the view may include but are not limited to the following:
 - a. Arrange separate volumes to frame important views.
 - b. Provide upper level step backs above the second floor to help maintain existing views or provide new views.
 - c. Create a significant jog in the building to maximize a view angle towards a special point of interest.

B. Building Design

APPLIES TO ALL DOWNTOWN DEVELOPMENT

2. Façade Composition

a. Intent: To establish a set of contextually responsive, clear and pleasing set of proportions that will contribute to a coherent building design and promote architectural excellence within the Downtown.

b. Applicable Downtown Design Principles:

- A. Provide a Vibrant Mix of Uses.
- B. Promote Excellence in Design.
- C. Create a Vibrant Public Realm.
- E. Incorporate Sustainability.
- F. Context Sensitivity.
- G. Preserve and Enhance Historic Character.

c. Guidelines

All Development

1. Elements such as sun shades, terraces, and rain water harvesting features can be used to compose and articulate the building's façade.
2. Size, proportion and locate façade components to provide a visual transition to adjacent structures that are smaller in scale.
3. Consider each building as a high-quality, long-term addition to downtown; use materials, fenestration, and details to unify a building's appearance. Apply recessed, or "punched" windows to create shadow lines and suggest solidity.

Non-Residential

4. Use façade rhythms and patterns to solidify the relationship between new and old buildings. Some of the best buildings downtown use vertical and horizontal expression to reflect changes in building form. For example, the structural bays of the building can be expressed with pilasters or other vertical detailing
5. Establish a rhythm of vertical elements along the façade: The regular cadence of display windows and shop entrances can encourage a variety of ground level uses.
6. Street level facades offer the greatest opportunity to support pedestrian activity; distinguish the ground level of a building from the upper levels, especially where a building orients to the street and/or defines public space.



Fig. B.2.c.4 facade rhythms using both vertical and horizontal expression



Fig. B.2.c.3. facade components establish the building's clear and pleasing set of proportions



Fig. B.2.c.1 sun shades used to unify the building's street wall and add three-dimensional quality

APPLIES TO ALL DOWNTOWN DEVELOPMENT



Fig. B.2.c.7. highly articulated facade with projections and recesses signify residential function of the building



Fig. B.2.c.9. three-dimensional design features at the corner and bays and balconies exhibit pleasing proportions and three-dimensional quality



Fig. B.2.c. 8&9. change in fenestration, color and recessed windows animates an otherwise flat facade

2. Façade Composition, cont'd

Residential

7. Articulate residential building facades to unify a building's appearance, and add interest, scale, and three-dimensional quality. Articulation may be provided through periodic changes in wall plane, building material and/or color, changes in building fenestration, or other approaches that create visual interest and/or shadow lines.
8. Use windows and doors to establish scale and give expression to residential buildings. Details that contribute to a residential scale and appearance, such as mullions, are encouraged.
9. Incorporate three-dimensional design features, such as balconies and bays. Windows other than bays should be recessed behind the plane of the building to create shadow lines.

Single-Family Attached

10. Refer to Section 7.0201(L) Architectural/Design Standards for the design of Single Family Attached Dwelling Structures (such as Townhouses), except 7.0201(L)(4). Deviations from the Downtown Plan District standards or applicable standards of Section 7.0200 may be allowed through the discretionary process as described in 4.1101(A)(1)(a).

B. Building Design

APPLIES TO ALL DOWNTOWN DEVELOPMENT

2. Façade Composition, cont'd

d. Standards

All Development

1. If sustainable features on the facade, such as sun shades and rain water harvesting features, are used they shall be used to organize the facade composition either horizontally or vertically.
2. Windows shall be recessed at least 4 inches to reinforce rhythms and cast shadows.

Non-Residential

3. A building of two or more stories shall have a belt line, fascia, or other horizontal datum line above the ground level façade.
 - a. Commercial buildings shall be designed to encourage multi-tenant occupancy and walk-in traffic at the street level. The following shall be incorporated in the design of street level facades:
 1. 15- to 30-foot-wide bays framed by columns or pilasters;
 2. Multiple storefront windows with 18- to 30-inch bulkheads or projecting sill.
4. Façade articulation shall consist of at least two of the following design features:
 - a. Changes in plane with a depth of at least 24 inches, vertically at intervals of not less than 30 feet and not more than 90 feet or horizontally not less than 20 feet and not more than 60 feet; or
 - b. Changes of color, texture, or material, vertically at intervals of not less than 30 feet and not more than 90 feet or horizontally not less than 20 feet and not more than 60 feet; or
 - c. A repeating pattern of wall recesses and projections at intervals of not less than 30 feet and not more than 90 feet, such as bays, offsets, reveals or projecting ribs, that has a relief of at least 12 inches.
5. On the facade facing the principal street, upper floors shall provide a minimum of 40% glazing between 3 and 8 feet, as measured from that story's finished floor level.

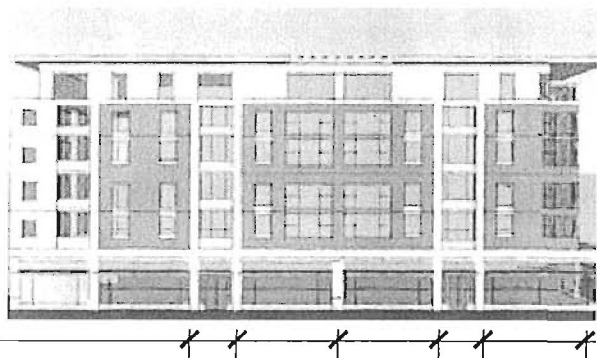


Fig. B.2.d.1 & 3 belt line above ground level and regular cadence of bays and storefronts framed by pilasters

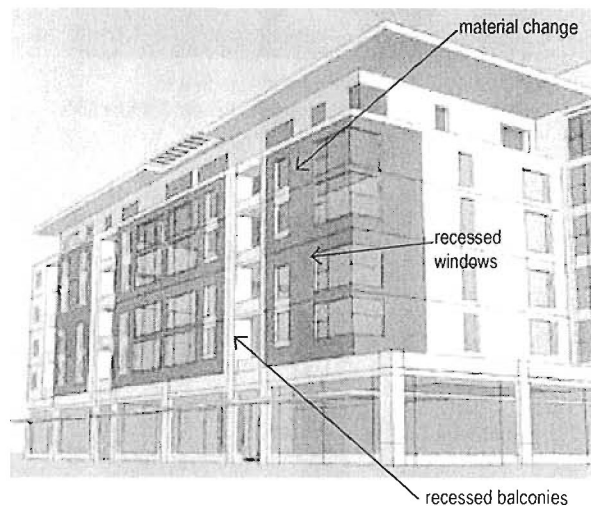


Fig. B.2.d.4 & 5 facade articulation features proportioned and applied in intervals to satisfy the requirement



Fig. B.2.d.6.a facade broken down into smaller volumes related to the interior use providing a clear design intent and visual appeal



Fig. B.2.d.6.b variation in window size, including projecting bays

2. Façade Composition, cont'd

Residential

6. Horizontal facades longer than 30 feet shall be articulated into smaller volumes to achieve a residential scale. At least two of the following methods shall be used:
 - a. variation in the building form related to the scale of individual dwelling units as recessed or projecting bays, shifts in massing or distinct roof shapes;
 - b. diversity of window size, shape or patterns that relate to interior function;
 - c. emphasis of building entries through projecting or recessed form, detail, color, or materials;
 - d. variation in detailing including sills, headers, belt courses, reveals, pilasters, window bays, and similar features.
7. The total area of windows and doors on the street-facing facade shall not be less than 25% to the total area of the facade, excluding roof shapes such as gables.
8. The first floor facade shall include windows that provide visual interest and visual connection to the street. Ground level windows are required.

Single-Family Attached

9. Refer to Section 7.0201(L) Architectural/Design Standards for Single Family Attached Dwelling Structures (such as Townhouses), except 7.0201(L)(4). Deviations from the Downtown Plan District standards or applicable standards of Section 7.0200 may be allowed through the discretionary process as described in 4.1101(A)(1)(a).

3. Ground-Level Details

a. **Intent:** To reinforce the streetscape by encouraging the greatest amount of visual interest along the ground level of buildings facing pedestrian streets.

b. **Applicable Downtown Design Principles:**

- A. Provide a Vibrant Mix of Uses.
- B. Promote Excellence in Design.
- C. Create a Vibrant Public Realm.
- D. Create strong connections between Plan Districts and Sub-Districts.
- G. Preserve and Enhance Historic Character.

c. **Guidelines**

All Development

1. Show creativity and individual expression in the design of the ground plane, and encourage visual details and richness that sustains interest along the street. Artisan produced details, such as handrails, lighting, doors and fences are especially encouraged.

Non-Residential

2. Use design elements such as separate storefronts, display windows, shop entrances, exterior light fixtures, awnings and overhangs to add interest and encourage a mix of businesses.
3. Incorporate architectural elements that reinforce Downtown's unique qualities; in particular, downtown's best building's tend to support an active street life.
4. Significant grade separations between sidewalks and ground floor uses and features are discouraged.
5. Avoid ornamentation and features at the ground level that appear artificial or "tacked-on."

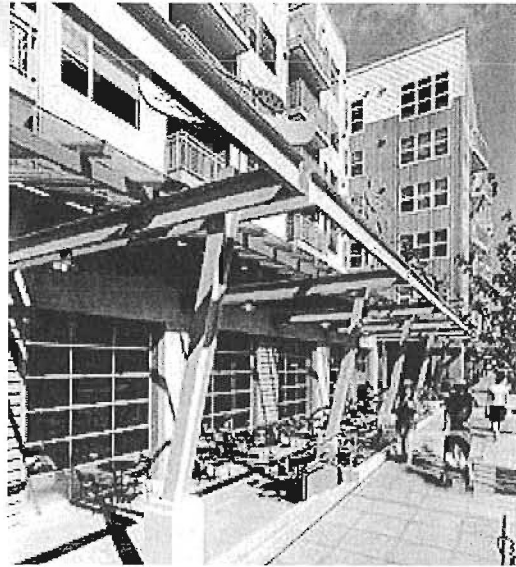


Fig. B.3.c.2 colorful, expressive ground-level facade elements help animate the street



Fig. B.3.c.3 medallions, seasonal planting, recessed entry, pedestrian signage all integrated into the building facade



Fig. B.3.c.1 artisan crafted details in the public realm

APPLIES TO ALL DOWNTOWN DEVELOPMENT

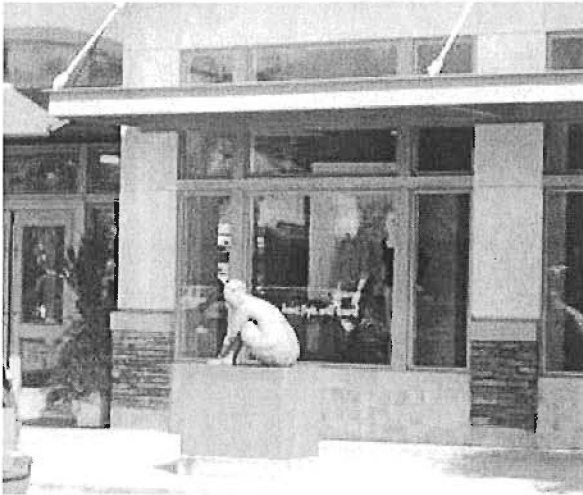


Fig. B.3.d.1 & 2 plinths for the columns and tilework on the bulkhead give greater attention to detail at the street level of a building

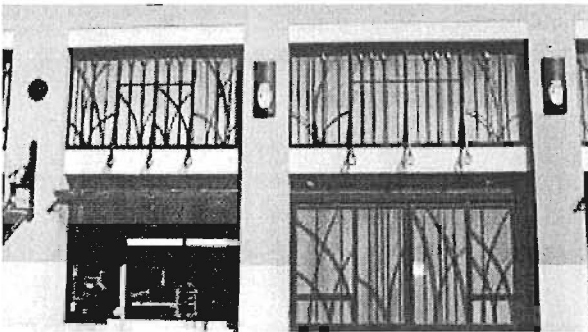


Fig. B.3.d.2 decorative sconces reinforce the regular cadence of storefronts

3. Ground-Level Details, cont'd

d. Standards

Non-Residential

1. Ground floor, street-facing facades of commercial and mixed-use buildings shall incorporate at least one of the following:
 - a. medallions, at a regular interval of bays and columns, every 15 to 40 feet
 - b. belt courses, above ground floor level and along entire facade
 - c. plinths for columns, at a regular interval of bays and columns, every 15 to 40 feet
 - d. bulkhead for store front window, between 1-2 feet from ground level
 - e. projecting sills, a minimum of 8 inches from window pane

And two of the following:

 - f. pedestrian-scaled lighting (such as building sconces), at a regular interval of bays and columns, every 15-40 feet (this is a high priority item to ensure well-lit and safe streets)
 - g. hanging baskets supported by ornamental brackets flanking entries and at the corner
 - h. ornamental tilework, a minimum of 5% of the ground level facade
 - i. planter box(es), a minimum of 6 square feet, not in a pedestrian way
 - j. an approved element not listed here, that meets the intent and as approved by the Manager or the Design Commission
2. Ground floor commercial uses shall be at the sidewalk elevation.
3. The use of applied foam ornamentation and Exterior Insulation Finishing System (EIFS) on exposed, ground level locations are prohibited on any building type.

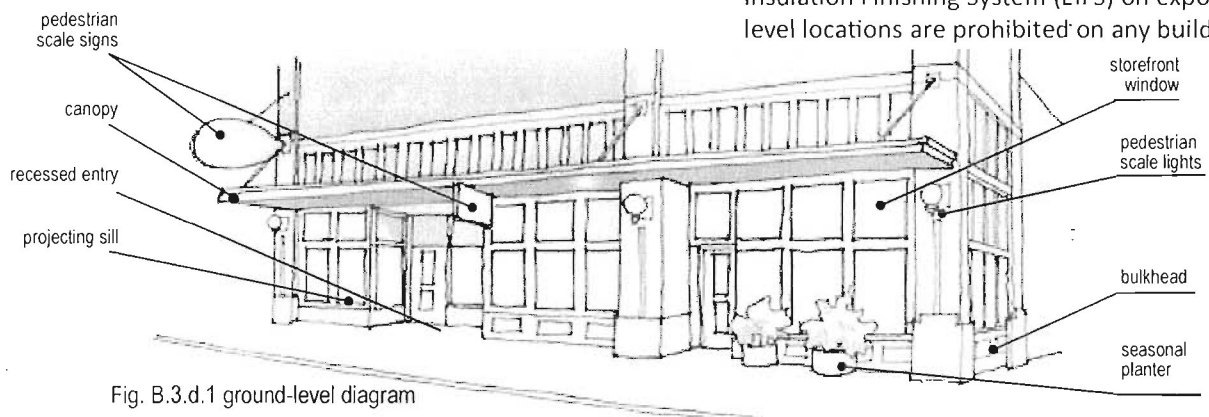


Fig. B.3.d.1 ground-level diagram

4. Sign Design

a. Intent: To ensure that signage is part of an integrated design approach to a project and to encourage interesting, creative and personalized elements in the public realm.

b. Applicable Downtown Design Principles:

- A. Provide a Vibrant Mix of Uses.
- B. Promote Excellence in Design.
- C. Create a Vibrant Public Realm.
- G. Preserve and Enhance Historic Character.

c. Guidelines

Non-Residential

1. **Unique Design and Identity:** As a unique expression of the business, the design possibilities are limitless. Projects are encouraged to add personality, imagination and innovation to the street environment while being mindful of the following considerations in the design and placement of exterior signs on a building:
 - a. **Sign orientation.** For the most part, signs in downtown Sub-Districts should be oriented to the pedestrian. This may include wall signs, projecting signs and window signs.
 - b. **Respect for architectural elements and details.** Care should be taken not to interfere with design elements that contribute to the building's character.
 - c. **Shield the light source.** Whenever projection lighting is used, care should be given to properly shield the light source.

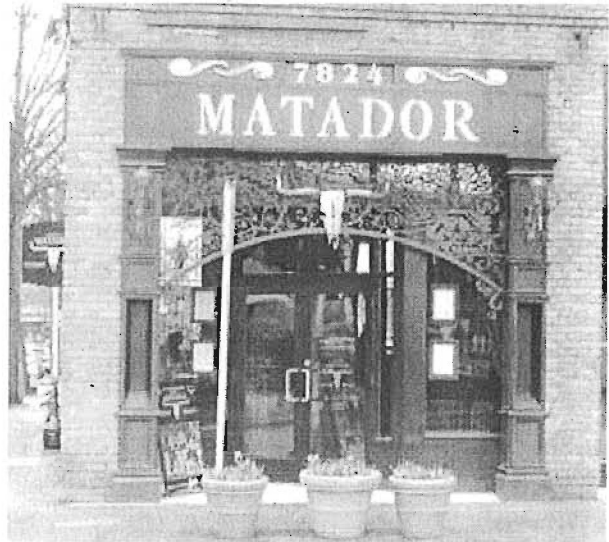


Fig. B.4.c.1 (1) well crafted wall sign adds to the street level experience



Fig. B.4.c.1 (2) lights shielded toward sign

APPLIES TO ALL DOWNTOWN DEVELOPMENT

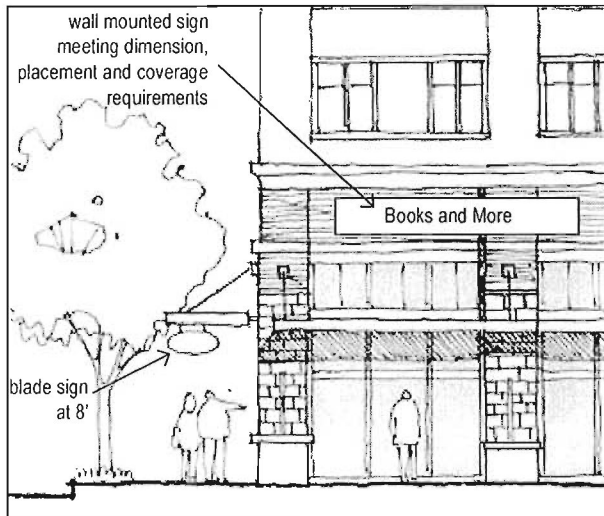


Fig. B.4.d. 1 & 2 ground floor sign diagram

4. Sign Design, cont'd

d. Standards

Non-Residential

1. Wall Signs: Position flush-mounted signs so they fit within architectural features:
 - a. Wall signs shall fit into a sign band of fascia on or directly above the ground level facade to help reinforce horizontal lines along the street.
 - b. Coordinate the color scheme with the building front.
 - c. Locate flush wall signs so that they do not extend beyond the outer edges of the building front.
 - d. The total maximum wall sign area on a building shall not exceed 7% of the overall ground level facade area.
 - e. The maximum height of an individual flush-mounted sign shall be 2 feet.
2. Projecting Signs: Locate projecting or blade signs along the ground-level facade, not above, and mount perpendicular to the building.
 - a. Clearance above the sidewalk shall be no less than 8 feet.
 - b. The maximum size of an individual projecting sign (one face) shall be 8 square feet.
 - c. The sign shall not extend from the building facade for a greater distance than 6 feet, or a distance equal to two-thirds the width of the abutting sidewalk, whichever is less.
 - d. Place signs near the business entrance, to guide customers to the door.
3. Window Signs: A window shall be painted or hung just inside the window. The maximum area of a window sign shall not exceed 20% of the window.
4. Internal illumination of signs shall not be permitted. However, neon is acceptable if used in limited quantities and where it is appropriate to the context.
5. Signs shall be durable and long lasting since they are exposed to extreme weather conditions.

5. Transparency

a. **Intent:** To promote the use of building facades to provide safe and comfortable waiting areas for transit and provide visual connections between activities inside and out.

b. **Accompanies Design Principles:**

- A. Provide a Vibrant Mix of Uses.
- C. Create a Vibrant Public Realm.

c. **Guidelines:**

Non-Residential

1. Promote active, pedestrian-oriented uses with a high degree of transparency along the street. Uses should be readily discernible to passers-by.
2. The use of reflective, opaque, and highly tinted glass should be avoided in storefront windows and doors.



Fig. B.5.c.1 merchandise and activity inside is highly visible from along the sidewalk



Fig. B.5.c.2 generous ground level transparency

APPLIES TO ALL DOWNTOWN DEVELOPMENT

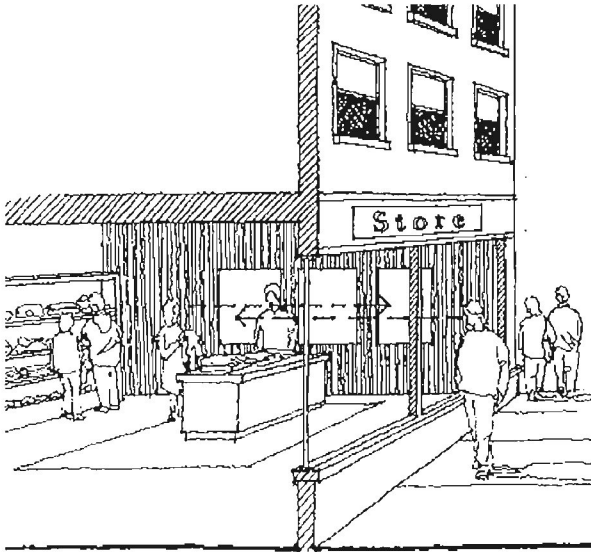


Fig. B.5.d.1 ground level transparency must provide visibility into and from the interior use

5. Transparency, cont'd

d. Standards:

All Developments

1. Ground level transparency requirements are defined in the Street Type Design Standards for all designated streets. For both commercial and residential projects located on Downtown streets without a street type designation, a minimum of 30% of any ground floor façade visible from any street shall be comprised of windows with clear, transparent glass.
2. The minimums above shall be calculated on the area of the ground floor facade between 2 feet and 12 feet above grade.

Non-Residential

3. To meet the clear, transparent glass requirement, storefront windows shall have a Visible Transmittance (VT) value of 90%.
4. For commercial uses, display windows for merchandise (that do not provide views into the store) can satisfy the clear, transparent glass requirement if the display windows are at least 24 inches deep and can contain three-dimensional objects, such as product displays or mannequins.
5. Films or other substances applied to the window to reduce the Visible Transmittance value below 90% shall be prohibited.

6. Prominent Entrances

a. Intent: To design and orient building entrances that readily inform people of their access and use. A main lobby entrance serves a different purpose than a storefront entrance to a shop, and these differences should be reinforced in their design. However, all public and prominent entrances should add character, identity and interest along the street.

b. Applicable Downtown Design Principles:

- A. Provide a Vibrant Mix of Uses.
- B. Promote Excellence in Design.
- C. Create a Vibrant Public Realm.
- D. Create strong connections between Plan Districts and Sub-Districts.

c. Guidelines

Non-Residential

1. The primary (front) building façade and main entry of nonresidential buildings shall be well-marked, articulated and oriented to and facing the primary public street.
2. Consider placing the main building entrance at a street corner, where feasible.
3. Entries shall be lighted and protected from weather.
4. Entries facing public streets shall be made visually prominent and receive architectural emphasis. A variety of techniques to accomplish this include:
 - a. recessed entries
 - b. projecting entries
 - c. elevated entries with stairways for residential uses
 - d. entry-related cover and/or roof line articulation (such as canopy articulation; parapet-roof articulation)
 - e. arched entries
 - f. decorative lintels or molding above doorways
 - g. landscape treatment and emphasis
 - h. surface treatment (such as paver or tiles)
 - i. entry courtyard
 - j. transom windows
 - k. signage
 - l. pilasters or columns supporting and/or framing the entrance
 - m. other techniques as appropriate



Fig. B.6.c.4 (1) oversized canopy and columns emphasize the main entry



Fig. B.6.c.3 trellis and landscape create a gracious main entry



Fig. B.6.c.4 (2) arched entry

APPLIES TO ALL DOWNTOWN DEVELOPMENT



Fig. B.6.d.5&6. double height entrance and stoop defines the main entry to a multifamily building



Fig. B.6.d.6 & 7. minor grade separation from residence to street and clear visual connection into the site balances the need for privacy with a strong relationship between the public and private realm



Fig. B.6.d.8. overhead weather protection, landscaping and benches provide pedestrian comfort

6. Prominent Entrances, cont'd

Residential & Single-Family Attached

5. Multiple residential building entrances that activate and are oriented to the street are encouraged.
6. Entry porches and stoops serving as a transitional space between the public sidewalk and residential building are encouraged. Porches and stoops shall not encroach upon a public sidewalk.
7. Ground-level pedestrian passages from sidewalk to entrances within the site are encouraged and should have a strong visual connection to the street.
8. Incorporate elements that address a pedestrian scale on and near entrances to a residential building.
9. Consider using lighting for security and as an enhancement to the building facade.

6. Prominent Entrances, cont'd

d. Standards

Non-Residential

1. Commercial buildings and mixed use buildings shall be designed to encourage multi-tenant occupancy and walk-in traffic at the street level. The following shall be incorporated in the design of street level facades:
 - a. Larger main entrances shall be differentiated from smaller storefront entries. The larger entrance shall be supported by a pair of large piers, while smaller entrances shall be framed with a pair of smaller columns; and
 - b. Building entrances shall not be located at intervals of no more than 75 feet along the elevation facing the principal street; and
 - c. When multiple entrances are not feasible, long walls must be broken up with glazing and other pedestrian amenities.
2. At least one customer and/or residential entrance (for mixed-use buildings) shall face the principal street and connect directly to the sidewalk along the secondary street on corner lots.
3. Building entrances shall be located under a shade device such as an awning or portico, with a minimum depth of 5 feet.
4. Primary entrances to the building shall have a dominant presence in the building facade; incorporate three of the following features:
 - a. recessed doorway, at least 2 feet from surrounding facade
 - b. overhang/canopy with a distinct form and larger size than other overhangs on the facade
 - c. windows, a minimum of 18 inches tall above the door and above the width of the entrance.
 - d. glass windows flanking the doorway, a minimum of 1 foot wide and the height of the doorway.
 - e. ornamental light fixtures, flanking both sides of the door
 - f. oversized entry door(s)
 - g. stone, masonry, or tile paving in entry, a minimum of a 6 square feet
 - h. planters with ornamental landscaping
 - i. seating, along at least one edge of the entry way



Fig. B.6.d.3 large, distinctive canopy highlights a main entrance

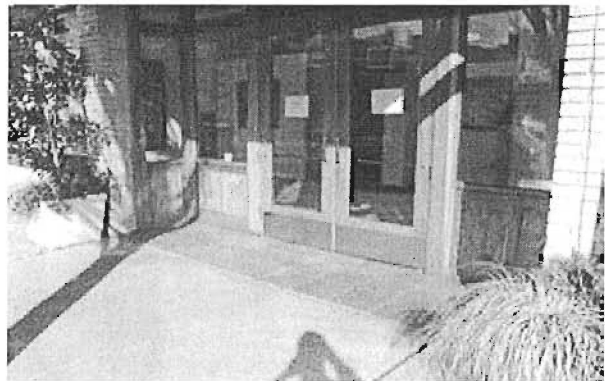


Fig. B.6.d.4 (1) 2 ft recessed doorway with tile accent at entry



Fig. B.6.d.4 (2) columns, light fixtures and ornamental landscaping frame the primary building entrance

APPLIES TO ALL DOWNTOWN DEVELOPMENT



Fig. B.6.d.5&7. multiple entrances and generous landscaping

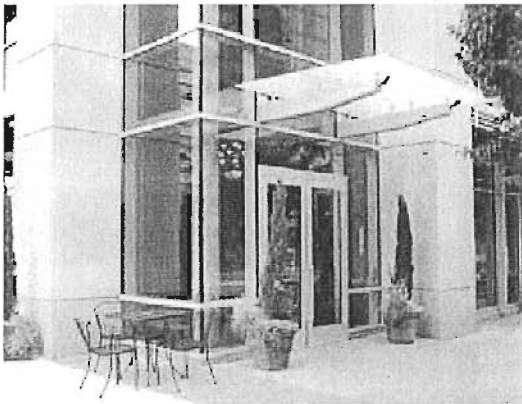


Fig. B.6.d.6. main residential entrance facing the street

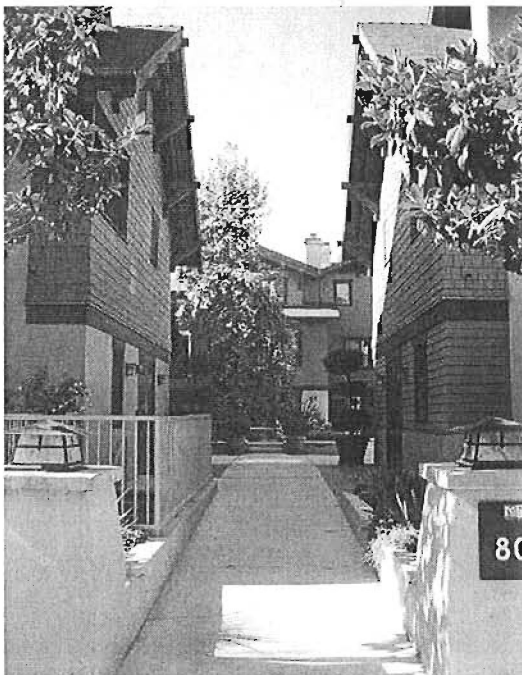


Fig. B.6.d.7. entry courtyard directly from the street into the site

6. Prominent Entrances, cont'd

Residential & Single-Family Attached

5. For developments with multiple ground-related units, such as townhouses, every unit facing the street shall have an entrance on the street.
6. The main residential entrance for multi-family buildings shall face the primary street.
7. Two or more of the following elements shall be used to highlight site entrances to multi-family developments:
 - a. special landscape treatment with seasonal color and flowering trees
 - b. entry courtyard
 - c. identifying building form such as a covered entry
 - d. unique pedestrian-scale lighting
 - e. prominent architectural feature, such as a trellis/arbor
 - f. ornamental gate and/or fence
 - g. water feature
 - h. seating
8. Ground-level entry patios are not permitted facing the street, unless landscaped screening is provided on all sides of the patio.
9. For multi-family developments (attached dwellings on a single lot), exterior corridors and stairs visible from the street are not permitted, except for main entry stair leading to the building entrance.

7. Roof lines

a. **Intent:** To break up the overall massing of the roof, enhance the quality and character of the building, and identify different functional areas within the building.

b. **Applicable Downtown Design Principles:**

- B. Promote Excellence in Design.
- E. Incorporate Sustainability.
- F. Context Sensitivity.
- H. Create Appropriate Transitions in Height, Bulk and Scale

c. **Guidelines**

All Developments

1. Express roofs in a visually interesting manner that complements the composition of the building and the surrounding area.
2. Green roofs that reduce stormwater runoff are encouraged.
3. Alter roof form and height to complement a residential building's mass and articulation.
4. Screening materials shall cover all four sides of mechanical units and appear as forms integrated into the design of the building.



Fig. B.7.c.1 over sized projecting eave highlights an important corner



Fig. B.7.c.3 combination of sloping roof forms and strong cornice line adds visual variety to the streetscape



Fig. B.7.c.2 rooftop terrace

7. Roof lines, cont'd

d. Standards

All Developments

1. Roof forms may be flat or sloped. All flat roofs shall employ trim, variation in brick coursing, a projecting cornice or projecting parapet to visually "cap" the building.
 - a. Cornices shall have a maximum projection of 2 feet
 - b. Parapets must be a minimum 36 inches with a maximum 2-foot projection
 - c. False storefront parapets, larger than 15% of the height of the supporting wall, are not permitted.
2. Sustainable Roof: Projects with greater than 40,000 square feet of floor area shall meet at least one of the following:
 - a. A vegetated roof for a minimum of 30% of the total roof surface; or
 - b. A system that collects rainwater for reuse on the building (e.g., site irrigation or gray water reuse) for a minimum of 50% of the total roof surface; or
 - c. An integrated solar panel system for a minimum of 30% of the total roof surface.
3. Sloping roofs shall include at least two of the following design elements:
 - a. Slope of at least 4:12
 - b. Two or more slope planes
 - c. Overhanging eaves extending at least 1 foot beyond the supporting wall
 - d. Acceptable sloping roofs include gable, hipped, shed, and butterfly roof forms
4. Mechanical and communication equipment and components shall be screened so that it is not visible at ground level from streets and other public spaces, including alleys.

It shall be screened in a manner that is compatible with the architectural character of the building. Appropriate screening for rooftop equipment includes parapet walls or fabricated enclosures such as panels. The Manager or Design Commission reserves the right to review screening of rooftop equipment by requesting sight line studies.

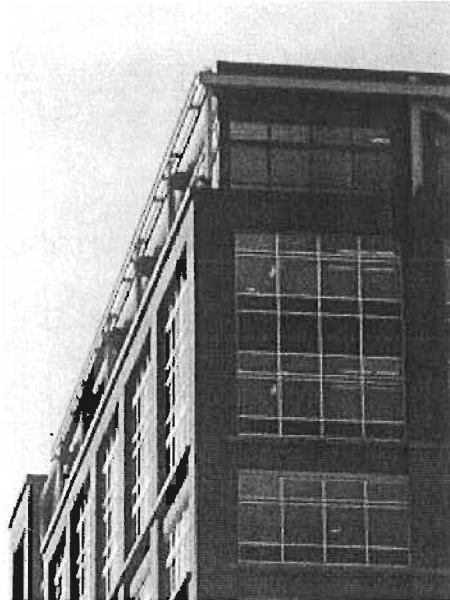


Fig. B.7.d.1 (1) treatment of a flat roof



Fig. B.7.d.1 (2) varied roof form helps break down the scale of the building



Fig. B.7.d.1 (3) cornice meeting projection standard

8. Corner Features

a. Intent: To allow unique design opportunities on corner lots because of their visibility and access from two streets.

b. Applicable Downtown Design Principles:

- B. Promote Excellence in Design.
- C. Create a Vibrant Public Realm.
- D. Create strong connections between Plan Districts and Sub-Districts.

c. Guidelines:

1. In some cases - particularly larger sites - visual and physical access into and through the site from public streets can aid in improving circulation Downtown. Ways of pulling back or punching through building corners at street intersections sufficient to promote visibility and to provide access through the site are encouraged.
2. Consider adding a focal point, such as a special building element, a water feature, an art sculpture or feature, or way finding element at the corner.
3. Employ especially high quality materials and distinct forms on the facade, roof, and ground level at the corner.

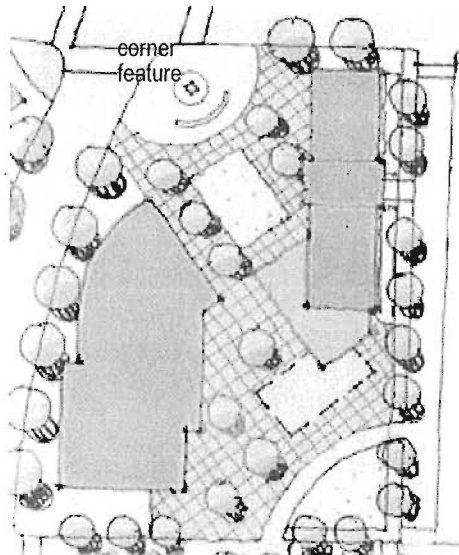


Fig. B.8.c.1 visibility and access into the site at an important corner



Fig. B.8.c.3 tower form at the corner

APPLIES TO ALL DOWNTOWN DEVELOPMENT



Fig. B.8.d.1 rounded corner treatment

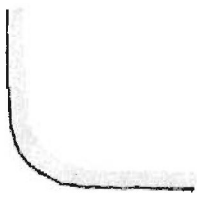


Fig. B.8.d.2 turret places strong architectural emphasis at the corner

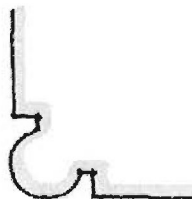
8. Corner Features, cont'd

d. Standards:

1. The orientation and massing of buildings on corner lots shall place strong visual emphasis on the street. Buildings shall include one of the features from the list at the bottom of the page (Diagrams 4.1151(B)(8)(d)(1)) or an alternative approved by the Manager or Design Commission.
2. Corner buildings shall include distinctive architectural expressions in its facade, including at least two of the following:
 - a. major entrance: double doors, large overhang, recessed entry
 - b. tower forms: minimum of 1/3 additional height to ground level floor
 - c. oversized windows: at least 1/3 larger pane than the rest of the ground level facade windows
 - d. expressive canopies: colors and materials distinctive from the rest of the building
 - e. higher bays: minimum 18 feet
 - f. plaza: minimum 600 square feet
 - g. forecourt: minimum 400 square feet
 - h. pilasters
 - i. cupola
 - j. turrets
3. T-111, EIFS, vinyl and split-face or standard concrete block shall not be allowed as an exterior finish material on corner facades of a building.



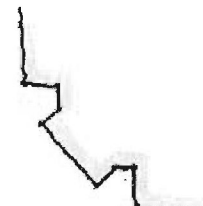
curved



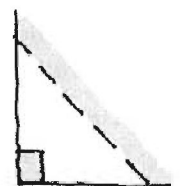
hinged



beveled



mitred



open bay w/ bevel

Diagrams 4.1151(B)(8)(d)(1): appropriate building corner treatments

9. Gateways

a. Intent: To create a strong architectural statement and/or enhance access at street corners to signify one's arrival into a district or important intersection, enhance way-finding and create visual interest and activity in the public realm.

b. Applicable Downtown Design Principles:

- B. Promote Excellence in Design.
- C. Create a Vibrant Public Realm.
- D. Create strong connections between Plan Districts and Sub-Districts.
- H. Create Appropriate Transitions in Height, Bulk and Scale

c. Guidelines:

Minor Gateways are:

- a. 6th and Eastman
- b. 3rd and Eastman
- c. Main and Powell
- d. Beech and Division, and
- e. Main and Division

1. Create a strong architectural statement at gateway locations to help create a strong identity
2. Use of highly crafted materials for lighting, paving, bollards and other streetscape elements or civic art pieces to accentuate the appearance of entrances and building form at all gateways.

Major Gateways are:

- a. Burnside and Eastman,
 - b. Division and Eastman,
 - c. Powell and Eastman,
 - d. Powell and Hogan, and
 - e. Division and Burnside.
3. In addition to the considerations above, place significant public space at the corner for major gateways.

Consult the District Specific Design Guidelines for additional guidance at the locations listed above.



Fig. B.8.c.1 (1) Prominent architectural features such as double story windows and entry plaza reinforce the corner and highlight entry to an important location



Fig. B.8.c.1 (2) fountain plaza at a major gateway

APPLIES TO ALL DOWNTOWN DEVELOPMENT



Fig. B.8.d.1 building form and main entrance orientation

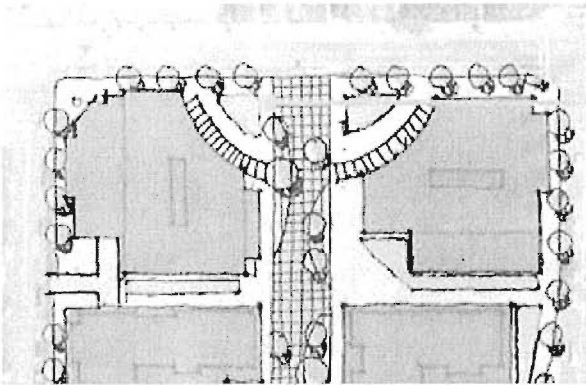


Fig. B.8.d.2 plaza design at a major gateway

9. Gateways

d. Standards:

Minor Gateways are:

- a. 6th and Eastman,
 - b. 3rd and Eastman,
 - c. Main and Powell,
 - d. Beech and Division, and
 - e. Main and Division.
1. The main double door entry shall be located within 10 feet of the corner of the building.
 2. Minor gateway sites shall incorporate at least three features from the following list:
 - a. major entrance: double doors, large overhang, recessed entry
 - b. tower forms: minimum of 1/3 additional height to ground level floor
 - c. oversized windows: at least 1/3 larger pane than the rest of the ground level facade windows
 - d. expressive canopies: colors and materials distinctive from the rest of the building
 - e. higher bays: minimum 18 feet
 - f. plaza: minimum 600 square feet
 - g. forecourt: minimum 400 square feet
 - h. pilasters
 - i. cupola
 - j. turrets

Major Gateways are:

- a. Burnside and Eastman,
 - b. Division and Eastman,
 - c. Powell and Eastman,
 - d. Powell and Hogan, and
 - e. Division and Burnside.
3. The main double-door entry shall be located within 10 feet of the corner of the building.
 4. Major gateway sites shall incorporate at least four features from the list above (B.9.d.2).

Consult the District Specific Design Standards for additional specific requirements at the locations listed above.

10. Blank Walls

a. **Intent:** To ensure that buildings do not display blank, unattractive walls to the abutting street or public areas.

b. Applicable Downtown Design Principles:

- B. Promote Excellence in Design.
- C. Create a Vibrant Public Realm.
- E. Incorporate Sustainability.
- F. Context Sensitivity.
- G. Preserve and Enhance Historic Character

c. **Guidelines**

1. Use vegetation, such as trees, shrubs, ground cover and/or vines adjacent to the wall surface. Green walls are strongly encouraged to manage stormwater runoff.
2. The use of façade articulation such as expressing the structural bays of the building with pilasters or other detailing shall be used to help animate an otherwise blank area of wall.
3. Use Artwork, such as bas-relief sculpture, murals or trellis structures. Use seating areas with special paving.
4. Use architectural detailing, reveals, and/or contrasting materials.

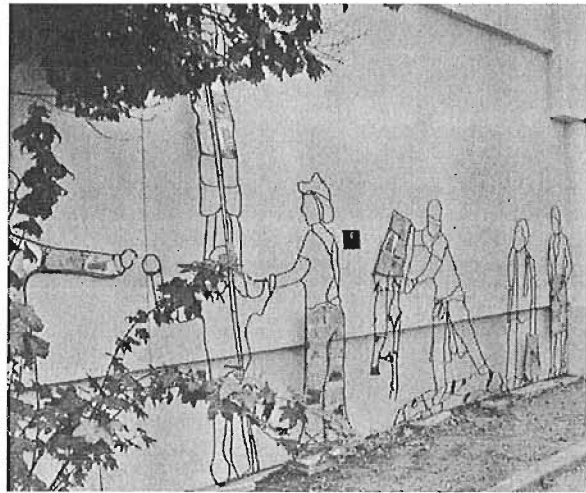


Fig. B.9.c.3 (1) blank wall with art work



Fig. B.9.c.3 (2) mural

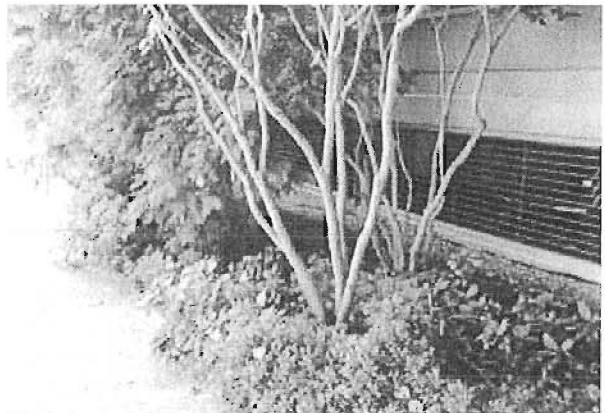


Fig. B.9.c.1 ground cover, shrubs, and trees

APPLIES TO ALL DOWNTOWN DEVELOPMENT

4.1151 Design Guidelines and Standards
All Downtown Sub-Districts
B. Building Design

10. Blank Walls, cont'd

d. Standards

1. A vertical vegetated, or "green," wall shall count towards meeting a project's required landscaping, except for parking lot landscaping.
2. Blank walls are not allowed on the façade facing the primary street. Blank walls shall use structural elements such as posts or columns every 30 feet.
3. On the side facades facing secondary streets, blank walls longer than 30 feet for small to medium buildings and longer than 50 feet for large buildings as defined in Section 4.1151(B)(1)(d)(1) fronting a public street shall incorporate three or more of the following throughout the length of the blank wall:
 - a. An architectural plinth (a stone or masonry base at least 36 inches high)
 - b. Belt course(s) of masonry
 - c. Trellis with vines planted that will grow vertically
 - d. Recesses at least 4 feet wide and 2 feet deep
 - e. Overhanging eave less than 12-18 feet from the ground level
 - f. Decorative tile work
 - g. Accent lighting
 - h. Artwork reviewed and approved for artistic merit by an arts review body designated by the City
 - i. Evergreen hedge
 - j. Seating (benches or ledges)

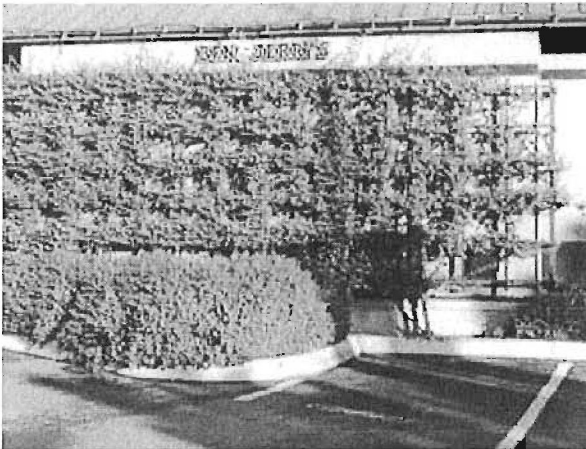


Fig. B.9.d.3 blank wall with trellis and vines

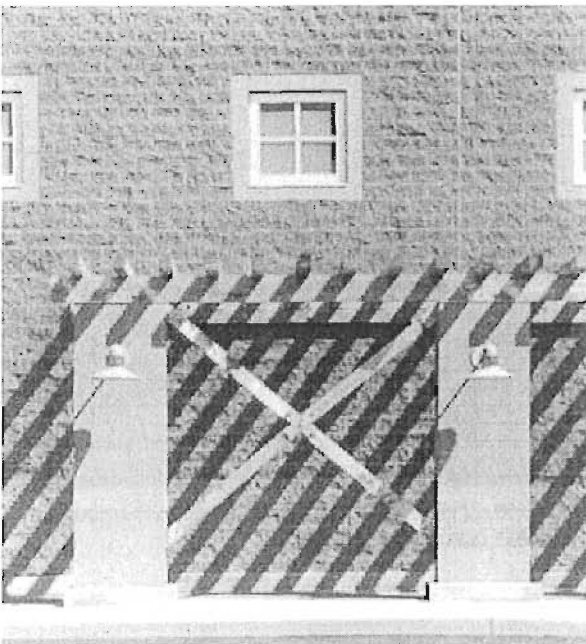


Fig. B.9.d.2 blank wall with trellis and lighting

11. Materials

Intent: To value each building as a high-quality, long-term addition to downtown.

b. Applicable Downtown Design Principles:

- B. Promote Excellence in Design.
- E. Incorporate Sustainability.
- F. Context Sensitivity.
- G. Preserve and Enhance Historic Character.

c. Guidelines

1. Smaller scale, natural materials are encouraged wherever possible, especially on the front façade and near pedestrian areas.

Developments are encouraged to use high quality materials common in Downtown's best buildings in contemporary ways, so long as visual cues with noteworthy buildings are established.

2. The use of sustainably harvested, salvaged, recycled or reused existing products is encouraged wherever possible.
3. When using more than one material on a facade, it is recommended to have one as the dominant theme with others serving to compliment or accentuate the design. For example, the main brick facade is accented with the use of special polished concrete block at the base, with matching concrete lintels to help tie the design together.
4. Employ durable and high-quality materials that show permanence and quality, minimize maintenance concerns and extend the life of the building.
5. Adjacent materials shall be compatible with each other in terms of heat and cold expansion for the long-term durability of the building.



Fig. B.10.c.1 creative materials create a lighter expression at the top of the building

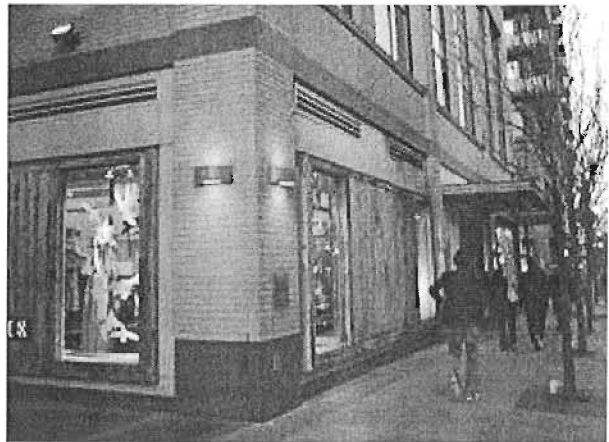


Fig. B.10.c.2 salvaged material integrated into the ground level facade in a prominent manner

APPLIES TO ALL DOWNTOWN DEVELOPMENT



Fig. B.10.d.1 (1) high quality finish materials are contrasted wisely to create a pleasing and unified design



Fig. B.10.d.1 (2) The use of IPE, a Brazilian hardwood, adds a natural and durable quality to the facade

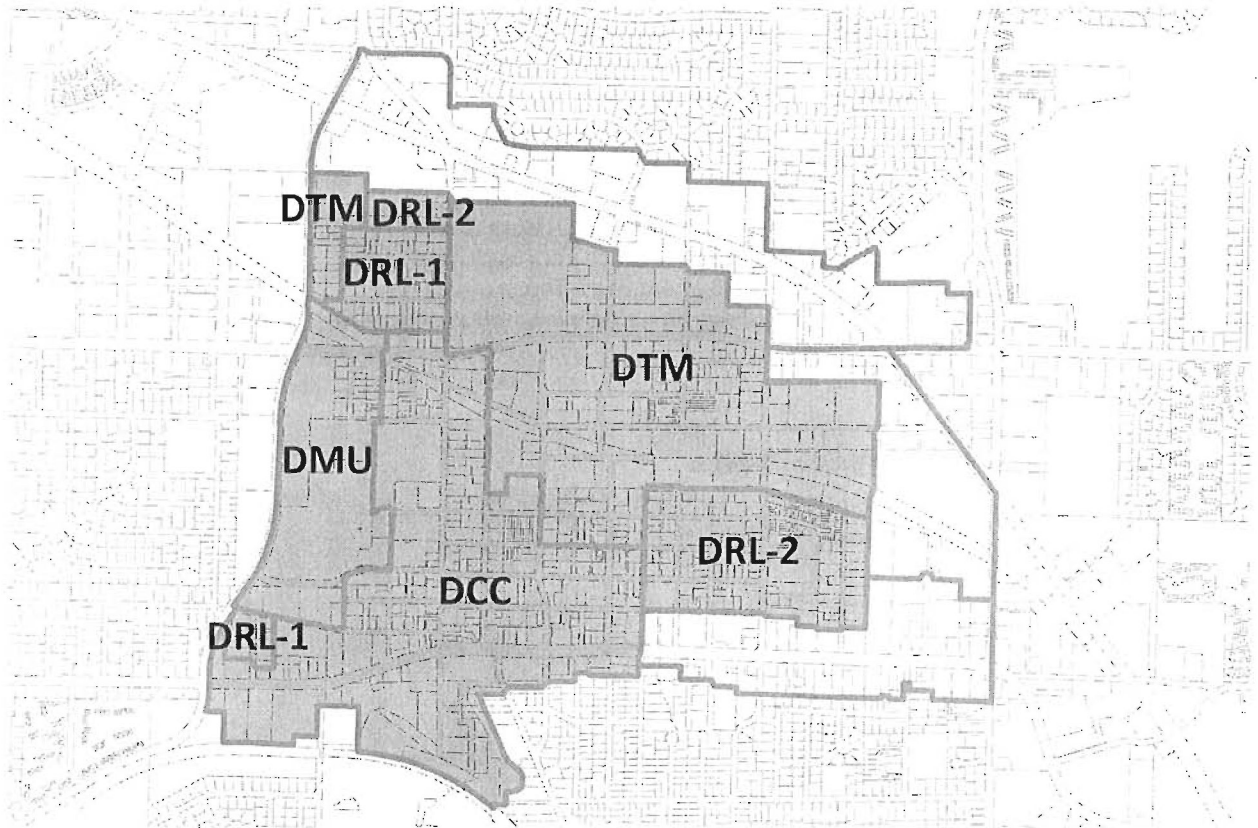
11. Materials, Cont'd

d. Standards:

1. The following primary materials shall be utilized on 65% of the building facade or greater:
 - a. Brick or tile masonry
 - b. Stucco
 - c. Wood siding and Wood Simulation Materials (such as Prodema)
 - d. Hardie-Plank
2. The following materials are prohibited as primary cladding on new building facades:
 - a. Vinyl
 - b. Corrugated metal
 - c. T-111 Plywood
3. The following secondary materials are permitted on no greater than 35% of building facade.
 - a. Metal
 - c. Pre-cast Masonry
4. The following materials are permitted as accent materials on no greater than 5% of the facade as trims or accents (e.g., flashing, projecting features, ornamentation, etc.)
 - a. Metals such as copper, iron, bronze and similar appearance metals as trims or accents
 - b. flashing, projecting features, ornamentation, etc.
 - c. Tile
 - d. Split-faced or standard block (for piers and foundation walls)
 - e. Gypsum Reinforced Fiber Concrete (for trim elements only)
5. Building Base Materials: Commercial ground level facades shall predominately consist of brick, tile masonry, or pre-cast masonry.
6. Change in Materials: Side and rear elevations that do not face a street, public parking area, pedestrian walkway or plaza and are not visible from an adjacent public street may utilize changes in materials in the interest of affordability, provided that the design is consistent with the overall composition of the building.

intentionally blank

Design Guidelines and Standards: Specific Downtown Sub-Districts



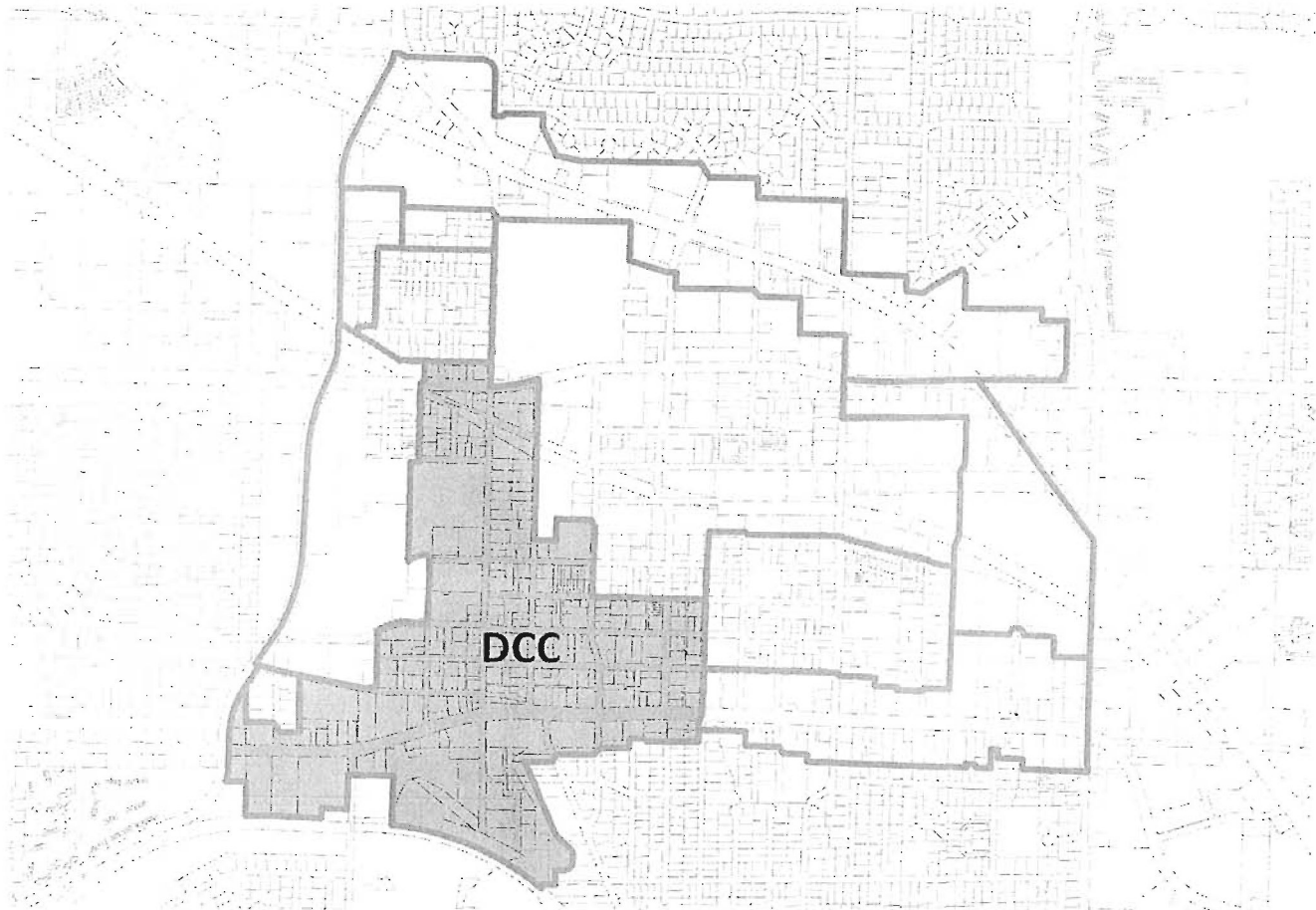
4.1152 Design Guidelines and Standards

Downtown Commercial Core (DCC)

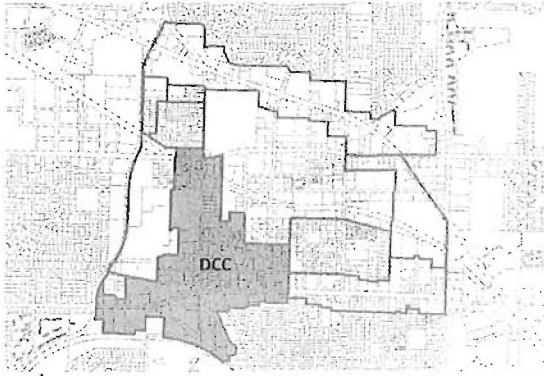
This is the City's long-standing center and features unique local businesses, small-scale storefronts, and intimate sidewalks. Main Avenue has a small-scale, walkable quality appreciated by residents. This Sub-District is intended to preserve this small-scale character on Main Avenue while encouraging an active, engaging mix of old and new. The DCC allows a wide range of uses – residential, office, retail, service – that will help create a vibrant Sub-District that is active most of the day.

This section does not apply to single-family detached or two-unit attached dwellings, unless specified in 4.1130. Guidelines and standards under the "All Development" heading or without a heading apply to all developments except single-family detached, two-unit attached, and single-family attached dwellings. Those under the "Non-Residential" heading apply to all development that are not residential, such as commercial, mixed-use, live-work, and employment uses. Those under the "Residential" heading apply only to attached dwellings on a single lot, unless otherwise specified.

Map 4.1152: Downtown Commercial Core Sub-District



APPLIES TO ALL DEVELOPMENT IN DISTRICT:



4.1152 Design Guidelines and Standards Downtown Commercial Core (DCC)

A. Site Design

4. Structured Parking/Parking Lot Screening



Fig. A.4.C.1 Parking garage structure lined by retail uses.

a. Intent: To mitigate breaks in the desired continuous building street wall.

b. Accompanies Design Principles:

A. Provide a Vibrant Mix of Uses.

C. Create a Vibrant Public Realm.

c. Guideline:

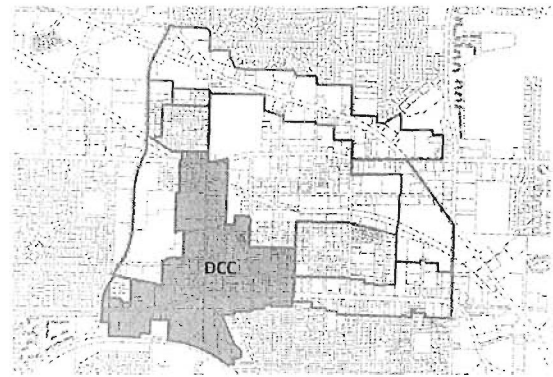
1 and 2. Facades on all sides of the building should be lined with activated uses such as, retail, residences with entries and windows, or offices.

d. Standards:

1. Parking lot screening: A structure, such as an architectural fence or low masonry wall 3 feet and 6 inches in height shall be built at the back edge of the sidewalk.
2. Structured parking facing pedestrian streets within the district shall be lined with uses that provide visibility into building interiors. Other sides of the building shall be treated with other architectural or landscaping means to break up and screen the blank wall surface.

4.1152 Design Guidelines and Standards
Downtown Commercial Core (DCC)
B. Building Design

APPLIES TO ALL DEVELOPMENT IN DISTRICT:



1. Massing

a. Intent: To reinforce the prevailing human scale quality of the district.

b. Applicable Downtown Design Principles:

- D. Create strong connections between Plan Districts and Sub-Districts.
- H. Create Appropriate Transitions in Height, Bulk and Scale

c. Guideline:

1. Employ design strategies and incorporate architectural elements that reinforce the Downtown core's unique and positive qualities; in particular, compatible design should respect the scale and massing of adjacent buildings.
2. New development should employ appropriate height and massing transitions to adjacent buildings.

d. Standards:

1. To integrate new development with the scale of its surroundings, buildings shall provide an upper level step back above 35 feet. This can be counted towards meeting the project's Downtown Massing requirement.
2. Within 50 feet of lower rise, residential district, the height of that portion of the structure within 50 feet of the property line shall not exceed 35 feet.

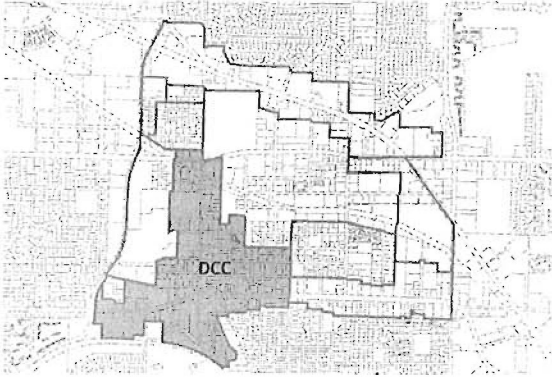


Fig. B.1.c.1 Simple massing emphasizes deep openings that create shadow lines in this "background" building.



Fig. B.1.d.1 Height steps down toward a residential zone

APPLIES TO ALL DEVELOPMENT IN DISTRICT:



4.1152 Design Guidelines and Standards
Downtown Commercial Core (DCC)

B. Building Design

2. Facade Articulation

a. Intent: To reinforce the positive architectural attributes of the immediate area and recognize the unique and evolving historical and cultural character of the downtown core.

b. Applicable Downtown Design Principles:

- B. Promote Excellence in Design.
- C. Create a Vibrant Public Realm.
- E. Incorporate Sustainability.
- G. Preserve and Enhance Historic Character.

c. Guideline:

1. A cohesive and continuous building street wall, composed of a stock of well-built, "background" buildings is the backbone of a quality downtown. However, buildings striving to have a heightened civic or social function are encouraged to include more visually prominent and dramatic architectural elements.
2. Consider retaining historically or architecturally significant buildings on a site through adaptive use.
3. Rehabilitate and restore historically significant buildings on a site according to the Secretary of Interior's Section 106 Standards.



Fig. B.2.c.2.1. former warehouse gets a new life as a residential building

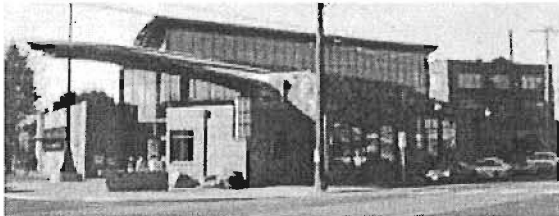
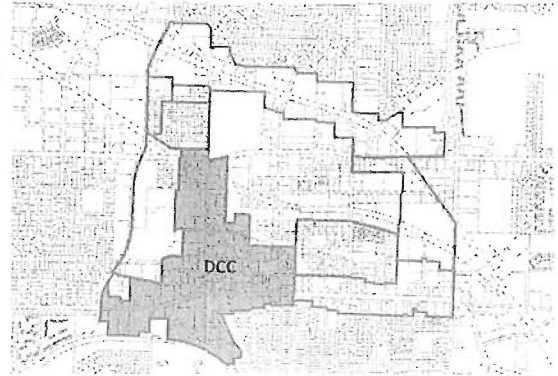


Fig. B.2.c.2.2. civic buildings can depart from conventional design standards



2. Facade Articulation, cont'd

d. Standards:

1. The architectural character of adjacent historic or noteworthy building shall be complemented, however imitation of historical styles is discouraged. New buildings shall accomplish this by taking visual cues from the immediate area and interpreting them in a contemporary manner. This includes:
 - a. Establishing a base, middle and top
 - b. Recessed, rather than strip windows on upper levels are required
 - c. Horizontal datum lines, such as belt lines and cornices shall line up with adjacent facades if applicable

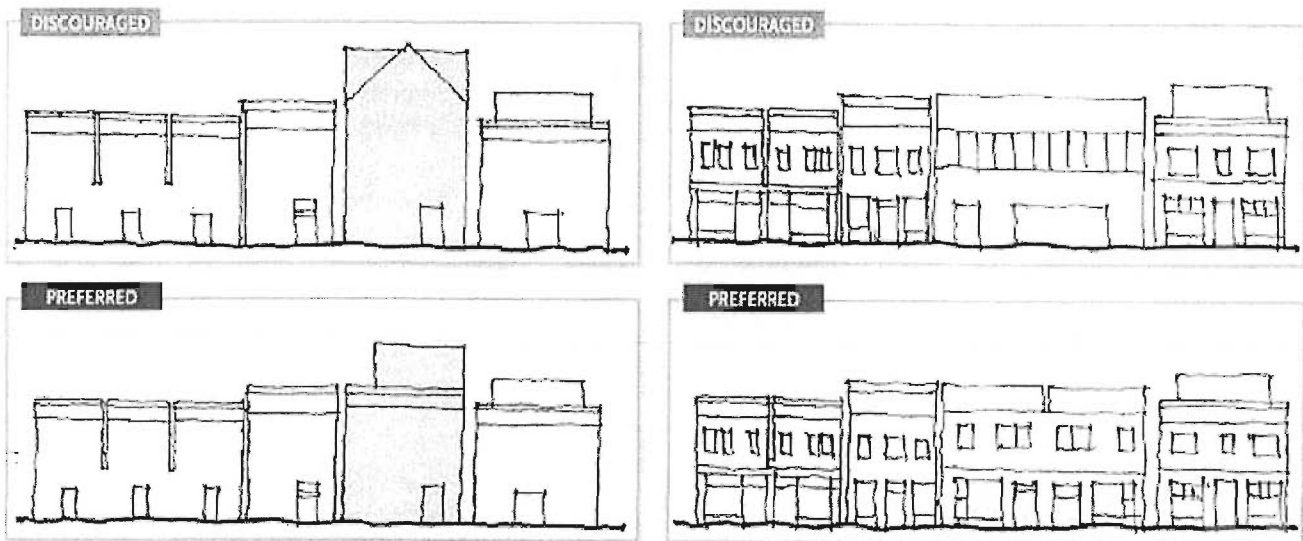
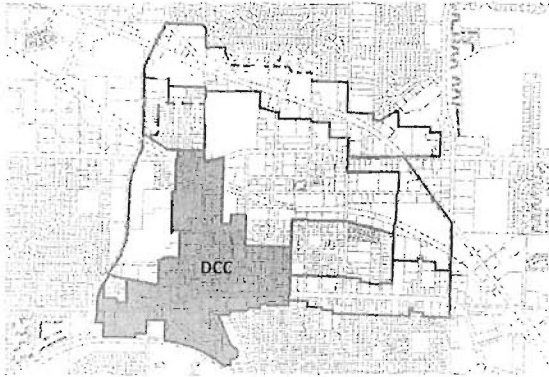


Fig. B.2.d.1 examples of cornice line treatment (left) and window treatments (right).

APPLIES TO ALL DEVELOPMENT IN DISTRICT:



4.1152 Design Guidelines and Standards
Downtown Commercial Core (DCC)
B. Building Design

3. Ground-Level Details

a. **Intent:** To accommodate public use at the ground floor in order to promote a socially and visually stimulating Downtown core.

b. **Applicable Downtown Design Principles:**

- B. Promote Excellence in Design.
- C. Create a Vibrant Public Realm.

c. **Guidelines:**

All Development

1. Give greater attention to the detail at the street level, including a variety of elements that emphasize artistry and uniqueness, such as artwork, color and materials.

Non-Residential

2. Elements such as separate storefront display windows, shop entrances, exterior light fixtures, awnings and overhangs should be designed to show creativity and individual expression in the design of the ground-level façade.

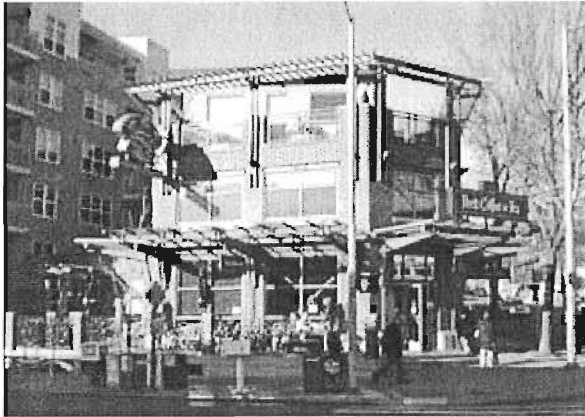
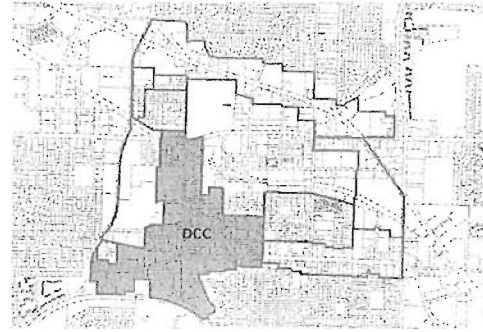


Fig. B.3.c.1 a robust array of building components and ornamental detail



APPLIES TO ALL DEVELOPMENT IN DISTRICT:



3. Ground-Level Details, cont'd

d. Standards:

Non-Residential

1. Commercial buildings shall be designed to encourage multi-tenant occupancy and walk-in traffic at the street level. The following shall be incorporated:
 - a. 15-20 foot-wide bays framed by columns or pilasters; and
 - b. Segmented sign band or fascia; and
 - c. Overhead cover such as canopies or awnings scaled and integrated into individual bays.
2. Five elements from the list in B.3, Ground Level Details in the Downtown-wide Design Standards shall be required.

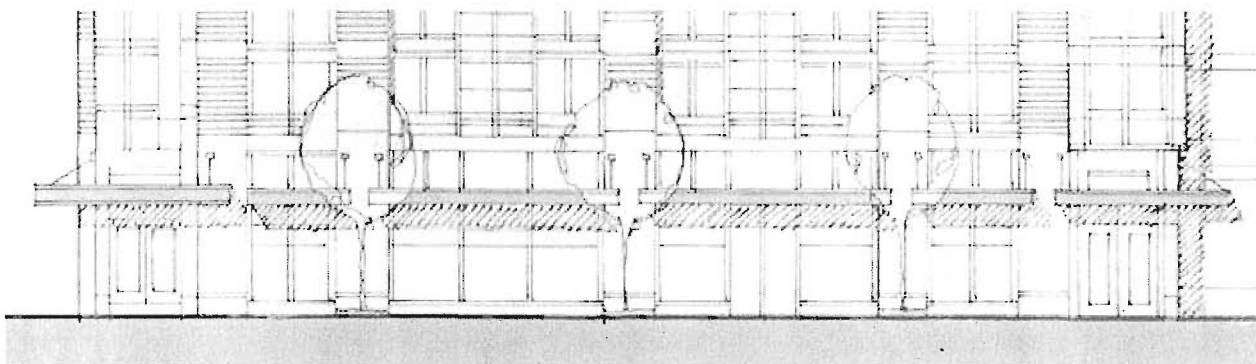


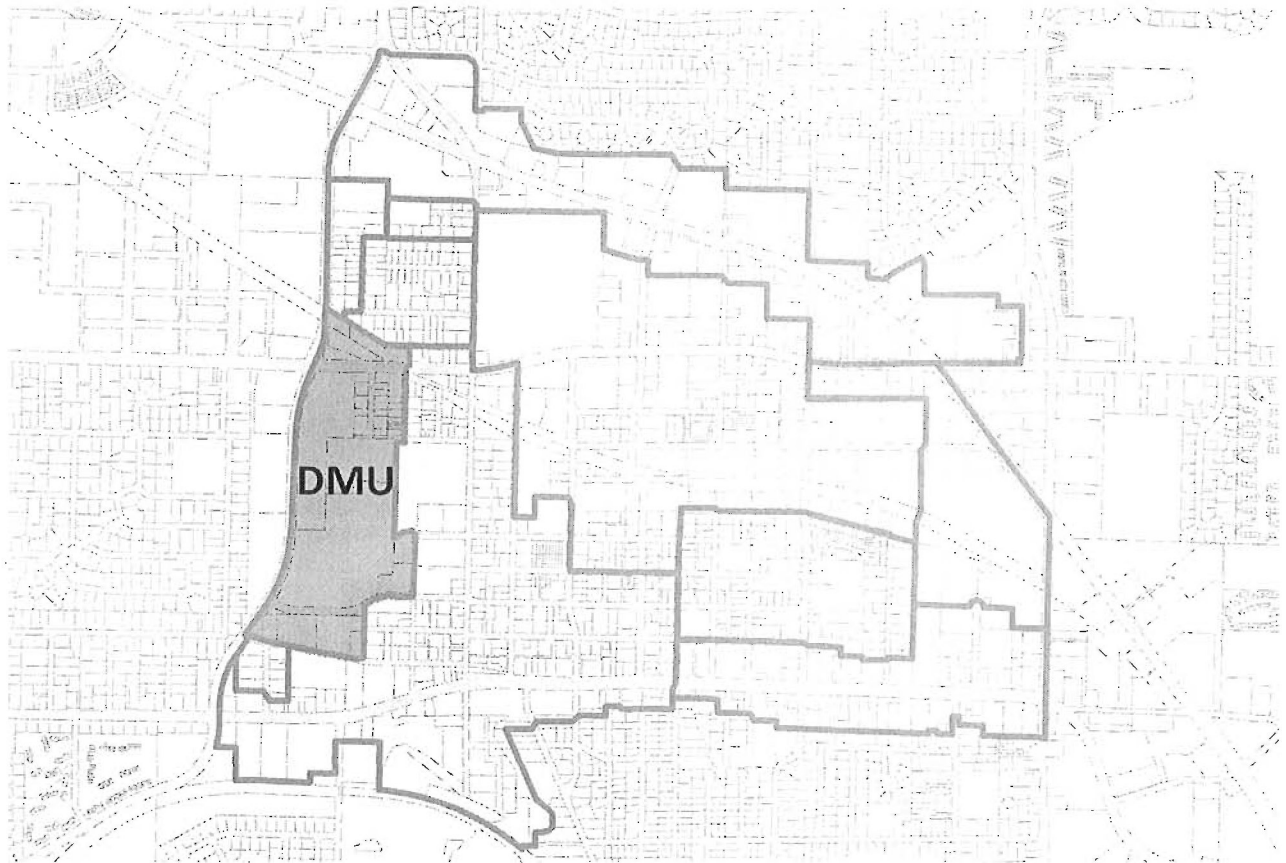
Fig. B.3.d.1 ground level details organized into 15- to 20-foot-wide intervals

4.1153 Design Guidelines and Standards: Downtown Mixed-Use (DMU)

This Sub-District is intended to evolve over time from shopping center largely organized around automobile trips and parking into a mixed-use Sub-District with jobs, housing and retail opportunities. This full-service Sub-District will contain new shopping streets, public spaces and better pedestrian and automobile connections to the DCC Sub-District. This Sub-District allows a mixture of residential, employment and retail uses. It allows denser urban housing as well as larger office buildings. It also allows the most intense, tallest development in Downtown.

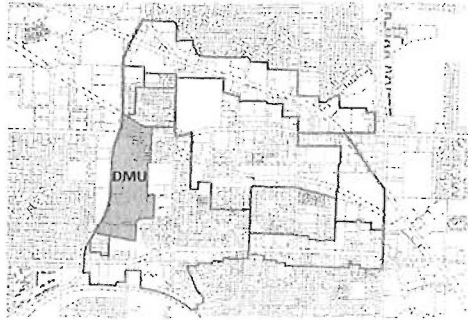
This section does not apply to single-family detached or two-unit attached dwellings, unless specified in 4.1130. Guidelines and standards under the "All Development" heading or without a heading apply to all developments except single-family detached, two-unit attached, and single-family attached dwellings. Those under the "Non-Residential" heading apply to all development that are not residential, such as commercial, mixed-use, live-work, and employment uses. Those under the "Residential" heading apply only to attached dwellings on a single lot, unless otherwise specified.

Map 4.1153: Downtown Mixed-Use Sub-District





APPLIES TO ALL DEVELOPMENT IN DISTRICT:



3. Walkways

a. **Intent:** To ensure that new development is sited and designed to encourage pedestrian safety and access, connections, and activity throughout the district.

b. **Applicable Downtown Design Principles:**

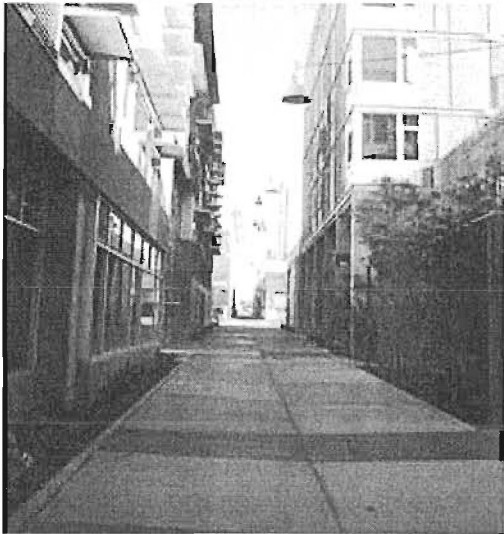
- C. Create a Vibrant Public Realm.
- D. Create strong connections between Plan Districts and Sub-Districts.

c. **Guidelines:**

1. Larger projects should incorporate frequent passageways.
2. Consider using architectural features to further enhance the pedestrian experience in these important community connectors:
 - a. Special paving
 - b. Landscaping
 - c. Lighting such as lighted bollards along the walkway and accent lighting on abutting structures
 - d. Trellis

d. **Standards:**

1. Through block walkways shall be required for new development every 200-300 feet.
2. Walkways shall be at least 10 feet wide and incorporate lighting for pedestrian safety.

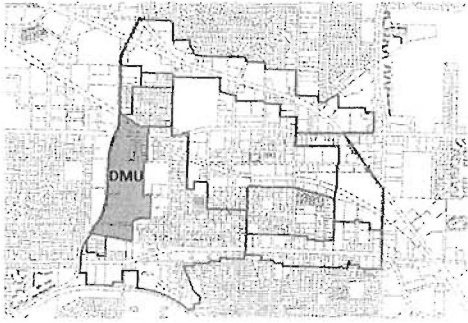


A.3.c.1 & 2 walkways with lighting and pedestrian amenities

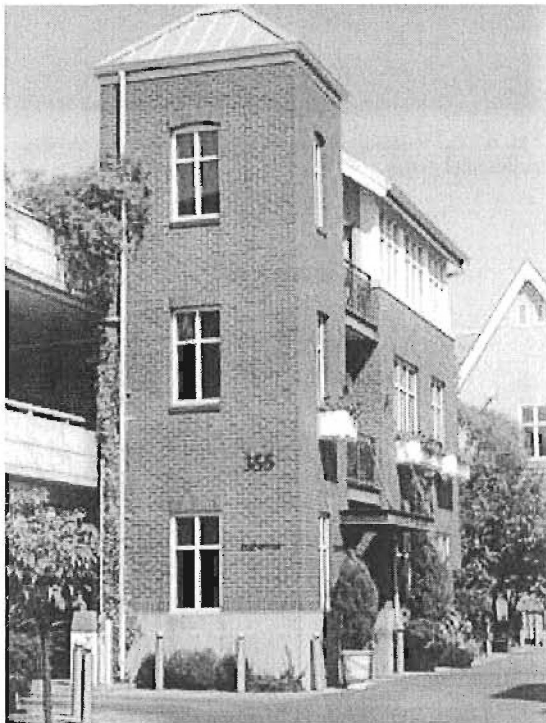


A.3.c 1 & 2 walkways with lush landscaping

APPLIES TO ALL DEVELOPMENT IN DISTRICT:



A.4.c.1 (1) active uses at the ground floor



A.4.c.1 (2) parking structure lined with well designed facade

4. Structured Parking Screening

a. Intent: To mitigate the potential blank wall surfaces of structured parking in high density, mixed use and commercial buildings envisioned for these Sub-Districts.

b. Applicable Downtown Design Principles:

- A. Provide a Vibrant Mix of Uses.
- C. Create a Vibrant Public Realm.

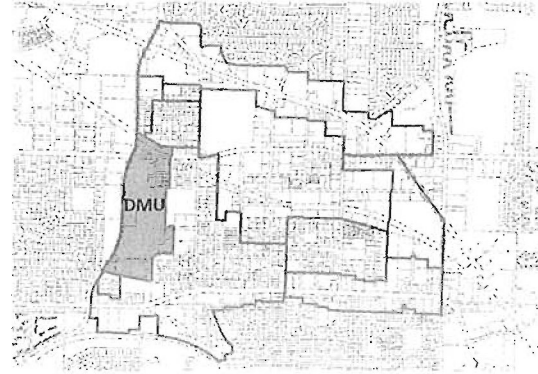
c. Guidelines:

1. Along urban commercial streets, facades on all sides of the building should be lined with active retail spaces or residences with entries and windows.
2. Uses lining parking structures should incorporate transparent glass to allow for visual interaction with the building interior from pedestrian areas.

d. Standards:

1. Structured parking facing pedestrian streets within the district shall be lined with uses that provide visibility into building interiors. Other sides of the building shall be treated with other architectural or landscaping means to break up and screen the blank wall surface.
2. Refer to ground-level details standards for appropriate treatment of activated facades.

APPLIES TO ALL DEVELOPMENT IN DISTRICT:



1. Massing

a. **Intent:** To create a step in perceived height, bulk and scale between buildings on zone edges.

b. **Applicable Downtown Design Principles:**

- B. Promote Excellence in Design.
- H. Create Appropriate Transitions in Height, Bulk and Scale.

c. **Guidelines:**

1. In some cases, careful siting and design treatment may be sufficient to achieve reasonable transition and mitigation of these impacts. For example, the use of architectural style, details (such as roof lines, cornices or fenestration), color, or materials that derive from the less intensive zone is one technique to consider.
2. Buildings should demonstrate, through the use of architectural style, details, color and materials that an appropriate transition from a less intensive to more intensive use zone is created.

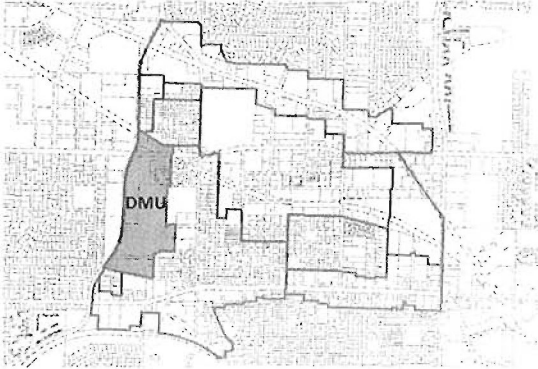
d. **Standard:**

1. Within 50 feet of Sub-Districts with a 45 feet height limit, no portion of the building shall be higher than 45 feet.



Fig. B.1.c.1 massing forms take cues from less intensive residential areas

APPLIES TO ALL DEVELOPMENT IN DISTRICT:



4.1153 Design Guidelines and Standards
Downtown Mixed Use (DMU)

B. Building Design

9. Gateways

a. **Intent:** To reinforce community gateways through the use of architectural elements, streetscape features, and public spaces.

b. **Applicable Downtown Design Principles:**

- C. Create a Vibrant Public Realm.
- D. Create strong connections between Plan Districts and Sub-Districts.
- E. Incorporate Sustainability.

c. **Guidelines:**

1. Gateways shall take advantage of opportunities to showcase sustainable features such as green walls, use of salvaged or recycled materials, unique storm water treatments, etc.
2. Gateways are transition locations and opportunities for announce that visitors are entering a special place. Developments at gateway locations should mark entry or departure points to a district for automobiles and pedestrians by providing a combination of significant public open space (plazas, fore-courts, greens), interesting building orientation and signage, lighting, artwork and façade treatments.
3. Gateways should embrace opportunities to frame or directly link other areas of interest.



Fig. B.8.c.2 (1) building form and generous ground floor height mark entry into the neighborhood business district



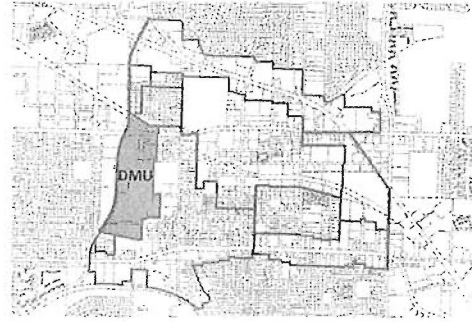
Fig. B.8.c.2 (2) marker embedded inside walk to denote area of special interest



Fig. B.8.c.3 gateway plaza leads the eye into the downtown core

4.1153 Design Guidelines and Standards
Downtown Mixed Use (DMU)
B. Building Design

APPLIES TO ALL DEVELOPMENT IN DISTRICT:



9. Gateways, cont'd

d. Standards:

1. Developments at gateway locations shall provide 1 square foot of public space for every 20 square feet of building floor area.
2. Gateway public space shall include the following:
 - a. hardscaping
 - b. landscaping
 - c. pedestrian scaled lighting
 - d. seating (formal or informal)
3. Buildings shall be sited to allow for visual connections to significant landmarks

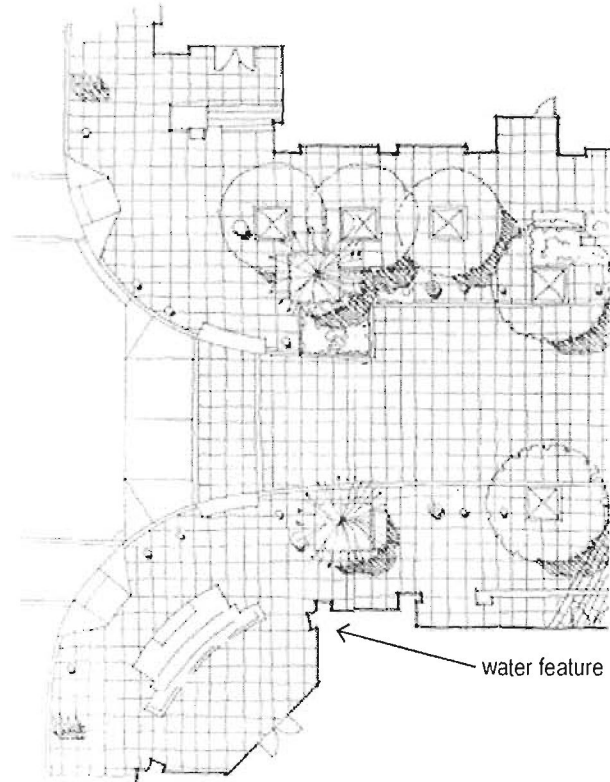


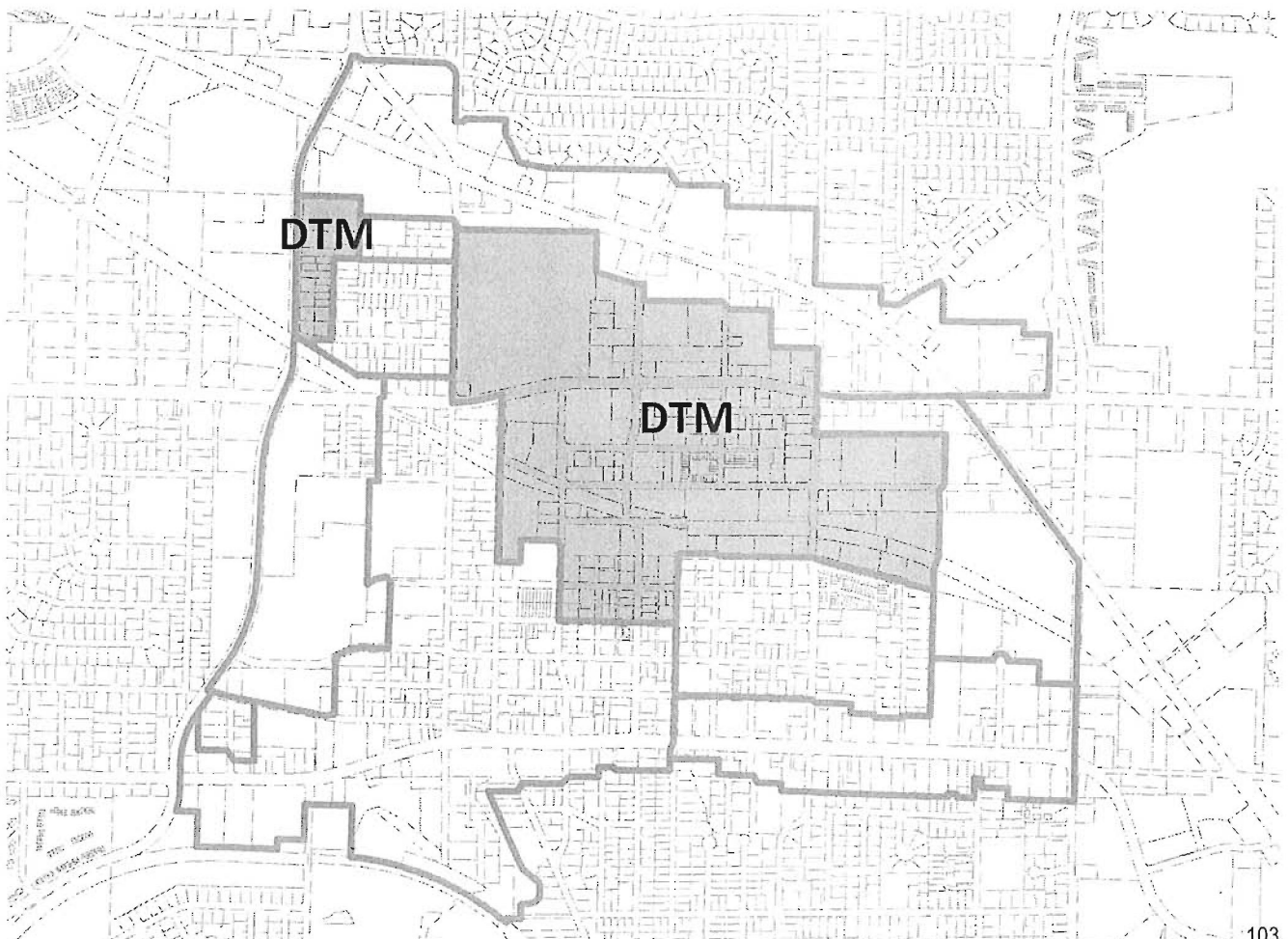
Fig. A.2.d.1 & 2 gateway plaza design with special hardscape, landscape and water features

4.1154 Design Guidelines and Standards: Downtown Transit Mid-Rise (DTM)

This mixed-use Sub-District provides a mid-rise, mixed-use character in the center of Downtown near its light-rail stations. Because of its proximity to transit, this Sub-District provides opportunities for those who live Downtown to buses and light rail to get to jobs and other destinations. It also supports the creation of employment uses Downtown so those who live outside the Downtown have easy access to work. It also supports the continued presence of institutional uses, such as government offices. This Sub-District allows a mix of residential, commercial and employment uses at a mid-rise intensity.

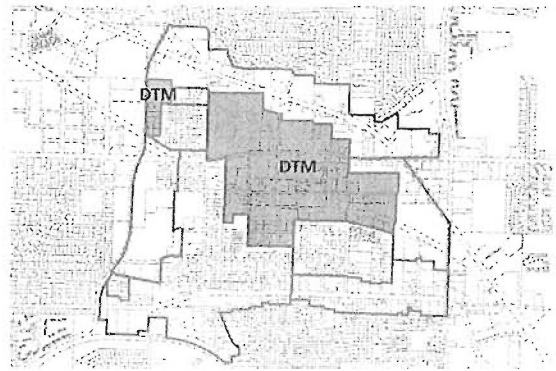
This section does not apply to single-family detached or two-unit attached dwellings, unless specified in 4.1130. Guidelines and standards under the "All Development" heading or without a heading apply to all developments except single-family detached, two-unit attached, and single-family attached dwellings. Those under the "Non-Residential" heading apply to all development that are not residential, such as commercial, mixed-use, live-work, and employment uses. Those under the "Residential" heading apply only to attached dwellings on a single lot, unless otherwise specified.

Map 4.1154: Downtown Transit Mid-Rise Sub-District





APPLIES TO ALL DEVELOPMENT IN DISTRICT:



1. Integrated Site Planning

a. **Intent:** To limit breaks in the street wall and minimize conflicts between vehicles and pedestrians.

b. **Applicable Downtown Design Principles:**

C. Create a Vibrant Public Realm.

c. **Guideline:**

- 1. Design alleys as attractive, safe and accessible public spaces.

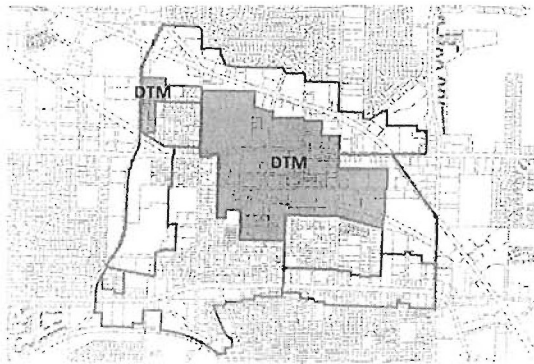
d. **Standard:**

- 1. For development sites greater than 40,000 square feet, preference for vehicle access shall be the alley (new or existing).



Fig. A.1.c.1 example of an appealing alley environment

APPLIES TO ALL DEVELOPMENT IN DISTRICT:



4.1154 Design Guidelines and Standards
Downtown Transit Mid-Rise (DTM)

A. Site Design

2. Publicly Accessible Spaces

a. Intent: To maximize opportunities for creating usable, attractive, well-integrated open space.

b. Applicable Downtown Design Principles:

- C. Create a Vibrant Public Realm.
- D. Create strong connections between Plan Districts and Sub-Districts.

c. Guidelines:

1. Encourage the presence of a well-defined outdoor space with visual and physical connection to the street. Consider linking outdoor spaces with comfortable paths to create a network of spaces.
2. Low Impact Development design strategies are especially encouraged, such as: rain gardens, pervious paving, xeriscaping, etc.

d. Standard:

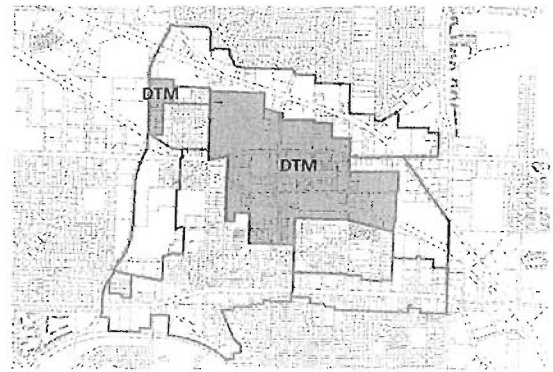
1. Ground-level open spaces shall be designed to contribute to a "livable neighborhood" urban setting and must incorporate one of the following:
 - a. Planted interior courtyard, minimum 600 square feet or 5% of the lot, whichever is greater
 - b. fore-court, minimum 600 square feet or 5% of the lot, whichever is greater
 - c. gardens, minimum 300 square feet
 - d. shared space design that incorporates access for a variety of travel modes, recreation/leisure opportunities and/or open space, if approved by the Manager of Design Commission.



Fig. A.1.c.1 courtyard housing with entries facing directly on open space



APPLIES TO ALL DEVELOPMENT IN DISTRICT:



3. Walkways

a. **Intent:** To ensure that new development are sited and designed to encourage pedestrian access, connections, and activity throughout the district.

b. **Applicable Downtown Design Principles:**

- D. Create strong connections between Plan Districts and Sub-Districts.

c. **Guideline:**

1. Larger projects should incorporate frequent passageways. Consider using architectural features to further enhance the pedestrian experience in these important community connectors:
 - a. Special paving
 - b. Landscaping
 - c. Lighting such as lighted bollards along the walkway and accent lighting on abutting structures
 - d. Trellis

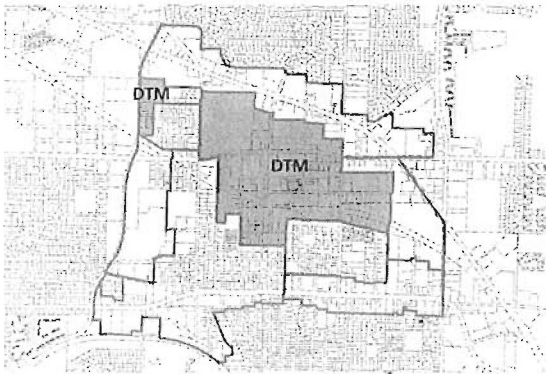
d. **Standard:**

2. Through block walkways shall be required for new development every 200 feet. Walkways shall direct pedestrians from parking areas to buildings, streets and public spaces.



Fig. A.3.c.1 network of pedestrian walkways is particularly important to support a transit oriented setting

APPLIES TO ALL DEVELOPMENT IN DISTRICT:



4.1154 Design Guidelines and Standards
Downtown Transit Mid-Rise (DTM)

B. Building Design

1. Massing

a. **Intent:** To respect urban residential development patterns.

b. **Applicable Downtown Design Principles:**

F. Context Sensitivity.

c. **Guideline:**

1. Employ other design strategies and incorporate architectural elements to break up the building into intervals appropriate for an urban residential setting.

d. **Standard:**

1. To integrate new development with the scale of its surroundings, no residential or mixed-use building mass shall be longer than 200 feet.

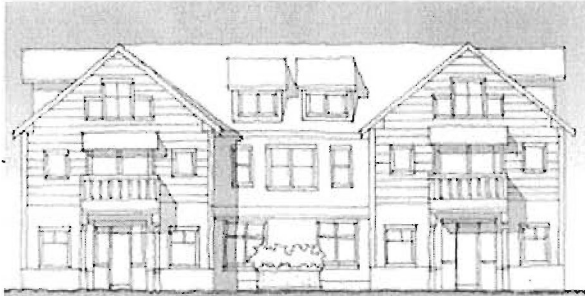


Fig. B.1.c.1 (1) residential building elements such as balconies, dormers and exterior staircases can be used to break up the massing of larger residential buildings

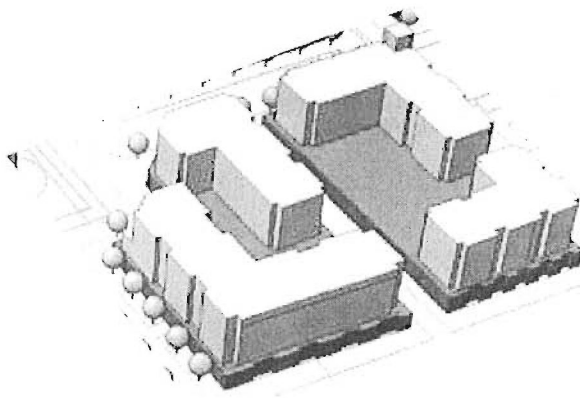
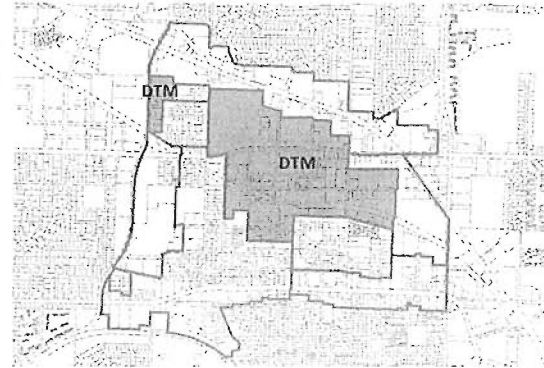


Fig. B.1.c.1 (2) individual building mass limited to 200 feet in length

APPLIES TO ALL DEVELOPMENT IN DISTRICT:



2. Facade Composition

a. **Intent:** To design building facades that reflect the residential uses contained within.

b. **Applicable Downtown Design Principles**

- B. Promote Excellence in Design.
- H. Create Appropriate Transitions in Height, Bulk and Scale

c. **Guidelines:**

1. Appropriate massing transitions such as stepping back the facade, should be considered for developments adjacent to low rise residential uses
2. Incorporate design elements to reflect residential character, such as changes in materials and openings that create shadow lines.

d. **Standards:**

1. Urban housing shall place a strong visual emphasis on the street with offsets, bay windows or projecting or recessed balconies every 30 feet.
2. Buildings shall be no taller than 45 feet within 50 feet of low rise residential uses.



Fig. B.2.c.1 & 2 an attractive facade rhythm using traditional residential building components such as bay windows, stoops and double hung windows

APPLIES TO ALL DEVELOPMENT IN DISTRICT:

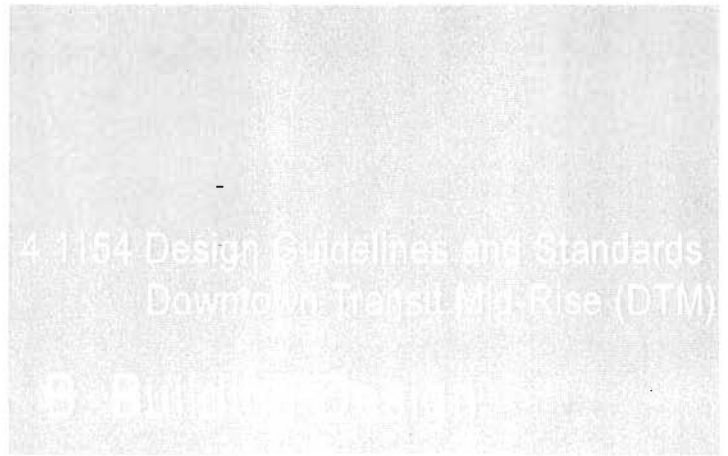
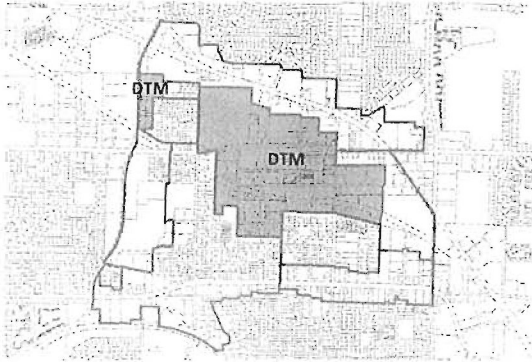


Fig. B.3.c.1 a live-work facade

3. Ground Level Details

a. Intent: To accommodate public use at the ground floor in order to promote a social interaction.

b. Applicable Downtown Design Principles

- A. Provide a Vibrant Mix of Uses.

c. Guideline:

All Development

- 1. Consider designing the first level of the building to accommodate commercial or live/work uses in the future.

d. Standards:

All Development

- 1. Main building entrances shall be oriented to directly face the primary street.

Residential

- 2. The ground level of new developments shall be designed to commercial standards.

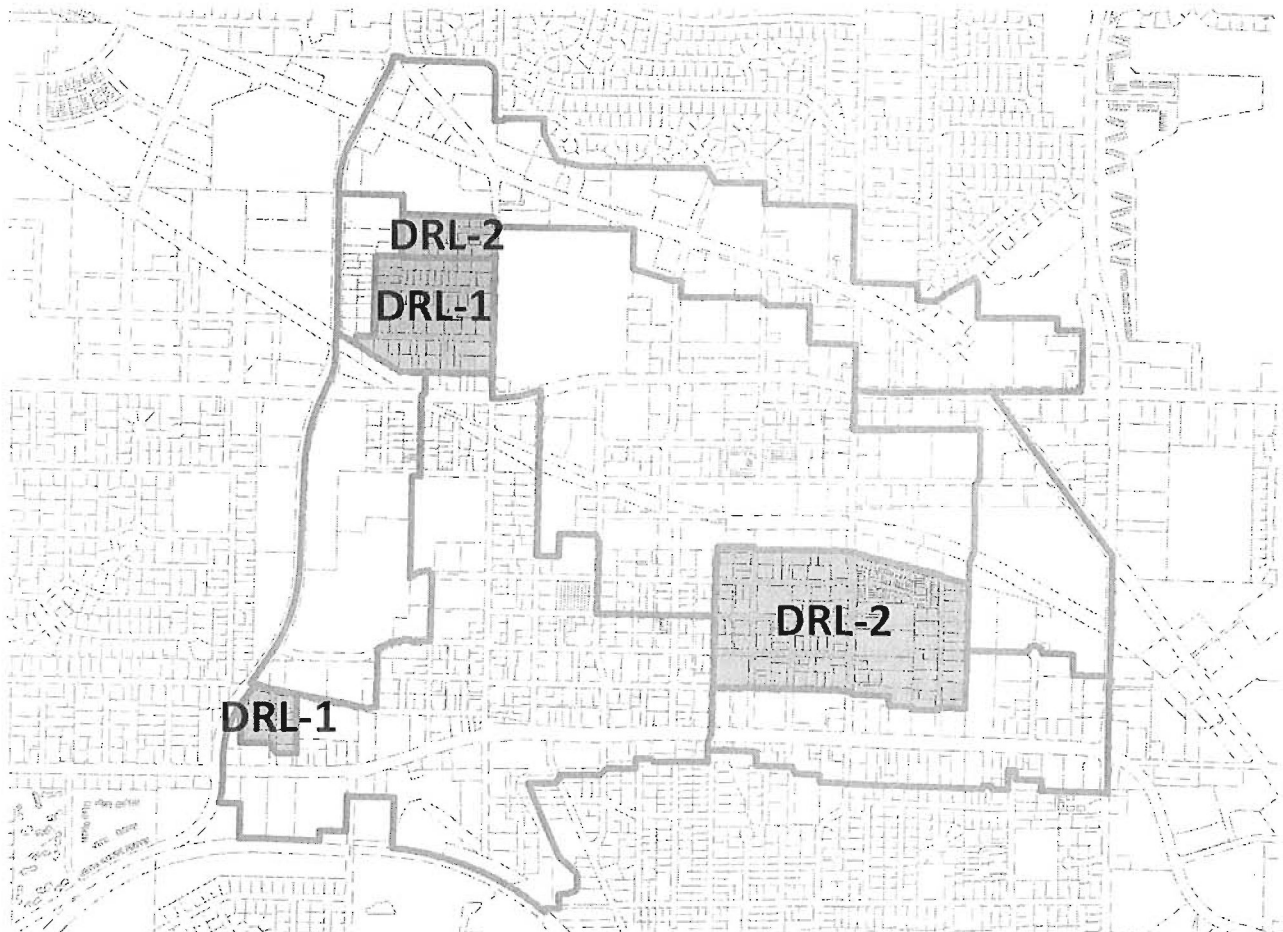
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4.1155 Design Guidelines and Standards Downtown Residential Low-Rise (DRL)

This Sub-District encourages sensitive multi-family infill development in established residential neighborhoods to gently add a broader mix of housing choices and provide more density to take advantage of the area's proximity to transit and nearby shopping and job centers. Single-family houses can remain, and some limited, small-scale commercial uses are allowed where appropriate. This Sub-District encourages residential buildings to add to the quality and character of Downtown, and it is intended to create distinctive, walkable neighborhoods within a short distance of transit and the Downtown core.

This section does not apply to single-family detached or two-unit attached dwellings, unless specified in 4.1130. Guidelines and standards under the "All Development" heading or without a heading apply to all developments except single-family detached, two-unit attached, and single-family attached dwellings. Those under the "Non-Residential" heading apply to all development that are not residential, such as commercial, mixed-use, live-work, and employment uses. Those under the "Residential" heading apply only to attached dwellings on a single lot, unless otherwise specified.

Map 4.1155: Downtown Residential Low-Ris Sub-District



1. Integrated Site Planning

a. **Intent:** To respect the existing development patterns in the area.

b. **Applicable Downtown Design Principles:**

- F. Context Sensitivity.
- H. Create Appropriate Transitions in Height, Bulk and Scale

c. **Guidelines:**

Residential

1. Consider single-family development patterns in the front yard site design of multi-family development when adjacent to single family developments.
2. Consider a combination of landscaping and structural elements to buffer multi-family activities from adjacent single-family residences.

d. **Standards:**

Residential

1. When a multi-family project is adjacent to a single-family residence, the building's front setback shall be within 5 feet of the setback provided by the single-family residence. For example, if the single-family setback is 20 feet, then the multi-family setback shall be set back 15 feet. This standard shall not cause a front setback to exceed 15 feet.
2. A 10-foot buffer shall be provided between the multi-family development and property lines abutting a single family line. Site obscuring landscaping shall be required, including a 6-foot, site-obscuring fence with shade trees planted a maximum 30 feet on center (2-inch caliper at planting); and/or other landscaping to provide visual buffering approved by the Manager or Design Commission.

APPLIES TO ALL DEVELOPMENT IN DISTRICT:

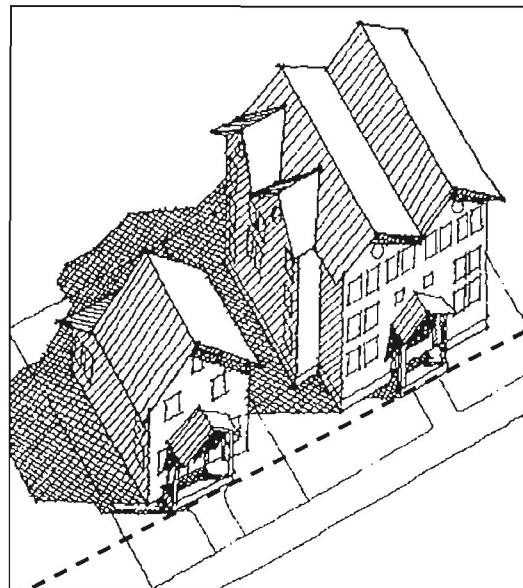
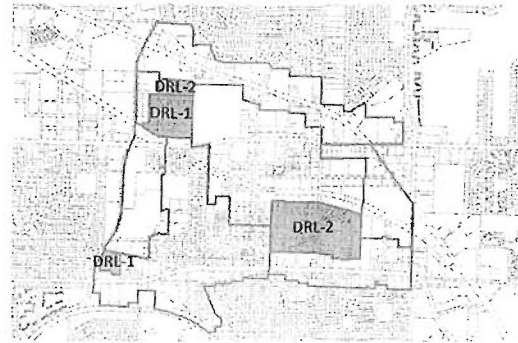
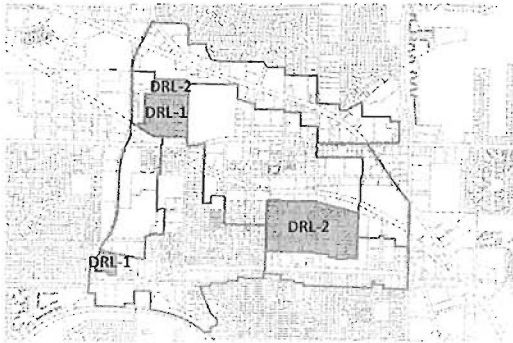


Fig. A.1.c.1 front yard setback compatibility between multi-family development (right) and single family residence (left)

APPLIES TO ALL DEVELOPMENT IN DISTRICT:



4.1155 Design Guidelines and Standards Downtown Residential Low-Rise (DRL) A. Site Design

2. Building Orientation

a. Intent: To provide sociable places that offer a sense of security.

b. Applicable Downtown Design Principles:

C. Create a Vibrant Public Realm.

c. Guidelines:

Residential

1. Consider providing multiple residential building entrances that activate and are oriented to the street.
2. Consider providing entry porches and stoops as a transitional space between the sidewalk and residential building.

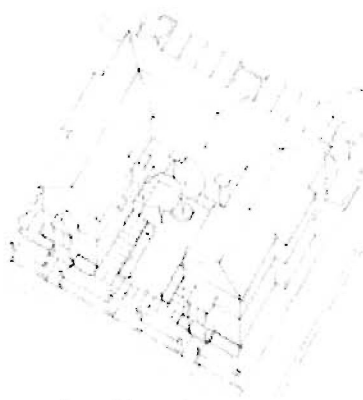
d. Standards:

Residential

1. The front of the buildings within 25 feet of the front line, except for frontages on courtyard space, shall include primary building entrance(s) that face the street.
2. Off-street parking or vehicular circulation shall not be allowed between a building and a street.



Fig. A.2.c.1 & 2 multiple entries and stoops enliven the street with minor grade separations to protect residential privacy



courtyard housing

Fig. A.2.c.1 variety of housing types with entrances facing to the street

A. Site Design

3. Publicly Accessible Open Space

a. Intent: To create spaces that are inviting and usable and integrated with the overall design of the project.

b. Accompanies Design Principles:

- E. Incorporate Sustainability.
- F. Context Sensitivity.

c. Guidelines:

Residential

1. Incorporate common outdoor spaces with multi-family projects. Locate and size these spaces so that they are functional and support interaction among residents.
2. Provide a convenient system of outdoor passages that connect residential outdoor spaces and tie these spaces to sidewalks, where feasible.
3. Incorporate pervious pavement and other sustainable features in the design of landscaping and outdoor furnishings.

d. Standards:

Residential

1. Inclusive of required yards, a minimum of 15% of the gross site area shall be reserved as open space.
2. For multi-family development at densities greater than 15 units per acre, a minimum of 0.15 square feet of common open space shall be required for each square foot of gross residential floor area.

Common open space areas provided to comply with this standard shall be at least 500 square feet with no horizontal dimension less than 15 feet.

APPLIES TO ALL DEVELOPMENT IN DISTRICT:

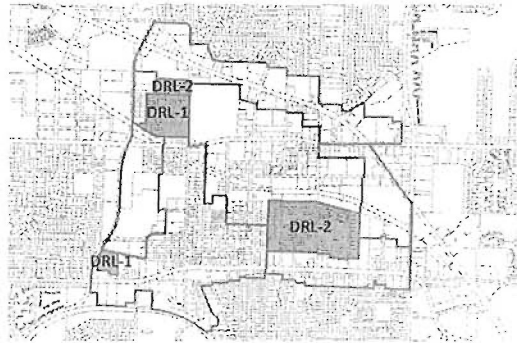


Fig. A.3.c.1 common open space designed to support a range of activities



Fig. A.3.c.3 generous amounts of landscaping and pervious surface

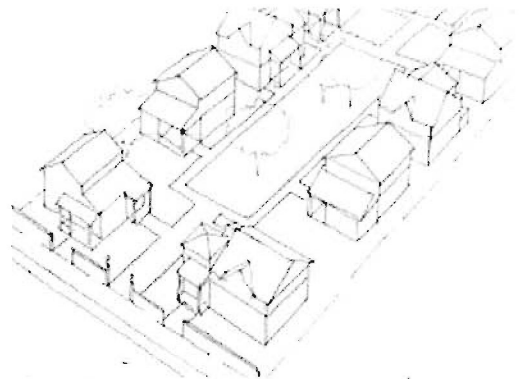


Fig. A.3.c.2 required common open space for multi-family development

APPLIES TO ALL DEVELOPMENT IN DISTRICT:

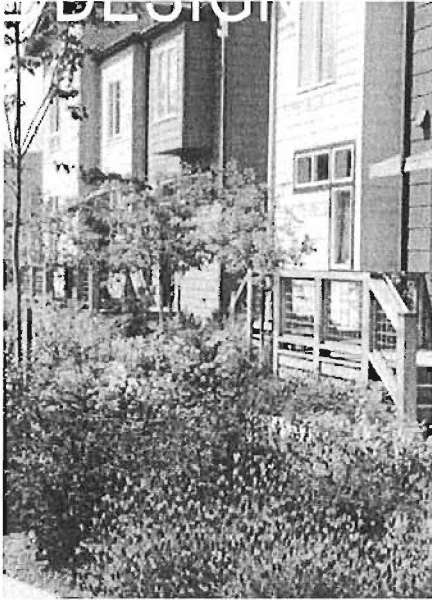


Fig. A.4.c.1 rich arrangement of understory, shrubbery and trees complements the colorful building design



Fig. A.4.c.2 landscaping in front setback and the sidewalk

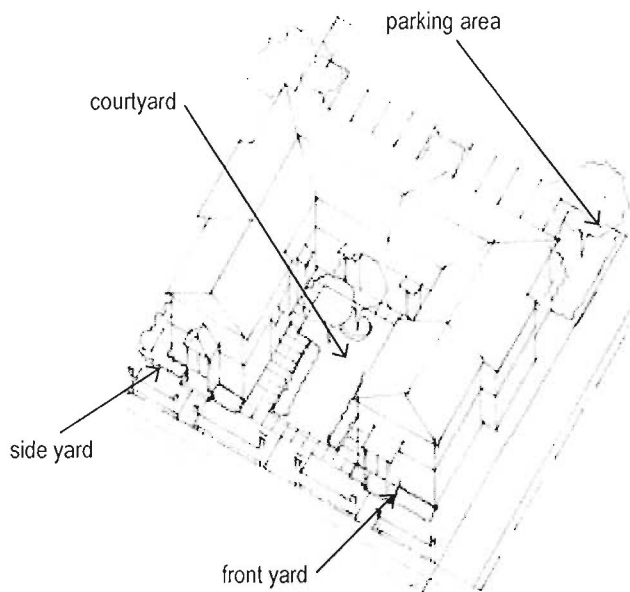


Fig. A.4.c.1 landscaping on a site to meet coverage requirement

4:1155 Design Guidelines and Standards Downtown Residential Low-Rise (DRL)

A. Site Design

4. Landscaping

a. Intent: To add landscaping to enhance the residential character of a development and minimize impacts on adjacent properties.

b. Applicable Downtown Design Principles:

- E. Incorporate Sustainability.
- F. Context Sensitivity.
- H. Create Appropriate Transitions in Height, Bulk and Scale

c. Guidelines:

All Development

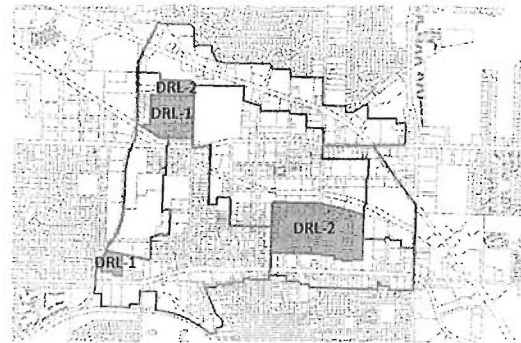
1. Use a landscape palette that reflects the climate and positive landscape character found in the neighborhood. In general, use a rich yet coordinated palette of landscape materials to provide scale, texture and color.
2. Extend the landscape character to the public realm by providing shade trees in the planting strip and understory that will enhance the street level environment.

d. Standards:

All Development

1. A minimum of 15% of the site shall be landscaped with a mix of vegetative ground cover, shrubbery and trees. Trees shall be planted a minimum 2 inches (dbh) in caliper and shrubbery a minimum of 24 inches in height.
2. Planting strips shall be required for all multi-family development on streets. Street trees shall be placed one per every 30 linear feet of street frontage.
3. The use of native and/or drought-tolerant landscaping shall be required, unless alternative plant selection is approved by the Manager or Design Commission. The property owner shall maintain all landscaping.

APPLIES TO ALL DEVELOPMENT IN DISTRICT:



5. Pedestrian Circulation

a. **Intent:** To provide convenient ways to move through larger development sites, such as from residence to the street, parking areas and open spaces.

b. **Applicable Downtown Design Principle:**

F. Context Sensitivity.

c. **Guidelines:**

Residential

1. Establish clear pedestrian connections on-site that are well-marked and accommodating; direct pedestrians to buildings, parking, streets and open spaces.

d. **Standards:**

Residential

1. Developments with more than 20 units shall provide pedestrian circulation meeting the following standards:
 - a. An internal walkway system shall connect all buildings on the site and shall connect the dwelling units to parking areas, bicycle parking, storage areas, all recreational facilities and common areas, and abutting sidewalks and pedestrian trails.
 - b. Surface treatment shall be concrete or masonry pavers, at least 5 feet wide.

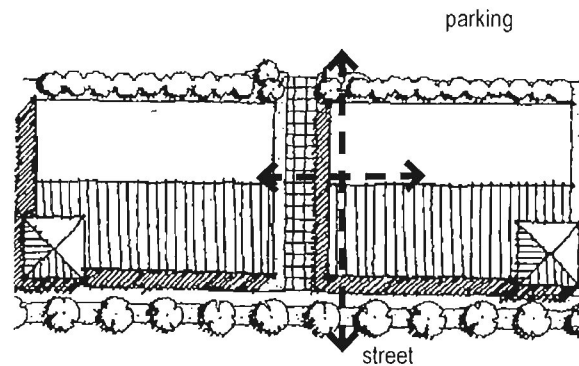
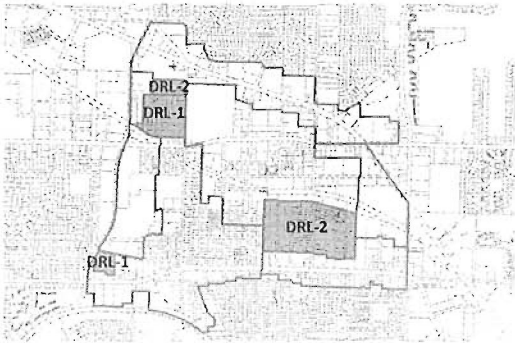


Fig. A.5.c.1 Internal walkway system connections units to parking and the street

APPLIES TO ALL DEVELOPMENT IN DISTRICT:



4.1155 Design Guidelines and Standards Downtown Residential Low-Rise (DRL)

B. Building Design

1. Massing

a. **Intent:** To break down the massing of multi-family development to provide a reasonable scale compatibility with existing residential properties nearby

b. **Applicable Downtown Design Principles:**

- F. Context Sensitivity.
- H. Create Appropriate Transitions in Height, Bulk and Scale

c. **Guideline:**

1. Consider dividing the structure into modules that are similar in scale to surrounding buildings, if smaller, to reduce the perceived size of a building.

d. **Standards:**

1. Structures shall not have a continuous horizontal distance exceeding 120 feet.
2. Roofs shall have gable, hip or other sloping form (minimum pitch 4:12) with at least a 12-inch overhang.

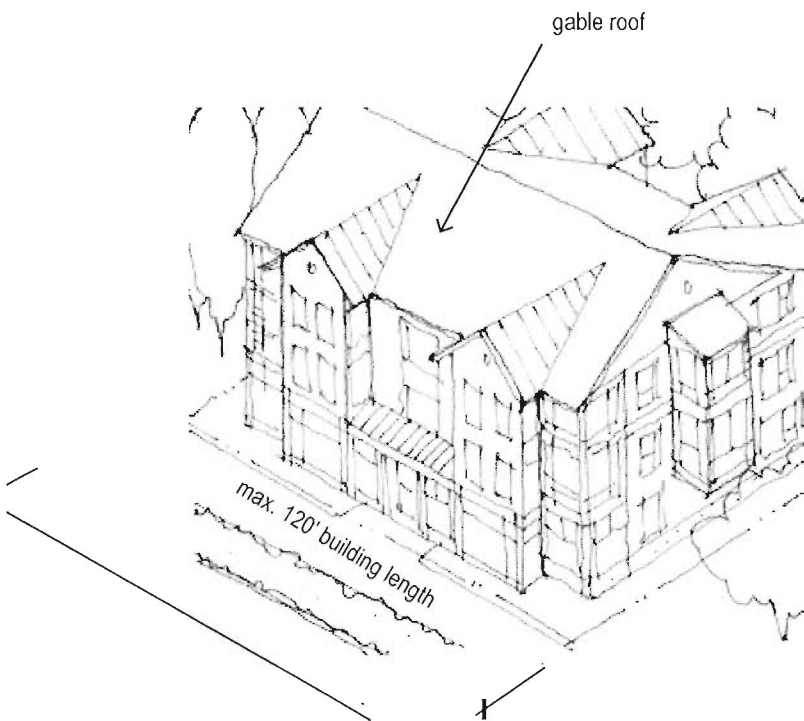


Fig. B.1.d.1 & 2 massing requirements

2. Facade Composition

a. **Intent:** To express the functional use of residential development in the design of exterior building elevations.

b. **Applicable Downtown Design Principles:**

- B. Promote Excellence in Design.
- F. Context Sensitivity.
- H. Create Appropriate Transitions in Height, Bulk and Scale

c. **Guidelines:**

Residential

1. Vary building facades to avoid monotony and add interest to the street. Consider upper floor balconies, bays and windows that overlook the street, enliven the elevation, and communicate the residential function of the building.

d. **Standard:**

Residential

1. Building facades shall incorporate design features such as off-sets, balconies, projections, recesses or similar elements to avoid large expanses of uninterrupted building surfaces. Such features shall occur at a minimum of every 30 feet.
 - a. Recesses and projections shall have a minimum depth of 2 feet and minimum length of 4 feet.

APPLIES TO ALL DEVELOPMENT IN DISTRICT:

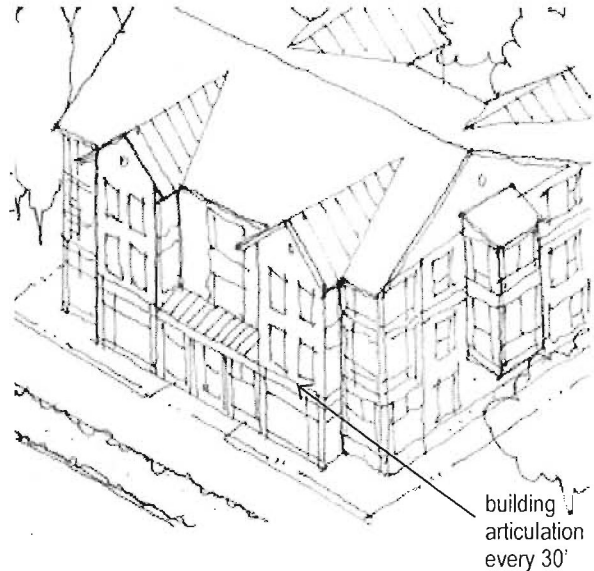
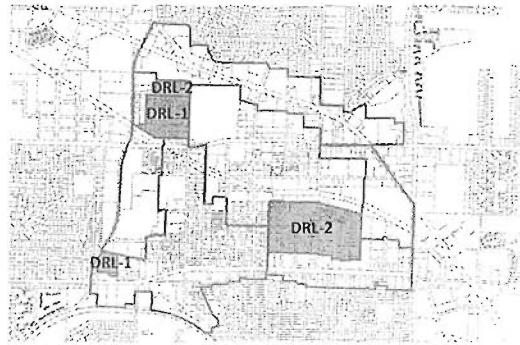


Fig. B.2.d.1 facade articulation requirement

DEFINITIONS

- Awning.** A shelter that provides weather protection, usually constructed of non-rigid canvas or canvas-like materials on a supporting framework that projects from the exterior wall of a building.
- Bay (of building façade).** Any division of a building between vertical lines or planes, especially the entire space between two adjacent supports.
- Bulkhead.** The solid portion of wall below the glass of a storefront window whose purpose is to protect against abrasion or impact loads. The bulkhead is also commonly referred to as a knee-wall.
- Canopy.** An architectural projection that provides weather protection, identity or decoration and is supported by the building to which it is attached. A canopy is comprised of a rigid structure over which a rigid covering is attached. See also sun screen/sun shade.
- Cornice.** The uppermost section of projecting ornamental moldings along the top of a building just below a roof or the top of a wall.
- Design Guidelines.** A set of design parameters for development in design districts that are based on the established Design Principles. The Design Guidelines are discretionary in nature, provide a statement of intent, and are used to evaluate the acceptability of a project's design. Design Guidelines provide the opportunity for creative design flexibility.
- Design Principles.** General statements that will guide the design of the built environment in design districts. They are the connection between general planning goals and policies and implementing Design Guidelines and Standards. Design Principles are normally described by several sentences and usually amplified with graphics such as diagrams, sketches, illustrations, and photographs.
- Design Standards.** A set of objective requirements for development in design districts that are based on design principles. Design standards provide a clear and objective way of evaluating the acceptability of a project's design.
- Eco-Roof.** Also known as Green Roof. A roof that has been constructed with an impermeable barrier, overlain with a layer of planting media (such as soil or other) and vegetation, with the purpose of slowing and filtering stormwater, insulating the building, and reducing the urban heat-island effect.
- Fore-Court.** An open area in front of a building's main entrance.
- Floor Area.** The gross area, under roof, of all of the habitable floors of a building, measured from the interior of exterior walls, excluding only space devoted to off-street parking or loading and excluding basement area.
- Green Street.** A street that incorporates Green Development Practices within or adjacent to the right-of-way to treat, retain, and infiltrate stormwater runoff. Green Street section and design standards are included in the City of Gresham's Public Works Standards for each street classification. Green Streets typically use rain gardens, stormwater planters, or porous pavement to manage stormwater runoff.
- Ground Floor Height.** The vertical distance from the upper surface of the ground floor to the upper surface of the floor immediately above. (See also Story):
- Light Cut-off.** An outdoor lighting fixture designed to direct light (usually downward) and prevent light from being emitted outside the area intended for lighting.
- Lintel.** A horizontal structural beam above an opening, such as a window or door that may be expressed

externally as an architectural feature.

Live-work. A live-work unit is a structure that combines a limited office, retail service, and/or business service use with a residential living space. The commercial space may be used by anyone residing at the unit and by no more than two non-resident employees.

Massing, Building. The mass or bulkiness of a building is its three-dimensional form, and its relationship to exterior spaces.

Modulation, Building. Design technique that breaks the massing of large buildings down into smaller units by providing varying depths for exterior walls.

- Vertical modulation. Used to make large buildings appear to be an aggregate of smaller elements or to add visual relief to long stretches of facades. Techniques can include the use of architectural features, setbacks or varying rooflines.
- Horizontal Modulation. Used to reduce the apparent mass of multi-story buildings and provide continuity at the ground level of a large building. Building facades can be divided with horizontal elements so that the façade appears less massive than those with sheer, flat surfaces. Techniques can include step-backs, balconies, and roof treatment.

Mural. A painting applied to a wall surface that will be reviewed and approved for artistic merit by an arts reviewing body designated by the City.

Outdoor Sales Display. An outdoor arrangement of objects, items, products, or other materials that is capable of rearrangement, is not in a fixed position and which is designed and used for the purpose of sales. An outdoor sales display does not exceed 15% of the ground floor area of the building(s) on the site or a maximum of 1,000 square feet, whichever is less. An outdoor sales display also does not remain outdoors for more than 24 hours and maintains an accessible pathway as defined in the Building Code.

Outdoor Storage. The keeping of materials or goods associated with the rental, distribution, wholesale, manufacturing, processing or repair of equipment or supplies in the same outdoor place for more than 24 hours.

Plinth. The base or platform upon which a wall, column, pedestal, statue, monument, or structure rests.

Pervious Paving. Also known as porous pavement or permeable paving. A paving method for paved areas such as roads, parking lots and walkways that allows the movement of water and air through the paving material. Pervious materials allow precipitation to percolate through areas that would traditionally be impervious and instead infiltrates the stormwater through to the soil below. Examples include: porous asphalt, concrete, paving stones, bricks, or paver systems that lock together but include small gaps to allow water to pass between the pavers.

Portico. A covered walkway or major porch with columns on at least one side; a covered colonnade.

Rain Garden. A shallow landscaped basin that accepts stormwater runoff from rooftops, parking lots, streets, or other impervious surfaces, with the purpose of storing and filtering or infiltrating the water into the ground.

Roof. A structural covering over any portion of a building or structure including projections beyond the walls or supports of the building or structure.

- Roof, Butterfly. A roof where planes are designed so the middle of the roof is lower and the outer edges are higher.
- Roof, Gable. A roof identified by the straight slope falling from ridge to eave, creating a peak or triangle on the side or front façade. Gabled structures have rakes on the gable facades and eaves on the non-gabled facades.
- Roof, Hipped. A roof that slopes on all sides up to a peak or ridge.
- Roof, Shed: A roof having only one sloping plane.

Scoring. A technique used to divide a sidewalk area by patterning grooves in the concrete for aesthetics or to control cracking.

Stormwater Planter. A rain garden with structural side walls constructed of concrete, steel, or other durable building material.

Sun Screen/Sun Shade. Attached projecting, architectural feature designed to provide shading from the sun. A sun screen/sun shade is a rigid structure and can add a decorative element to building design, but provides a functional energy conservation benefit to the building by deflecting solar heating away from building windows or walls. See also canopy.

Transom Window. A window above a door that is usually hinged to a horizontal crosspiece over the door.

Visible Transmittance. The amount of light that passes through a glazing material.

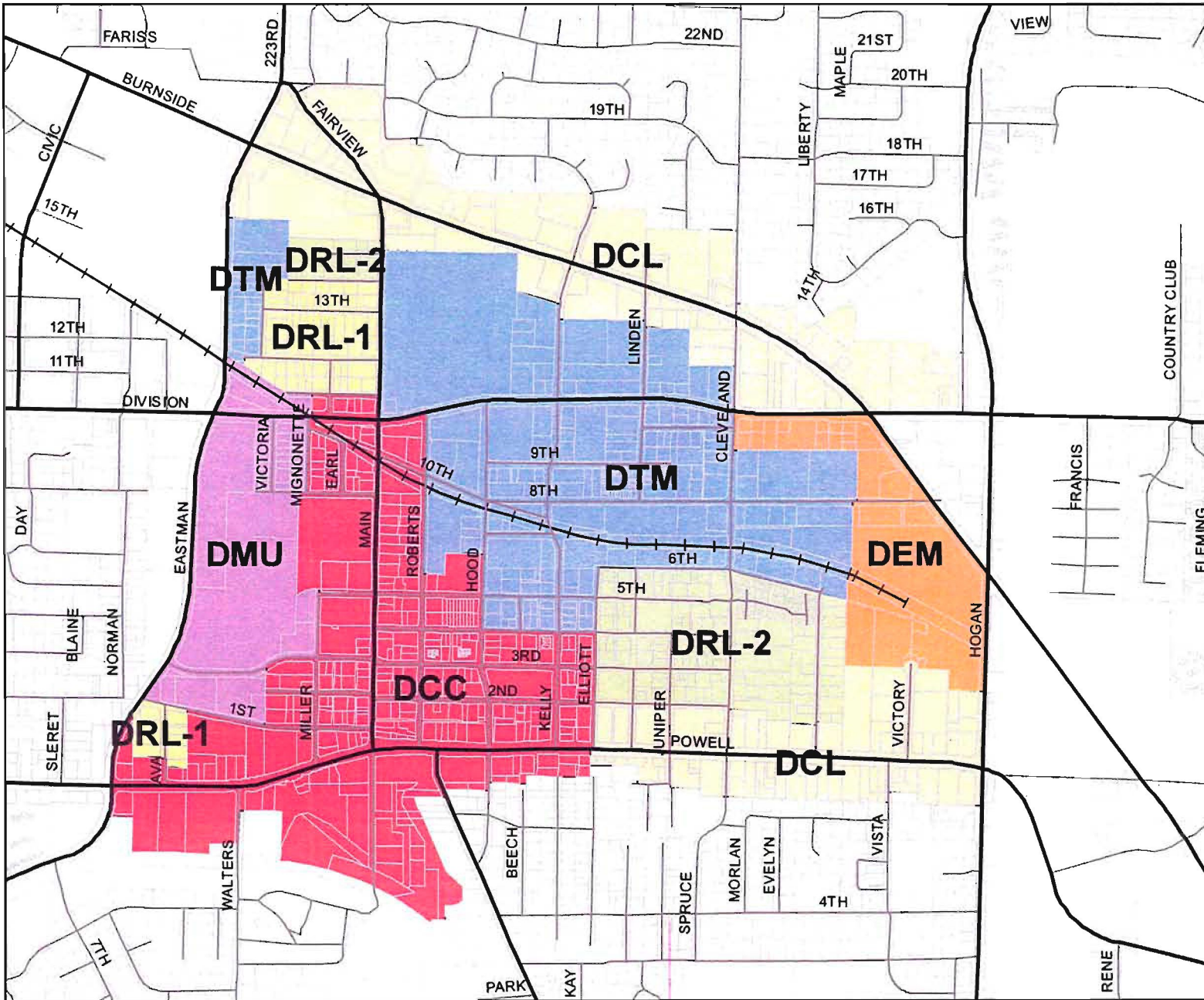
Xeriscaping. A landscaping method that utilizes water-conserving techniques, such as the use of drought-tolerant plants and efficient irrigation.

Proposed Downtown Plan Sub-Districts - Exhibit B - Legend

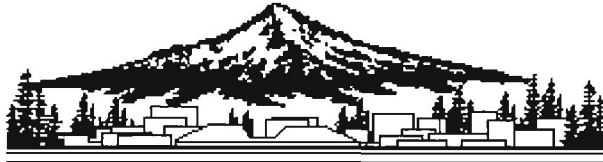
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Proposed Sub-Districts

- Downtown Commercial Core
- Downtown Commercial Low-rise
- Downtown Employment Mid-rise
- Downtown Mixed Use
- Downtown Residential Low-rise - 1
- Downtown Residential Low-rise - 2
- Downtown Transit Mid-rise



This map is based on digital databases from the City of Gresham. The City cannot accept any responsibility for errors, omissions, or positional accuracy. There are no warranties, expressed or implied.



*Urban Design & Planning Services
City of Gresham*

CERTIFICATION OF MAILING

FILE NO.: CPA 09-017

PROJECT: City of Gresham-Downtown
Design Manual

I, TAMMY J. RICHARDSON, CERTIFY THAT I HAVE MAILED THE
ATTACHED NOTICE OF ADOPTION TO THE FOLLOWING PARTIES:

DLCD
Plan Amendment Specialist
635 Capitol Street, NE #150
Salem, OR 97301-2540

Metro
Growth Management
600 NE Grand
Portland OR 97232-2736

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PO Box 20308
Keizer, OR 97307

Denny Deveny
Steve Thoren Properties LLC
1335 SW 25th Ct.
Gresham, OR 97080

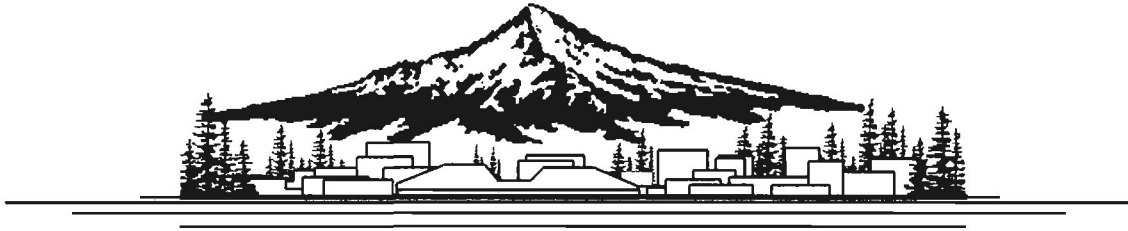
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Dwight Unti
GDDA
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Gresham, OR 97030

Megan Braunsten
323 NE Roberts
Gresham, OR 97219

SIGNATURE: Tammy J. Richardson

DATE OF MAILING: June 23, 2009



CITY OF GRESHAM
Urban Design & Planning Office
1333 NW Eastman Parkway
Gresham, Oregon 97030

NOTICE OF FINAL DECISION

June 23, 2009

On June 16, 2009, the Gresham City Council Approved the application of **City of Gresham (Council Order No. 615 and Ordinance No. 1675)** regarding amendments to the Gresham Community Development Plan relating to the **Gresham Downtown Plan Design Manual**.

The record for this project is maintained at Gresham City Hall, City of Gresham **File No. CPA 09-017**, and may be reviewed at the City's Urban Design & Planning office Monday through Friday, 8:00 AM to 5:00 PM.

An appeal of this decision may be filed with the Land Use Board of Appeals (LUBA) within 21 days of this Notice of Decision. LUBA has the jurisdiction to review all governmental land use decisions. An appeal of a land use decision must conform to the procedures and requirements of LUBA. They may be contacted in Salem at:

LUBA
550 Capitol Street, NE – Suite #235
Salem, Oregon 97301-2552
(503) 373-1265

DLCD
Attn: Plan Amendment Specialist
635 Capitol Street, NE #150
Salem, OR 97301-2540



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