



Oregon

Theodore R. Kulongoski, Governor

Department of Land Conservation and Development

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Salem, OR 97301-2540

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www.lcd.state.or.us



## NOTICE OF ADOPTED AMENDMENT

11/30/2010

TO: Subscribers to Notice of Adopted Plan  
or Land Use Regulation Amendments

FROM: Plan Amendment Program Specialist

SUBJECT: City of Gresham Plan Amendment  
DLCD File Number 006-10

The Department of Land Conservation and Development (DLCD) received the attached notice of adoption. Due to the size of amended material submitted, a complete copy has not been attached. A Copy of the adopted plan amendment is available for review at the DLCD office in Salem and the local government office.

Appeal Procedures\*

DLCD ACKNOWLEDGMENT or DEADLINE TO APPEAL: Monday, December 13, 2010

This amendment was submitted to DLCD for review prior to adoption pursuant to ORS 197.830(2)(b) only persons who participated in the local government proceedings leading to adoption of the amendment are eligible to appeal this decision to the Land Use Board of Appeals (LUBA).

If you wish to appeal, you must file a notice of intent to appeal with the Land Use Board of Appeals (LUBA) no later than 21 days from the date the decision was mailed to you by the local government. If you have questions, check with the local government to determine the appeal deadline. Copies of the notice of intent to appeal must be served upon the local government and others who received written notice of the final decision from the local government. The notice of intent to appeal must be served and filed in the form and manner prescribed by LUBA, (OAR Chapter 661, Division 10). Please call LUBA at 503-373-1265, if you have questions about appeal procedures.

\*NOTE: The Acknowledgment or Appeal Deadline is based upon the date the decision was mailed by local government. A decision may have been mailed to you on a different date than it was mailed to DLCD. As a result, your appeal deadline may be earlier than the above date specified. NO LUBA Notification to the jurisdiction of an appeal by the deadline, this Plan Amendment is acknowledged.

Cc: Dan McAuliffe, City of Gresham  
Gloria Gardiner, DLCD Urban Planning Specialist  
Jennifer Donnelly, DLCD Regional Representative  
Thomas Hogue, DLCD Regional Representative  
Bill Holmstrom, DLCD Regional Representative

<paa> YA



FORM

2

DLCD

# Notice of Adoption

In person  electronic  mailed

DATE  
STAMP

DEPT OF

NOV 23 2010

LAND CONSERVATION  
AND DEVELOPMENT

For Office Use Only

This Form 2 must be mailed to DLCD within **5-Working Days after the Final Ordinance is signed** by the public Official Designated by the jurisdiction and all other requirements of ORS 197.615 and OAR 660-018-000

Jurisdiction: **City of Gresham**

Local file number: **CPA 10-026**

Date of Adoption: **11-16-2010**

Date Mailed: **11-22-2010**

Was a Notice of Proposed Amendment (Form 1) mailed to DLCD?  Yes  No Date: 9-23-10

Comprehensive Plan Text Amendment

Comprehensive Plan Map Amendment

Land Use Regulation Amendment

Zoning Map Amendment

New Land Use Regulation

Other:

Summarize the adopted amendment. Do not use technical terms. Do not write "See Attached".

Creates Design Principles, Guidelines and Standards to promote quality site and building commercial design, and allows a two-track review process with clear and objective, and discretionary, regulations in the Corridor Design District.

Does the Adoption differ from proposal? Please select one

Updated/added Policies in Volume 2; limits to building sizes and development intensity in MC, CC & CMU districts; incorporated tiered development standards for commercial buildings up to 30,000 sf and over 30,000 sf w/o specific term "Tier" in Code; landscape standards to apply to all development regardless of size; window transparency requirements modified; bldg frontage requirements based on site frontage vs. "Tier".

Plan Map Changed from: CC/MC/GC

to: MC or CMU/NC/MC or NC

Zone Map Changed from:

to:

Location: Various

Acres Involved: 97

Specify Density: Previous:

New:

Applicable statewide planning goals:

1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19

Was an Exception Adopted?  YES  NO

Did DLCD receive a Notice of Proposed Amendment?

45-days prior to first evidentiary hearing?

Yes  No

If no, do the statewide planning goals apply?

Yes  No

If no, did Emergency Circumstances require immediate adoption?

Yes  No

DLCD file No. 006-10 (18538) [16428]

Please list all affected State or Federal Agencies, Local Governments or Special Districts:

Metro

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Local Contact: **Lauren McGuire**

Phone: (503) 618-2108 Extension:

Address: 1333 NW Eastman Parkway

Fax Number: - -

City: **Gresham**

Zip: **97030**

E-mail Address:

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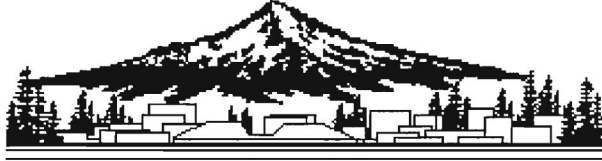
## **ADOPTION SUBMITTAL REQUIREMENTS**

**This Form 2 must be received by DLCD no later than 5 days after the ordinance has been signed by the public official designated by the jurisdiction to sign the approved ordinance(s)**  
per ORS 197.615 and OAR Chapter 660, Division 18

1. This Form 2 must be submitted by local jurisdictions only (not by applicant).
2. When submitting, please print this **Form 2** on light green paper if available.
3. Send this Form 2 and One (1) Complete Paper Copy and One (1) Electronic Digital CD (documents and maps) of the Adopted Amendment to the address in number 6:
4. **Electronic Submittals: Form 2 – Notice of Adoption will not be accepted via email or any electronic or digital format at this time.**
5. The Adopted Materials must include the final decision signed by the official designated by the jurisdiction. The Final Decision must include approved signed ordinance(s), finding(s), exhibit(s), and any map(s).
6. **DLCD Notice of Adoption must be submitted in One (1) Complete Paper Copy and One (1) Electronic Digital CD via United States Postal Service, Common Carrier or Hand Carried to the DLCD Salem Office and stamped with the incoming date stamp.** (for submittal instructions, also see # 5)] **MAIL the PAPER COPY and CD of the Adopted Amendment to:**

**ATTENTION: PLAN AMENDMENT SPECIALIST  
DEPARTMENT OF LAND CONSERVATION AND DEVELOPMENT  
635 CAPITOL STREET NE, SUITE 150  
SALEM, OREGON 97301-2540**

7. Submittal of this Notice of Adoption must include the signed ordinance(s), finding(s), exhibit(s) and any other supplementary information (see ORS 197.615 ).
8. Deadline to appeals to LUBA is calculated **twenty-one (21) days** from the receipt (postmark date) of adoption (see ORS 197.830 to 197.845 ).
9. In addition to sending the Form 2 - Notice of Adoption to DLCD, please notify persons who participated in the local hearing and requested notice of the final decision at the same time the adoption packet is mailed to DLCD (see ORS 197.615 ).
10. **Need More Copies?** You can now access these forms online at <http://www.lcd.state.or.us/>. You may also call the DLCD Office at (503) 373-0050; or Fax your request to: (503) 378-5518.



*Urban Design & Planning Services  
City of Gresham*

## CERTIFICATION OF MAILING

FILE NO.: CPA 10-026

PROJECT: City of Gresham-Retail Design  
and Development Standards

I, TAMMY J. RICHARDSON, CERTIFY THAT I HAVE MAILED THE  
ATTACHED NOTICE OF ADOPTION TO THE FOLLOWING PARTIES:

**DLCD**

**Plan Amendment Specialist  
635 Capitol Street, NE #150  
Salem, OR 97301-2540**

**Metro  
Growth Management  
600 NE Grand  
Portland OR 97232-2736**

**Carol Rulla  
5162 SE 28<sup>th</sup> Drive  
Gresham, OR 97080**

**Suzan Wells  
2861 SW Lillyben Place  
Gresham, OR 97080**

**Christina Hur  
2102 SW Butler Rd.  
Gresham, OR 97080**

**Charles Colling  
3130 NE 13<sup>th</sup>  
Gresham, OR 97030**

**Sally Macklin  
2935 SW 14<sup>th</sup> Drive  
Gresham, OR 97080**

**Mark Peterson  
PO Box 55453  
Portland, OR 97238**

SIGNATURE: Tammy J. Richardson

DATE OF MAILING: November 22, 2010

BEFORE THE CITY COUNCIL OF THE

CITY OF GRESHAM

IN THE MATTER OF AMENDMENTS TO VOLUME 2, ) Order No. 627  
POLICIES, AND VOLUME 3, DEVELOPMENT CODE, )  
OF THE GRESHAM COMMUNITY DEVELOPMENT ) CPA 10-026  
PLAN, REGARDING RETAIL DESIGN STANDARDS )

On November 16, 2010, the City Council held a public hearing to take testimony on amendments to Volume 2, Policies, and Volume 3, Development Code, of the Gresham Community Development Plan, regarding Retail Design Standards.

The hearing was conducted under Type IV procedures. Mayor Shane T. Bemis presided at the hearing.

The Council closed the public hearing and approved the proposed amendments, and a decision was made at the November 16, 2010 meeting.

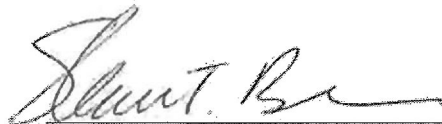
A permanent record of this proceeding is to be kept on file in the Gresham City Hall, along with the original of the Order.

The Council orders that these amendments are approved, and adopts the findings, conclusions, and recommendations as stated in the attached Planning Commission Recommendation Order and staff reports.

Dated: November 16, 2010



\_\_\_\_\_  
City Manager



\_\_\_\_\_  
Mayor



## MEMORANDUM

### URBAN DESIGN & PLANNING

#### ADDENDUM STAFF REPORT TYPE IV HEARING—COMPREHENSIVE PLAN AMENDMENT RETAIL DESIGN AND DEVELOPMENT STANDARDS PROJECT

To: Gresham Planning Commission

From: Mike Abbaté, Urban Design & Planning Director  
Jonathan Harker, Long Range Planning Manager  
Dan McAuliffe, Associate Comprehensive Planner  
Lauren McGuire, Senior Comprehensive Planner

Hearing Date: November 8, 2010

Report Date: November 4, 2010

File: CPA 10-026

Since the Planning Commission report was issued on October 28 it has come to staff's attention that the current minimum value for visual light transmittance (VT) in the Downtown and Multi-Family design regulations and proposed Corridor Commercial design regulations has conflicts with the new Oregon Energy Code. This was discovered as a result of the Downtown Storefront Improvement Program. The design regulations require a Visible Transmittance Value of 90% for windows and clear glass. The Oregon Energy Code requires windows to have a solar heat gain coefficient (SHGC) no greater than 0.4 and this is typically achieved through coatings applied to the glass. Research indicates that it may be difficult, if not impossible, to find a glass that meets both requirements.

#### **Findings:**

The conflict arises out of two separate intentions:

1. **Visibility:** This requirement is intended to provide clear visible connections between the interior and exterior spaces. This activates the street by allowing views into the space and allows passive crime prevention surveillance of exterior spaces from the interior.
2. **Energy Conservation:** This Building Code requirement is intended to minimize solar gain and associated energy usage, typically attributed to cooling during summer months. Reaching the Energy Code SHGC through windows can be achieved by applying coatings and moderate tints to the window.

Research to date indicates that there are no storefront glass products available which meet both the 90% VT and the 0.4 SHGC requirements. There are products, however, which have a 60% VT and

still meet the 0.4 SHGC. After looking at window samples with various SHGC and VT values, staff believes 60% visual light transmittance allows windows to meet the energy code while maintaining sufficient visibility into and out of the space.

This is not only an issue for the proposed design regulations for commercial development in to corridors but also in the Downtown and Multi-Family design regulations.

**Recommendation:**

In response to these conflicting provisions, staff recommends the following changes to Sections 7.0603(B)(4)(D)(9) [Corridors], 4.1151(B)(5)(D)(4 &6) [Downtown] and 7.0103(B)(2)(d)(14) [Multi-Family]; based on the findings and recommendations of this Addendum Staff Report.

(New language is double underlined, proposed deleted language is ~~stricken~~.)

7.0603(B)(4)(D)(9). To meet the clear glass requirement, windows shall have a visible transmittance value (VT) no less than ~~ninety percent (90%)~~ sixty percent (60%). Where clear glass is required, the use of reflective, tinted or spandrel glass shall not be permitted.

4.1151(B)(5)(D)(4). To meet the clear, transparent glass requirement, storefront windows shall have a Visible Transmittance (VT) value of ~~90%~~ sixty percent (60%) or greater.

4.1151(B)(5)(D)(6). Films or other substances applied to the window to reduce the Visible Transmittance value below ~~90%~~ sixty percent (60%) shall be prohibited.

7.0103(B)(2)(d)(14). To meet the transparent glass requirement, storefront windows shall have a Visible Transmittance (VT) value of ~~ninety percent (90%)~~ sixty percent (60%) or greater.



## MEMORANDUM

### URBAN DESIGN & PLANNING

#### STAFF REPORT TYPE IV HEARING—COMPREHENSIVE PLAN AMENDMENT RETAIL DESIGN AND DEVELOPMENT STANDARDS PROJECT

To: Gresham Planning Commission

From: Mike Abbaté, Urban Design & Planning Director  
Jonathan Harker, AICP, Long Range Planning Manager  
Dan McAuliffe, Associate Comprehensive Planner  
Lauren McGuire, Senior Comprehensive Planner

Hearing Date: November 8, 2010

Report Date: October 28, 2010

File: CPA 10-026

Proposal: To adopt Comprehensive Plan amendments to Volume 2 and Volume 3 of the Community Development Plan with:

- 1) Text amendments updating commercial land use policies and adding new policies for commercial design in the Corridor Design District. (Volume 2)
- 2) Map amendments replacing four commercial nodes from Community Commercial (CC) to Moderate Commercial (MC), one node from CC to Corridor Mixed Use (CMU), one node from General Commercial (GC) to MC and two nodes from GC to Neighborhood Commercial (NC). (Volume 2, Appendix C)
- 3) Text amendments modifying the CC, MC and CMU land use districts and deleting the GC land use district; adding new design regulations for commercial development in the Corridor Design District; and miscellaneous modifications regarding definitions, clarifications and cross-references. (Volume 3)

Exhibits: 'A' – Text Amendments, Proposed Council Bill 13-10, and Attachment 'A' to Council Bill 13-10 Community Development Plan  
'B' – Map Amendments, Proposed Council Bill 13-10, Community Development Plan Map

Recommendation: Staff recommends **adoption** of the proposed Comprehensive Plan amendments.

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## SECTION I EXECUTIVE SUMMARY

### Background

The Retail Design and Development Standards (RDDS) project is on the 2010 Council Work Plan. The purpose of the project is to ensure commercial development in the Corridor Districts meets the Community's vision and the City's goals.

This project is ensuring that:

- Commercial development in the corridors is developed in appropriate locations and at appropriate scales in order to minimize impacts on adjacent low-density residential property and encourage continued growth of the Regional Town and Station Centers.
- Gresham's Community Development Plan contains urban design provisions that promote the livability of commercial developments.
- Design Principles and Guidelines are created, and Design Standards are updated that:
  - Identify elements that constitute good site and building design;
  - Foster commercial developments which are integrated with surrounding neighborhoods, improving the sense of community and livability in Gresham;
  - Incorporate Crime Prevention through Environmental Design (CPTED) Principles where feasible;
  - Support the City's sustainability goals; and
  - Allow a two-track review process with a choice of a discretionary Design Guidelines process or clear and objective Design Standards process by the Design Commission or Manager.

On October 13, 2009, Council discussed a staff report regarding issues associated with large format retail (big box retail) and how other jurisdictions address these issues. At the meeting Council provided policy direction to pursue a Comprehensive Plan Amendment to both address issues associated with large format retail and establish the two-track design review process for commercial development in the Corridor Design District.

The project began in January 2010. The planning process included conducting research and analysis of issues; developing alternative approaches to those issues; and selecting the approach. This process was done with public participation that included five Planning Commission work sessions; seven Design Commission work sessions; five Stakeholder Group meetings; three Community Forums and three Council policy meetings. The issues included:

### Vision

There are no up-to date policies which specifically address the role of commercial development in the Corridor Districts. The current policies are over 20 years old and do not fully address development trends which have occurred in recent years. Additionally, they provide no direction for scale and intensity of development in the Corridors as compared to development in the Rockwood Town Center and in Gresham's Regional Center (Downtown and Civic Neighborhood).

The approach to this issue was to develop a vision for commercial development in the Corridor Districts. One aspect of this vision was that retail development in the Corridor Districts should be of less intensity, size and height than development in Rockwood, Downtown and Civic Neighborhood. Rockwood is a 2040 Growth Concept Town Center and the Downtown and Civic Neighborhood Plan Districts combined are a 2040 Growth Concept Regional Center. Additionally, the vision continues to see retail focused on nodes of key transit street intersections and intended to serve surrounding local neighborhoods.

### Location

Within Gresham Corridors there are 23 commercial nodes [consisting of Community Commercial (CC), Moderate Commercial (MC) and General Commercial (GC)] which could be developed with large format retail development. An issue for the commercial nodes was if their existing land use designations were appropriate for the location and if they supported the commercial vision.

The approach to this issue was to analyze each of the commercial nodes using primary factors: the quantity of low-density residential districts within a quarter mile of the node, the presence of transit service, traffic capacity of the Corridors, development potential and Habitat Conservation Areas. It was found that some land use designations were not consistent with the vision which resulted in proposed plan map amendments.

### Commercial Building Size Limits

This issue focuses on the question of size limitation for commercial development: should a limit be set and, if so, what should be the limit for commercial development in the CC, GC or MC districts. Neither the CC nor the GC district currently has a building size or footprint size limit. The height limit for CC is 80 feet and for GC 65 feet. The MC district has a 40,000 square foot footprint limit but does not have a building size limit. It has a 45 foot height limit. The CC and GC Districts allow building at scales as great as or greater than those allowed in Rockwood, Civic Neighborhood and Downtown. The nodes that the commercial Corridor Districts are located in are often adjacent to residentially designated lands.

The approach to this issue was to focus on the CC district because proposed land use changes resulted in the elimination of the GC district and the MC is limited to smaller scale buildings. An analysis assessed the impacts different size buildings could have on walkability and visual impact; potential scale of one-story development at existing sites; retail types; and non-conforming developments. It was found that buildings larger than 100,000 square feet have the potential to disconnect neighborhoods and have design issues associated with them such as larger walls that are difficult to design at a pedestrian scale or to design with visual interest and lack of repetitiveness. It was found that a 100,000 square foot size limitation would allow flexibility for potential retail types; also, a 100,000 square foot size limit was not a significant issue for non-conforming developments and for potential development at existing sites.

### Design Regulations

This issue is that the Corridor Districts included in this project are part of the Corridor Design District. Design Districts have been established for most areas of Gresham where commercial, multi-family and civic development is allowed. A Design District is intended to enable a two-track design review system in which a developer can choose to use a clear and objective Design Standards track or a discretionary Guidelines track when applying for a development permit. All discretionary applications and many clear and objective applications are considered for approval by the Design Commission. In order for the two-track design review system to be utilized Design Principles, Guidelines and clear and objective Standards must be established for the Design District. Previously this has been done for the Downtown Plan District and for multi-family development in Design Districts; a similar project is underway for the Rockwood Design District.

The approach to this issue in part was that the retail and commercial development in the Corridor Design District could build off these previously adopted Downtown and Multi-Family design regulations but with differences to address the types of development anticipated in the Corridor Districts.

The project compared retail development in Gresham to local and national models and found the existing standards in place do not achieve the desired levels of design or connectivity exhibited in the models. The existing commercial development standards provide little direction to the design of commercial development in the Corridors. While the existing Standards do address some concerns successfully, it was found that new design Guidelines and Standards are needed to address connectivity; circulation; building placement and orientation; landscaping; lighting; service area configuration; architectural building design; transparency; materials and sustainability.

### Public Involvement

The project has included an extensive research and outreach process: three community forums with more than 80 total participants attending the events were part of this outreach process. At the first forum, participants evaluated four commercial developments, which provided staff with guidance on public design preferences. For example, many participants stated a desire to have walkable, human-scale commercial

development which was integrated into surrounding neighborhoods rather than a large store served by a larger parking lot. Several participants felt current standards permit development which is out of scale with the surrounding context, allows development which threatens the vitality of existing businesses and generates excessive quantities of traffic.

These preferences and concerns were integrated into the design regulations or influenced policy recommendations. The second community forum was policy-focused with participants providing comments on recommended plan map changes and building scale regulations.

In addition to community forums, several public meetings were held with the Planning Commission and the Design Commission to discuss alternatives and receive feedback on the preferred approach. Three work sessions were held with the City Council.

### **Proposed Comprehensive Plan Amendment Overview**

Text changes to Volumes 2 (Community Development Plan Goals and Policies) and 3 (Development Code) are proposed. The format of the attached Exhibit "A" is ~~strikeout~~ underline with comments inserted into the documents to help explain the relevance for each proposed change. The overview below provides a summary of some of the key proposed amendments and additional rationale for the changes. The new Section 7.0600 Corridor Design District Commercial Criteria, Guidelines and Standards is included as Exhibit 'B.'

#### **Volume 2 (Goals, Policies and Action Measure) – Exhibit 'A'**

Section 10.312, Commercial Land Use Policies, is proposed to have amended implementation strategies for Policy 1 and 2. These changes incorporate the vision to have greater intensity development take place in the Regional, Town and Station Centers (Downtown, Civic Neighborhood and Rockwood) and lower intensity commercial development in the Corridors.

A new Section 10.413.3 Design Standards for Commercial Development in the Corridor Design District is proposed and includes the Goal of commercial development that is human-scaled, attractive, safe, active and of excellent design that utilizes high-quality and sustainable materials. Innovation and creativity in design is encouraged. The new Goal supplements and supports Goal 10.413 – Community Design whose purpose is to encourage efficient and effective development designs to maintain and improve the qualities of the relationships among buildings and surrounding neighborhoods and to ensure that individual developments contribute to a quality environment.

The proposed new Goal 10.413.3 promotes accessible and attractive commercial development to benefit the physical environment and aesthetics of Gresham. It establishes a series of Policies that will further describe how the Goal can be achieved. It also establishes Design Principles which have been incorporated into the Volume 3 Code Section 7.0602. Design Principles provide a connection between general planning Goals and Policies and implementing regulations. The main purpose of Design Principles is to convey a sense of preferred quality for a place and to formulate design direction on such topics as site design, open spaces, landscaping, street orientation, transportation mode provisions, architectural design excellence, sustainable elements and high quality materials. The Design Principles are the basis for both the discretionary Design Guidelines and the clear and objective Design Standards.

Also proposed are a set of Action Measures that are specific actions to implement the Goals and Policies that can be taken as time and resources allow. One Action Measure is to implement a process which provides two alternative review tracks: one that is clear and objective and one that is discretionary for commercial developments. This Measure is being done with the Volume 3 Code amendments. The second Action Measure creating an illustrated design guidebook will be done at a later date.

#### **Volume 2 (Community Development Plan Map) – Exhibit 'B'**

Plan map amendments for eight nodes are proposed. They are:

- Nodes at 182nd and Powel, 182nd and Division, 162nd and Halsey and Division and Kane are proposed to change from Community Commercial to Moderate Commercial

- Nodes at 202nd and Glisan and Palmquist and Hogan are proposed to change from General Commercial to Neighborhood Commercial
- The node at Birdsdale and Division is proposed to change from Community Commercial to Corridor Mixed-Use.
- The node at Burnside and Palmquist is proposed to change from General Commercial to Moderate Commercial.

A result of these plan map amendments is that no properties will be designated as General Commercial (GC) and therefore the GC District will be deleted.

### **Volume 3 (Community Development Code) – Exhibit ‘A’**

#### Article 3 Definitions

A number of new or revised definitions are proposed to address new commercial regulations.

#### Section 4.0200

The Commercial Districts section is modified to reflect the deletion of the General Commercial (GC) district.

#### Section 4.0413-4.0415

Descriptions of the Community CC, MC and CMU districts have been changed to incorporate language from the commercial vision.

#### Section 4.0420:

The permitted uses table has been modified to incorporate building footprint and commercial space limitations for the CC, MC and CMU districts:

#### Community Commercial:

- 100,000 square foot maximum building footprint limitation
- 100,000 square foot maximum total building size for commercial uses
- 45 foot height limitation (currently 80 feet)

#### Moderate Commercial:

- 40,000 square foot maximum building footprint limitation (existing)
- Grocery Store footprint limit of 60,000 square feet
- 80,000 square foot maximum total building size for commercial uses

Buildings could exceed the maximum total building size for commercial uses if residential units were developed above the commercial space; however, buildings could not exceed the footprint limits.

Corridor Mixed-Use is also proposed to be amended to address a potential problem which could allow unlimited commercial development in mixed-use buildings. Mixed-use buildings are proposed to follow dimensional standards contained within the existing Multi-Family Design Standards which limit building dimensions to 160-200 feet, depending on configuration.

#### Section 4.0430

Modifications to the development standards table include reduced front setbacks which are specific to the street type for CC and MC. Yard setbacks are also proposed to change to 10 feet on arterials and 5 feet on local streets (currently 20 feet); no changes are proposed in the CMU district.

#### Section 4.0434

The building height table has been modified to incorporate the proposed 45 foot height limit for CC.

#### Section 4.0435

The Transit Design Criteria and Standards section has been modified to exempt commercial development within the Corridor Design District.

#### Section 4.1101 and Section 7.0101

The Downtown and Multi-Family Code are proposed to be modified to incorporate minor text revisions to the "How to use the Code" section.

### **Volume 3 (Community Development Code) – Attachment A of Exhibit 'A'**

#### Section 7.0202

This section contains the previous commercial design standards. The section is no longer applicable to commercial developments within the Corridor Design District and thus references the applicable new Section 7.0600 for all Corridor Design District commercial developments. The section does remain in effect, however, for commercial developments in other areas of the City where design regulations do not currently exist.

#### Section 7.0600

This new section incorporates the Commercial Code Criteria and Design Guideline and Standard regulations. It is a series of regulations that are graphically illustrated with photographs and images to help guide the design of the built environment.

#### Section 7.0601

This new section establishes general criteria for commercial developments. It explains applicability, purpose, the design review process and how to use the Code. The section provides the applicant with the option to follow the discretionary process meeting the Design Guidelines and the Design Principles or the clear and objective process meeting the Design Standards. This is the same process currently in place for the Downtown Plan Design District and the Multi-Family Code.

#### Section 7.0602

This is a new section which describes a series of commercial development Design Principles that relate to Site Design and Building Design as well as sustainability. These Design Principles are the general, overarching statements and considerations that guide the design of commercial development. The Design Guidelines and Standards are written to support and carry out the Principles on a project-specific level.

The Design Principles are:

1. **Accessibility:** Large commercial developments shall be sited and designed to accommodate multiple modes of transportation including transit, bicycles and pedestrians as well as the automobile while facilitating comfortable pedestrian movement to and throughout the site and reducing automobile dependence and dominance.
2. **Activity:** Sites shall be developed in a manner that fosters activity throughout the day.
3. **Building and Site Orientation:** Buildings shall be oriented toward and placed in close proximity to the street, defining the public space while creating a consistent and appropriate street edge.
4. **Parking:** Surface parking shall be configured in a manner that minimizes its visual and environmental impacts on the site and on surrounding properties. Parking shall not dominate the site and the quantity of parking shall relate to actual usage and the needs of the patrons.
5. **Public Spaces:** Site plans shall incorporate outdoor public spaces that encourage activity, gathering and enjoyment. These spaces shall foster desirability, place identity and enhance the overall aesthetic of the site.
6. **Landscaping:** Landscaping shall be utilized to create attractive commercial environments by enhancing building and site appearance, defining pedestrian spaces, walkways and streets, breaking down the scale of parking areas, and screening service and loading areas.
7. **Sustainability:** Commercial development shall utilize sustainable development practices. Development shall address issues such as stormwater, energy and water usage in order to promote the efficient use of land and resources, minimize their environmental impacts and protect natural resources.
8. **Safe Design:** Site and building design shall integrate Crime Prevention through Environmental Design (CPTED) strategies as appropriate to enhance the safety, security and comfort of customers, employees and neighboring residents.

9. Impact Mitigation: Sites shall be designed to mitigate or minimize impacts on surrounding properties and public spaces.
10. Building Form and Articulation: Design strategies that break down the scale of large commercial buildings into smaller human-scale masses shall be utilized, creating visual interest and eliminating blank facades.
11. Building Activity and Glazing: Glazing shall be incorporated at pedestrian levels to allow views into active interior spaces and to provide a visual connection between the interior and exterior.
12. Prominence and Hierarchy: The building shall be designed in a manner that enriches and gives design prominence to critical locations.
13. High Quality Materials: Buildings shall utilize a combination of complimentary, high-quality materials that are attractive, durable and context appropriate.
14. Sustainable Architectural Design: Architectural design and practices shall be incorporated which conserve energy, resources and minimize life cycle costs. Large commercial buildings shall be designed to accommodate future tenants or uses in the event of vacancy.

### Section 7.0603

This new section contains new Design Guidelines and Standards which regulate new commercial developments. It incorporates many of the original commercial Code regulations found in Section 7.0202, Section 7.0210, Section 9.0823 and Section 9.0824 and builds upon those with new Site Design and Building Design related regulations. Because all relevant standards from these sections are incorporated into the proposed regulations, commercial development which follows the Commercial Design Standards are exempt from those sections. The Code requirements address site design, building design and sustainability. Several subsections exist within these requirements.

### **Staff Report Organization and Contents**

- Section I is an Executive Summary of the project and provides an overview of proposed Community Development Plan and Code changes anticipated as part of this project.
- Section II identifies applicable Development Code procedures that apply to the proposal.
- Section III identifies those current Community Development Plan Policies that apply to the proposal.
- Section IV identifies the applicable Metro Urban Growth Functional Plan (UGMFP) titles that apply to the proposal.
- Section V identifies the applicable Statewide Planning Goals applicable to this proposal.
- Section VI contains findings of fact that indicate how the proposal is consistent with Sections III through V:
  - Subsection A is findings of fact for the Community Development Code procedures.
  - Subsection B is findings of fact for the Community Development Plan Goals and Policies.
  - Subsection C is findings of fact for the Metro Functional Plan titles.
  - Subsection D is findings of fact for the Statewide Planning Goals.
- Sections VII and VIII summarize staff conclusions and recommendations.
- Exhibit 'A' includes proposed amendments to Volume 2 (Policies) and Volume 3 (Development Code) as well as commentary.
- Attachment 'A' to Exhibit 'A' includes the new Section 7.0600 Corridor Design District Commercial Design Guidelines and Standards.
- Exhibit 'B' are the proposed plan map amendments to Volume 2, Appendix C.

## **SECTION II APPLICABLE COMMUNITY DEVELOPMENT CODE PROCEDURES**

<b>Section 11.0201</b>	Initiation of an Application
<b>Section 11.0204</b>	Classification of Applications
<b>Section 11.0600</b>	Type IV Procedure – Legislative
<b>Section 11.1000</b>	Public Hearings

**SECTION III  
APPLICABLE COMMUNITY DEVELOPMENT PLAN GOALS & POLICIES**

<b>Section 10.014</b>	Land Use Policies and Regulations
<b>Section 10.100</b>	Citizen Involvement
<b>Section 10.312</b>	Commercial Land Use
<b>Section 10.319.1</b>	Transit Corridor Plan Area
<b>Section 10.413</b>	Community Design
<b>Section 10.600</b>	Statewide Planning Goal 10: Housing

**SECTION IV  
APPLICABLE METRO URBAN GROWTH FUNCTIONAL PLAN TITLES**

<b>Title 8</b>	Compliance Procedures
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**SECTION V  
APPLICABLE STATEWIDE PLANNING GOALS**

<b>Goal 1</b>	Citizen Involvement
<b>Goal 2</b>	Land Use Planning
<b>Goal 9</b>	Economic Development

**SECTION VI  
FINDINGS OF FACT**

The proposed Community Development Plan amendments attached as Exhibit 'A' and 'B' are consistent with all applicable procedures, Goals and Policies of the Community Development Plan and applicable titles of the Metro Urban Growth Management Functional Plan as indicated in the following findings.

**A. COMMUNITY DEVELOPMENT CODE PROCEDURES**

- 1. Section 11.0201 – Initiation.** This section provides that only the City Council may initiate a Type IV legislative application to amend the text or map of the Gresham Community Development Plan or Code. This project was initiated by the City Council when they adopted the 2010 Council Work Plan which included this Retail Design and Development Standards project.
- 2. Section 11.0200 – Classification of Applications.** This section provides that Type IV procedures are legislative and typically involve the adoption, implementation or amendment of policy by ordinance and that it generally applies to a relatively large geographic area containing many property owners. This project meets those conditions and is being processed under the Type IV procedures.
- 3. Section 11.0600 – Type IV Legislative Procedures.** For a Type IV Comprehensive Plan Amendment this section requires a submittal to the Department of Land Conservation and Development at least 45 days prior to the Planning Commission hearing. This submittal was made on September 23, 2010, which is at least 45 days prior to the Planning Commission hearing date of November 8, 2010. This section also requires a notice to property owners when amendments may affect the permissive uses of the property to be sent not more than 40 days nor less than 20 days prior to the Planning Commission hearing. A notice was sent to those affected properties on October 18, 2010, which is not more than 40 days nor less than 20 days from the November 8, 2010, Planning Commission hearing. This section also requires that hearings be scheduled, a notice published in a newspaper of general circulation in the City and a copy of the decision be mailed to those required to receive such notice. Required notice of public hearing for these proposed text amendments has been published in the Gresham Outlook as required by this section.

This section also requires that the Planning Commission shall hold a public hearing and make a recommendation to the Council for an amendment to the Community Development Code and the Community Development Plan. The Council shall hold another public hearing and make a final decision. Interested persons may present evidence and testimony relevant to the proposal. The Planning Commission will make a recommendation and the Council will make a decision that will be based on findings of fact contained in this report and in the hearings record, and a decision will be sent to those who participated in the hearings. A decision shall be made accompanied by findings and an order.

**3. Section 11.1000 - Public Hearings.** The section provides for a hearing process consistent with Section 11.1000. Both the Planning Commission and the City Council, at public hearings in conformance with provisions of this section, will consider this proposal.

## **B. COMMUNITY DEVELOPMENT PLAN GOALS AND POLICIES (VOLUME II)**

This section identifies the applicable Community Development Plan Goals and Policies. The text (*italicized*) of the Policy is followed by corresponding findings and conclusions. The applicable Policies are grouped by general categories.

### **1. General Goals & Policies**

#### Section 10.014 Land Use Policies and Regulations

*Goal: Maintain an up-to-date Comprehensive Plan and implement regulations as the legislative foundation of Gresham's land use program.*

*Policy 1: The City's land use program will be consistent with state and regional requirements but also shall serve the best interests of Gresham.*

*Policy 2: The City's land use regulations, actions and related plans shall be consistent with and implement the Comprehensive Plan.*

*Policy 3: Gresham's Community Development Plan Map shall implement the Comprehensive Plan by providing for a range of needed urban land uses including:*

- a. Residential;*
- b. Commercial and office uses including business parks;*
- c. Mixed-Use;*
- d. Industrial uses;*
- e. Overlay Districts where conditions warrant the use of special regulatory tools, and*
- f. Community services where compatible with existing land uses.*

*Policy 12: The City shall establish design standards to assure quality development and enhance the community's attractiveness and livability.*

*Policy 20: The City shall periodically review and update the Comprehensive Plan text and the Community Development Plan Map(s) to ensure they remain current and responsive to community needs; provide reliable information and dependable, factually based policy direction, and conform to applicable state law, administrative rules and regional requirements.*

*Policy 21: Council may, upon finding it is in the overall public interest, initiate legislative processes to change the Comprehensive Plan text and Community Development Plan Map(s) and Development Code.*

#### **Findings:**

These general Goals and Policies establish the City's intent to use its Comprehensive Plan (Gresham Community Development Plan [GCDP]) as the basis for appropriate planning processes and resulting land use plans.

The proposed amendments are part of the Retail Design and Development Standards project, which was requested by the Gresham City Council in 2010 to address the location, scale and design of future commercial developments. The City Council endorsed the project by adopting it in its 2010 Work Plan on December 15, 2009.



The project is intended to ensure commercial development in the Corridor Districts meets the community's vision and the City's goals by identifying appropriate locations and scales for large commercial development and creating design regulations which foster an attractive environment. Many of the land use designations were not consistent with the vision and permitted large commercial development to occur in locations removed from and not supporting the Centers. The 23 commercial nodes were evaluated for potential land use district changes based on several factors, including the presence of low-density residential districts, transit service and traffic capacity on adjacent arterials, development potential and the presence of environmentally sensitive areas. Based on these factors and extensive public discussion at community forums, Planning Commission and Council meetings, eight nodes are recommended to change land use districts. Allowable building sizes were also evaluated in a similar manner, with criteria including walkability, design and development capacity of large commercial sites. These size alternatives were also discussed at community forums, Planning Commission and Council meetings.

The proposed Code and Policy amendments address issues identified through an extensive public participation process including many public meetings with the Design Commission, the Planning Commission, City Council, residents, property owners, business owners and other interested parties. The proposal is consistent with state and regional provisions and is intended to meet the interests of the public for quality designs. The amendments provide Design Standards that are clear and objective in conformance with **ORS 197.307** governing local government reviews for needed housing as well as Design Guidelines for the discretionary review process. Density and residential use provisions are not being changed, in conformance with Metro housing standards.

The proposed plan text and plan map amendments were the result of a public planning process which included research and analysis of issues raised by the community; development, analysis and selection of a preferred approach to addressing the issues; and the drafting of text and map amendments that implement that preferred approach. Policy direction was provided by the public, Design Commission, Planning Commission and Council throughout the project.

#### **Conclusions:**

Policies 1, 2 and 12 are addressed because the proposed amendments are consistent with state and regional regulations and are intended to meet the interests of the citizens of Gresham. The proposed amendments with new commercial Design Principles, Guidelines and Standards implement the Development Plan Goals and Policies with regulations that promote high quality design. The updates are consistent with state and regional requirements.

Policy 3 is addressed because the proposed amendments are consistent with and support land use goals and policies which have been approved by the Gresham City Council, such as the Aspirations Presentation.

Policy 20 is met by the proposed text and plan amendments which are based on a public planning process that is based on factual information, issues identified by the community and on public policy direction.

Policy 21 is addressed because the proposed amendments were initiated with Council's adoption of the Council Work Plan in January of 2010.

The proposal is consistent with the applicable general Goals and Policies listed in this section.

## **2. Citizen Involvement Goals & Policies**

### **Section 10.100 - Citizen Involvement**

*Goal: The City shall provide opportunities for citizens to participate in all phases of the planning process by coordinating citizen involvement functions; effectively communicating information; and facilitating opportunities for input.*

*Policy 1: The City shall ensure the opportunity for citizen participation and input when preparing and revising policies, plans and implementing regulations.*

*Policy 2: The City shall consider the interests of the entire community and the goals and policies of the Comprehensive Plan when making decisions.*

*Policy 3: The City shall foster regular and ongoing two-way communication between citizens and City elected and appointed officials.*

*Policy 6: The City shall ensure that technical information necessary to make policy decisions is readily available.*

*Policy 7: The City shall facilitate involvement of citizens in the planning process, including data collection, plan preparation, adoption, implementation, evaluation and revision.*

*Policy 10: The City shall ensure the opportunity for the public to be involved in all phases of planning projects and issues.*

### **Findings:**

The public involvement Goals and Policies establish the City's intent that its citizens have meaningful opportunities throughout a planning project to be informed and to affect proposals. The Retail Design and Development Standards project is on the 2010 Council Work Plan, which began in January of 2010. The project has actively engaged the public in the planning process, including three community forums and numerous public Design Commission, Planning Commission and City Council meetings.

The City's Comprehensive Planning project web page has been kept up to date with schedules and drafts of the proposal. Public notice was also provided to the *Gresham Outlook* and notice was also sent to the State Department of Land and Conservation Development (DLCD) and Metro.

The outreach included:

- Design Commission meetings on 3/17, 6/16, 7/21, 8/18, 9/01, 9/15 and 10/20/2010.
- Developer's Group presentations on 4/23 and 8/26/2010.
- Planning Commission work sessions on 3/29, 6/04, 7/26, 9/13 and 10/11/2010.
- City Council work sessions on 4/19, 8/17 and 10/12/2010.
- Community Forums on 4/20, 8/04 and 10/14/2010.
- Neighborhood Coalition meeting on 6/08/2010.
- City staff prepared project documents (including public input summaries) and posted them on a project web page on the City's website. Interested parties were invited to evaluate the documents and propose revisions and new ideas.
- Information on the project has been made available at other Comprehensive Planning workshops.
- The askGresham e-mail tool has been used to alert interested parties when new materials are available on the website and when upcoming meetings will occur.
- Project information has been available at the Urban Design & Planning office.

### **Conclusion:**

*Policies 1, 2, 3, 7 and 10* are addressed by the work sessions and community forums attended by property owners and developers during the City-initiated review process, by staff presentations at Design Commission, neighborhood coalition and other meetings, and by Planning Commission and City Council public meetings and work sessions.

*Policy 6* is addressed by the posting of materials on the City's website and by making materials available through City staff in the Comprehensive Planning Section within the Urban Design & Planning offices.

The *Citizen Involvement Goal (10.100)* is met by the combination of work sessions, community forums, mailings, and meetings as well as providing information about the proposal and schedule on the City website and at City Hall.

The proposal is consistent with the applicable citizen involvement Goals and Policies.

### **3. Commercial Land Use Goals and Policies**

#### **10.312 Commercial Land Use**

##### **Applicable Policies**

*Policy 1: It is the policy of the City to provide an adequate amount of serviceable commercial land to facilitate the development of commercial centers or infill commercial strip development and prevent the need for lateral expansion of commercial strips along major streets.*

##### **Applicable Implementation Strategies**

- 1: *The city will meet consumer commercial services and retail needs by designating land for the development of regional, community and neighborhood scale centers.*
5. *The city will promote focused commercial development in downtown and Central Rockwood to take advantage of expected increased population densities in and around these areas as a result of the light rail transit system. Downtown will be the focus of retail and office development while transit-oriented retail, service, and office development will be promoted in Central Rockwood.*
6. *The city will establish locational criteria for siting commercial development.*

*Policy 2: It is the City's policy to encourage commercial development which increases employment opportunities; reduces dependency on outside of-city goods and services; promotes energy-efficient travel patterns; is compatible with neighboring land uses; and promotes good community design.*

##### **Applicable Implementation Strategies**

1. *The Community Development Standards document shall establish standards for commercial and office uses to ensure:*
  - a. *residential areas are buffered from potential adverse effects;*
  - b. *street access points are consolidated;*
  - c. *pedestrian circulation and safety is accomplished;*
  - d. *loading and parking areas are adequate to meet the demand;*
  - e. *adequate lighting is provided for crime prevention; and*
  - f. *landscaping is employed to enhance the appearance of the project.*
2. *The city shall encourage intensified commercial development in the city's downtown and Rockwood commercial district.*

#### **Findings:**

Commercial Land Use Goals and Policies establish the City's need to have an adequate supply of land for commercial development to support employment opportunities and economic development while ensuring retail needs of the community are met. The Goals and Policies also establish the City's support of the Regional, Town and Station Centers, encouraging development in the Downtown and Rockwood areas. The Retail Design and Development Standards found commercial land use regulations were in conflict with sections of these policies. The City's larger supply of commercial land in the Corridor Districts and the land use regulations of these districts do not support the City's policies to encourage development within the Regional, Town and Station Centers. Certain districts have no building size limit or minimum density requirement and the combination of these two regulations allows large commercial development which would draw from a very large market area. Staff heard many comments from citizens expressing a desire to have lower density commercial development in the Corridors that served a local, rather than regional, market area.

The proposed Code amendments address this issue through changes to the Community Plan Map and standard changes in the Development Code. The resulting action encourages the most intense commercial development to occur in the Centers. The changes allow development of lower intensities in the Corridor Districts to serve the surrounding residential areas, limiting long distance travel to obtain needed goods and services.

The proposed design regulations address several aspects of commercial development. Surrounding residential areas are buffered by perimeter screening, which is enhanced at loading areas to ensure year-round effectiveness. Regulations address connectivity between adjacent commercial sites to minimize unnecessary travel on existing streets and potential access points. Pedestrian circulation systems, including sidewalks and other paths, are required by the standards to include landscaping and other features which result in a pleasant and safe experience. Lighting standards are incorporated to ensure

safety on sites but to minimize negative impacts on adjacent properties. The regulations ensure that landscaping is used appropriately to improve the appearance of the development.

**Conclusion:**

Policy 1 and 2 are met through the proposed Community Plan Map amendments, changes to standards in the Development Code and proposed design regulations.

**4. Transit Corridor Plan Area Goals and Policies**

**10.319.1 Transit Corridor Plan Area**

**Applicable Policy**

*Policy 1: The City will permit and encourage land use types and intensities of use which support the creation of transit supportive development along the city's transit streets, accommodate forecast growth and are otherwise consistent with the Urban Growth Management Functional Plan and the 2040 Growth Concept Map.*

**Findings:**

A transit-supportive intensity for a Transit Corridor District is 25 persons per acre. This can intensity can be based on residents and employees. A retail development can generally accommodate this intensity with a minimum 0.25 floor-area-ratio (FAR). The propose change to Community Commercial to limit buildings to 100,000 square feet or within the Moderate Commercial to 80,000 square feet can easily accommodate a 0.25 FAR; considering that multiple buildings are permitted on a single property, the impact on commercial intensity may be negligible. Also, proposed building size limits do not include residential developed over commercial uses. Mixed-use building would still have to comply with footprint limits and maximum building heights. No proposed changes would result in intensities which are not transit-supportive.

**Conclusion:**

Policy 1 is addressed through proposed land use changes which maintain transit supportive densities.

**5. Community Design Goals & Policies**

**10.413 Community Design**

**Applicable Policies:**

*Policy 1: It is the City's Policy to establish a community design process which evaluates and locates development proposals in terms of scale and related community impacts with the overall purpose being a complementary land use pattern and long term stability.*

*Policy 2: It is the City's Policy to establish a community design process which evaluates individual development from proposals from a functional design perspective, considering such factors as privacy, noise, lights, signing, access, circulation, parking provisions for the handicapped, and crime prevention techniques.*

**Findings:**

There are currently no Community Design Goals and Policies which address commercial development in the Corridor Districts. For this reason a new section – *10.413.3 - Design Standards for Commercial Development in the Corridor Design District* – is included in the proposed text amendments. Below are the proposed new goal and policies and how the proposed new regulations are consistent with this new section:

*Goals: Commercial developments in the Corridor Design District will be human-scaled, attractive, safe and active places of excellent design which utilize high-quality and sustainable materials. Innovation and creativity in design is encouraged.*

**Policies:**

*Policy 1: Commercial developments should be designed and constructed to produce human-scale, high quality, safe, and comfortable shopping environments.*

*Policy 2: Commercial developments should appropriately respond and relate to their surroundings especially public streets, open spaces and recreation areas.*

*Policy 3: Commercial developments should minimize the impacts of parking, loading and garbage service areas on public streets, residents and adjacent properties.*

*Policy 4: Commercial developments should be thoughtfully and aesthetically designed with regard to site and building design.*

*Policy 5: Commercial developments should incorporate sustainable measures and the efficient use of land and resources.*

*Policy 6: Commercial developments should create sites with multi-modal transportation connections.*

*Policy 7: Open space within commercial developments should be attractive, functional, safe and of high quality to provide opportunities for active social interaction.*

*Policy 8: Standards applicable to commercial developments should provide measures of consistency and certainty to expedite the development review process.*

**Applicable Implementation Strategies:**

*Identify and assess methods that could be utilized to implement the Design Principles such as the two alternative review processes: the clear and objective process applying the Design Standards and the discretionary process applying the Design Guidelines.*

The proposed design regulations for sites and buildings are intended to create development which are attractive, human-scale and integrated into the surrounding neighborhoods or commercial districts. The regulations ensure buildings are sited appropriately and create attractive buildings oriented toward streets, with specific design attention paid to pedestrian-level facades. Large sites are required to have public streets or private drives with a street character at regular spacing to break down the scale of these areas; regulations provide specific direction to enhance connectivity to foster pedestrian accessibility, transit utilization and minimize unnecessary vehicular movements. The design regulations utilize landscaping extensively to improve the appearance of the sites, minimize visual impacts from parking and service areas, provide opportunities for stormwater management and shade landscapes to reduce heat islands. Additional sustainability measures are incorporated within the design regulations to minimize environmental impacts associated with large commercial developments.

The new amendments provide the applicant with a choice to follow the clear and objective process by meeting the Design Standards or to follow the discretionary process by meeting the Design Guidelines. Both the Guidelines and the Standards are based upon the Design Principles. The discretionary process is intended to provide the Architect or designer creative flexibility to present a very innovative project that may not specifically meet the Design Standards but do meet the Design Guidelines.

**Conclusion:**

The proposed new 10.413.3 Goal and Policies 1 through 8 are met through several regulations contained in the Site Design, Building Design and Sustainable Development section of the proposed Code amendment and by allowing an applicant to choose to follow the clear and objective or the discretionary design review process track.

Goal 10.413 and Policy 1 and Policy 2 are met by the addition of new Goal 10.413.3 based on the findings for this new goal and policies.

**C. METRO URBAN GROWTH MANAGEMENT FUNCTIONAL PLAN (UGMFP)**

**Title 8 Compliance Procedures**

**Findings:**

Section 3.07.820 of this title requires that at least 45 days prior to the first evidentiary hearing on an amendment to a Comprehensive Plan or land use regulation that the City submits the proposed amendments to Metro. Metro may review the amendments and can request that the City provide an analysis of the compliance of the amendment with the Functional Plan.

The City submitted the proposed amendments to both DLCD and Metro on September 23, 2010 which was at least 45 days prior to the first evidentiary hearing of November 8, 2010. Metro has submitted no comments or request for an analysis.

**Conclusion:**

The City has submitted the proposed amendments to Metro at least 45 days prior to the first evidentiary hearing and Metro has made no comments or request about the proposal. The proposal is consistent with Title 8.

**D. STATEWIDE PLANNING GOALS**

**1. Goal 1 – Citizen Involvement**

*Goal 1: To develop a citizen involvement program that insures the opportunity for citizens to be involved in all phases of the planning process.*

**Findings:**

Goal 1 requires municipalities adopt and publicize a program for citizen involvement that clearly defines the procedures by which the general public will be involved in the on-going land-use planning process.

The Retail Design and Development Standards project has followed the City of Gresham Public Participation Guide and has provided significant opportunity for the public to comment on and shape the recommendations throughout the process. Three community forums were held with the express purpose of educating the public about the project and to gain input. No less than 18 public meetings were held.

**Conclusion:**

Goal 1 is met by utilizing a public participation plan consistent with the City's Public Participation Guidelines.

**2. Goal 2 – Land Use Planning**

*Goal 2: Land Use Planning. To establish a land use planning process and policy framework as a basis for all decisions and actions related to use of land and to assure an adequate factual base for such decisions and actions. The goal also specifies components of a citizen involvement program.*

**Findings:**

Goal 2 requires the actions of jurisdictions and government agencies (in regards to land use) to be consistent with Comprehensive Plans adopted by cities, counties and regional governments. The Goal also describes considerations to be made as part of the development and adoption of Comprehensive Plans.

Gresham already has a State acknowledged Comprehensive Plan. The proposal here is a modification of that Plan and its Implementation Policies. Prior findings (above) have been provided to illustrate how the proposed modifications comply with the City's Comprehensive Plan.

**Conclusion:**

Goal 2 is met by showing conformance of the proposed provisions with the City's acknowledged Comprehensive Plan.

**3. Goal 9 – Economic Development**

*Goal 9: Economic Development: To provide adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare, and prosperity of Oregon's citizens.*

**Findings:**

Goal 9 ensures comprehensive plans address economic development to promote stable and healthy economies. The Goal specifies elements to be incorporated into a comprehensive plan, such as an analysis of the community's economic patterns.

The proposed land use changes support existing economic development efforts by encouraging higher density commercial development in the Regional, Town and Station Centers. By locating future large

commercial developments, especially retail, in these areas, existing businesses can benefit from the enhancement of traffic patterns in these areas and the associated visibility that comes with it. Proposed land use changes generally maintain the commercial nodes, though some nodes are proposed for lower intensity districts.

**Conclusion:**

Goal 9 is met by maintaining an adequate supply of sites suitable for commercial uses and locating future commercial development to support existing businesses in the city.

**SECTION VII  
CONCLUSION**

The proposed Comprehensive Plan amendments attached as Exhibit 'A' are consistent with applicable Goals and Policies of the Community Development Plan; the applicable Development Code procedures of the Community Development Plan; and applicable Metro UGMFP Code as indicated by findings contained or referenced in Section VI of this report.

**SECTION VIII  
RECOMMENDATION**

Staff recommends **adoption** of the proposed Comprehensive Plan Policy and Code amendments as contained in the attached Exhibit 'A' and 'B'.

***End of Staff Report***

Proposed new language is double-underlined;  
Proposed deleted language is ~~stricken~~.

CB 13-10

ORDINANCE NO. 1695

AMENDMENTS TO VOLUME 2, POLICIES, AND VOLUME 3, DEVELOPMENT  
CODE, OF THE GRESHAM COMMUNITY DEVELOPMENT PLAN, REGARDING  
THE RETAIL DESIGN AND DEVELOPMENT STANDARDS PROJECT AND  
DECLARING AN EMERGENCY AND PROVIDING FOR ENACTMENT IN ONE  
READING TO BE EFFECTIVE IMMEDIATELY

THE CITY OF GRESHAM DOES ORDAIN AS FOLLOWS:

Section 1. Volume 2, Policies to be amended as follows:

10.312 COMMERCIAL LAND USE

SUMMARY OF FINDINGS

\*\*\*\*\*

POLICY I

It is the policy of the City to provide an adequate amount of serviceable commercial land to facilitate the development of commercial centers or infill commercial strip development and prevent the need for lateral expansion of commercial strips along major streets.

IMPLEMENTATION STRATEGIES

\*\*\*\*\*

4. The city will provide opportunities for commercial uses in high density residential areas along major arterial streets. ~~These commercial uses are to be less intense than the Regional, Town and Station Center areas. The commercial district intensities should be graduated with the neighborhood type commercial uses being the least intense and should be low the lowest~~ traffic generating, smallest scale "walk-in" types of commercial uses.

5. The City will promote the most intense, focused commercial development in ~~d~~Downtown, Civic Neighborhood and Central Rockwood to take advantage of expected increased population densities in and around these Regional, Town and Station Center areas as a result of the light rail transit system. Downtown will be the focus of retail and office development while transit-oriented retail, service, and office development will be promoted in Civic Neighborhood and Central Rockwood.

\*\*\*\*\*

POLICY II

It is the City's policy to encourage commercial development which increases employment opportunities; reduces dependency on outside of-city goods and services; promotes energy-efficient travel patterns; is compatible with neighboring land uses; and promotes good community design.



## IMPLEMENTATION STRATEGIES

\*\*\*\*\*

2. The city shall encourage the most intensified commercial development in the city's dDowntown, Civic Neighborhood and Rockwood commercial districts. The commercial intensities along the major arterials outside of the Regional, Town and Station Center areas shall be less intense and more appropriate for serving the surrounding neighborhoods while supporting transit facilities.

\*\*\*\*\*

Volume 2, Policies to be amended by adding a new section 10.413.3 DESIGN STANDARDS FOR COMMERCIAL DEVELOPMENT IN THE CORRIDOR DESIGN DISTRICT as follows:

### BACKGROUND

Early in 2010, the City established a Council Work Plan project to address the potential negative impacts of large commercial developments in the Corridor Design District of the City such as their imposing size, design and visual character; their large parking areas and associated stormwater run-off; and their traffic generation, etc. This project builds off the Downtown Plan and the Multi-Family Design Standards projects and includes design regulations intended to promote a sense of community and to directly improve the safety, livability and aesthetic appearance of commercial developments. The new site, building and sustainability design regulations help facilitate the development of attractive and innovative commercial developments by providing two (2) alternative review tracks: one (1) clear and objective standards track and one (1) discretionary guidelines track to provide architectural flexibility and allow for greater Design Commission input. Extensive design analysis research was done on commercial development locally as well as excellent precedents found throughout the nation.

The project has involved an extensive public outreach effort including:

- Three Community Forums to gather citizen and interested party input;
- Several Stakeholder Group meetings of elected officials, residents, business owners and commercial development specialists; and
- Multiple public meetings with the Design Commission, Planning Commission and the general public.

### ISSUES

The result of these outreach efforts is that a series of issues relating to commercial developments have been identified which can be summarized as follows:

- Corridor Design District Commercial Vision: The Gresham Community Development Plan needs a clearly defined vision for superior quality design in commercial development, particularly large format commercial developments, which addresses design excellence, sustainability, access, building material quality, and crime prevention;
- Corridor Design District Commercial Goals, Policies, Principles and Action Measures: Additional Goals, Policies, Principles and Action Measures specific for commercial developments throughout the City are needed;

- Large Format Commercial Development Definition: Consensus must be established as to what the definition of large commercial development is in order to further regulate large commercial developments.
- Land Use Changes: Community Plan Map changes are necessary to the Corridor Design District to implement the vision by providing low intensity commercial uses with local neighborhood focus in those areas with high percentages of surrounding low density residential lands and less frequent transit service.
- Site, Building and Sustainable Design: The existing commercial clear and objective standards included in the Gresham Community Development Code (GCDC) are in need of updating. New standards are needed to define and limit building sizes, address site design, neighborhood connectivity, building orientation, open space location and character, landscaping, lighting, storage, crime prevention, architectural building design and sustainability to ensure higher quality commercial projects.

The Site Design issues raised include the following:

- Development Intensity. Developments need to be appropriately scaled to foster a pedestrian friendly environment.
- Site Design. Developments need to prioritize land uses and orient the buildings to the street or a central open space in order to encourage pedestrian activity on the street or open space.
- Connectivity. Connections between uses, both on-site and connections to adjacent sites, are necessary to create a more pedestrian friendly, livable community.
- Parking Lot Design and Circulation. The unsightly, unscreened massive parking areas typically associated with large commercial developments need to be mitigated.
- Street Orientation. The buildings need to be oriented at and toward the street with entries and windows facing the street for maximum and safe pedestrian interactions.
- Safe Design. Reviewing and incorporating the appropriate Crime Prevention through Environmental Design Standards (CPTED) needs to be considered for creating safer, more livable developments with natural access control, natural surveillance, and territorial reinforcement.
- Open Spaces. Open spaces that attract shoppers need to be defined so they encourage active use and enliven the development.
- Landscaping. The landscaping requirements need to create lush, attractive landscapes that enhance the appearance of the development, soften the bulk and scale of buildings, and screen parking areas. The long term maintenance of the site also needs to be addressed.

The Building Design issues raised include the following:

- Building Size. Commercial buildings need to be sized and scaled appropriately so they promote the livability and pedestrian quality of the commercial area.

- Design Excellence and Architectural Expression. The Code needs to facilitate design excellence and eliminate flat, poorly designed building facades in the built environment by addressing architectural elements like building articulation and facade design.
- Transparency in Architectural Design. Transparent windows need to be required where it is essential to provide visibility into and out of the buildings for pedestrian safety, comfort and interest.
- High Quality Materials. There is a need for developments to use the highest quality construction and the most durable materials in order to minimize long term maintenance issues and provide long lasting commercial developments.

The Sustainability Design issues raised include the following:

- Larger developments have a great impact on the environment and so the requirements need to be increased to mitigate those effects.
- Commercial developments should incorporate elements to create an attractive, sustainable site which conserves energy, protects our natural resources and promotes a healthier environment for residents.
- Two-Track Process: The Development Code now has a two track review process established for Downtown and Multi-Family developments in an effort to provide flexibility and allow innovative developments that may not comply with all Design Standards. There is a clear and objective series of Standards that the applicant can chose to follow or a series of discretionary Guidelines for review by the Design Commission based upon Design Principles. The Design Principles are the general statements that guide the design of commercial development and are the foundation for the discretionary Guidelines and the clear and objective Standards. This two track process will be applicable to commercial developments in the Corridor Design District as well.

## VISION

*The most intense commercial development shall occur in the Regional, Town and Station Centers while limiting commercial intensities in the Corridor Design District (particularly the Community Commercial, Moderate Commercial Corridor and Corridor Mixed Use-land use districts) to those intensities appropriate for serving surrounding neighborhoods while supporting transit facilities.*

## GOAL

Commercial developments in the Corridor Design District will be human scaled, attractive, safe and active places of excellent design which utilize high-quality and sustainable materials. Innovation and creativity in design is encouraged.

## POLICIES

1. Commercial developments should be designed and constructed to produce human scale, high quality, safe, and comfortable shopping environments.

2. Commercial developments should appropriately respond and relate to their surroundings especially public streets, open spaces and recreation areas.
3. Commercial developments should minimize the impacts of parking, loading and garbage service areas on public streets, residents and adjacent properties.
4. Commercial developments should be thoughtfully and aesthetically designed with regard to site and building design.
5. Commercial developments should incorporate sustainable measures and the efficient use of land and resources.
6. Commercial developments should create sites with multi-modal transportation connections.
7. Open space within commercial developments should be attractive, functional, safe and of high quality to provide opportunities for active social interaction.
8. Standards applicable to commercial developments should provide measures of consistency and certainty to expedite the development review process.

## DESIGN PRINCIPLES

The Design Principles, as general guiding statements, are the connection between the general planning goals and policies, and the implementing Design Guidelines and Standards. The Design Principles were drafted to address the issues and to formulate design direction with input from the Design Commission, Planning Commission, the general public and City staff. They are categorized as Site Design Principles and as Building Design Principles and are included in the Gresham Community Development Code, Volume III. Site Design Principles topics include:

- Accessibility
- Activity
- Building and Site Orientation
- Parking
- Public Spaces
- Landscaping
- Sustainability
- Safe Design
- Impact Mitigation

Building Design Principles topics include:

- Building Form and Articulation
- Building Activity and Glazing
- Prominence and Hierarchy
- High Quality Materials
- Sustainable Architectural Design

## ACTION MEASURES

1. Identify and assess methods that could be utilized to implement the Design Principles such as the two alternative review processes:
  - The clear and objective process applying the Design Standards; and

- The discretionary process applying the Design Guidelines.

2. Create an illustrated design guide, to be used as a handout, to assist developers, designers, decision makers, and the general public in understanding the design review process and the design regulations for commercial development proposals.

\*\*\*\*\*

Volume 2, Appendix C, the Community Development Plan Map is amended as shown in Exhibit B maps.

Section 2. Volume 3, Development Code, Article III Definitions is amended as follows:

\*\*\*\*\*

**3.0002. List of Terms**

Terms used in the Development Code are presented below. General terms that apply throughout the Code are listed in **Section 3.0010**. Terms that are specific to a Development Code section are listed after the General Terms. These categories are:

- A. Habitat Conservation Area Terms and Definitions. **Section 3.0020**
- B. HCA, ESRA, and Article V Terms and Definitions. **Section 3.0030**
- C. Solar Related Terms and Definitions. **Section 3.0040**
- D. Tree Related Terms and Definitions. **Section 3.0050**

If a term is defined in both the General Definitions and in a Section-specific category, the Section-specific definition shall be used if the application of the term is within the parameters of the Code Section.

**General Terms**

\*\*\*\*\*

**Amenity Zone**

\*\*\*\*\*

Building, Contiguous

\*\*\*\*\*

Entry, Primary

\*\*\*\*\*

Façade

- Prominent Façade Sections

\*\*\*\*\*

Grocery Store

\*\*\*\*\*

Landscaping

- Parking Area Landscaping

\*\*\*\*\*

Liner Space

\*\*\*\*\*

Parking Module

\*\*\*\*\*

Storefront Window

\*\*\*\*\*

Structural Soil

\*\*\*\*\*

Window Mullion

\*\*\*\*\*

Tree

- Tree Caliper

\*\*\*\*\*

### 3.0010 General Terms and Definitions

#### Tree Related Terms

- Caliper

\*\*\*\*\*

Amenity Zone. The area beginning at the back of the curb or outside edge of the street shoulder and extending to the property line, lying within the public right-of-way or on publicly owned property or in an easement. This area typically can include a planter strip with landscape plantings, street trees, and/or site furnishings like benches and lighting.

\*\*\*\*\*

Building, Contiguous. A contiguous building for purposes of the Commercial Design Standards is a single building or combination of buildings planned as a single development, regardless of structural independence, development phase or final lot lines which have a continuous and/or common wall plane. Referred to herein as Building within Section X.XXXX.

\*\*\*\*\*

Driveway (Drive). An area that provides access for vehicles to a site.

\*\*\*\*\*

Entry, Primary: A principal entry for people into a building which faces a public street.

\*\*\*\*\*

**Façade.** All exterior walls or faces of a building facing a public way or space. This may include the front, sides and/or rear of the building.

- Base. The lower portion of the building façade adjacent to the ground. This may include windows, texture, projections, awnings, canopies, ornamental detailing, etc. to enhance the pedestrian realm at the street level.
- Top. The upper portion of a building façade. This may include cornice detailing, roofs, dormers, and gable ends, etc.
- Prominent Façade Sections. Select areas of buildings which shall receive special design attention due to their location. These include building corners which front intersections of public streets or facade sections facing an intersection of two (2) arterial streets and facade sections which terminate the view down a right-of-way or primary internal drive.

\*\*\*\*\*

Grocery Store. A retail trade establishment in which more than 50% of the public floor area is dedicated to the sale of perishable and non-perishable food items which are intended for preparation and consumption off-site.

\*\*\*\*\*

## Landscaping.

\*\*\*\*\*

- Parking Lot Landscaping. Landscaped areas that are located within ten (10) feet of parking modules, internal drive aisles or parking stalls. This landscape area includes parking area perimeter buffers, landscaped islands, major landscape islands, tree wells and landscaping on internal public streets, primary internal drives and site buffers. Paved surfaces and walkways do not count toward any landscape area calculations for commercial developments in the Corridor Design District.

\*\*\*\*\*

Liner Space: Small commercial tenant space along the perimeter of a large commercial building. A liner space typically has an individual storefront and an entry from the exterior of the building. This space(s) can be used to conceal outdoor loading areas.

\*\*\*\*\*

Parking Lot. Pavement/hard surface area and associated circulation routes dedicated to used for parking vehicles off-street or beyond the right-of-way, either free or for a fee. When calculating the size of the parking lot, it shall include paved parking stalls, drive aisles, primary internal drives and those internal public streets which include on-street parking. Service drives and paved loading areas shall not count when calculating the area of the parking lot. Parking areas for one and two-unit dwellings are not parking lots.

\*\*\*\*\*

Parking Module. One (1) or two (2) rows of parking stalls of any length served by a single drive aisle for access.

\*\*\*\*\*

Storefront Window: A large ground-floor window of transparent glass located between the heights of two (2) and twelve (12) feet above grade and which is used for display purposes and/or for visibility into the store.

\*\*\*\*\*

Structural Soil. Structural Soil: A type of soil which meets the load-bearing requirement for structurally sound pavement installation while encouraging an enhanced growing environment and deep root growth for trees away from the pavement surface. Typical examples include CU (Cornell University) Structural Soil™ and sand based structural soil.

\*\*\*\*\*

Tree Caliper. An ANSI (American National Standards Institute) standard for the measurement of nursery trees. For trees up to six (6) inches in diameter, caliper is measured at six (6) inches above the ground level. Trees that are seven (7) to twelve (12) inch in diameter, caliper is measured at twelve (12) inches above the ground. For nursery stock above twelve (12) inches in diameter, a DBH measurement is used (see Diameter Breast Height).

\*\*\*\*\*

Window Mullion: A dividing element in a window or window opening which separates and supports individual panes of glass.

\*\*\*\*\*

### 3.0050 Tree Related Terms and Definitions

The following definitions apply to Tree-related applications, including Sections 9.1000, Tree Regulations and Appendix 14, Significant Trees.

- Caliper. An ANSI (American National Standards Institute) standard for the measurement of nursery trees. For trees up to 6 inches in diameter, caliper is measured at 6 inches above the ground level. Trees that are 7 to 12 inch in diameter, caliper is measured at 12 inches above the ground. For nursery stock above 12 inches in diameter, a DBH measurement is used (see Diameter Breast Height).

Section 3. Volume 3, Development Code, Section 4.0200 Commercial Land Use Districts is amended as follows:

\*\*\*\*\*

4.0201 Purpose

#### Land Use District Characteristics

4.0210 Neighborhood Commercial District (NC)

4.0211 General Commercial District (GC)

4.0212 Central Rockwood and Corridor Districts

#### Permitted Uses

4.0220 Permitted Uses

4.0221 Other Permitted Uses

#### Commercial Land Use District Standards

4.0230 Commercial Land Use District Standards

4.0231 Additional Commercial Land Use District Standards

#### General

##### 4.0201 Purpose

Development on lands designated Neighborhood Commercial, ~~General Commercial, Extensive Commercial and Exclusive Commercial~~ Districts is permitted when consistent with the provisions of this section and all other applicable requirements of the Community Development Code.

#### Land Use District Characteristics

##### 4.0210 Neighborhood Commercial District (NC)

The Neighborhood Commercial District is intended to provide for small to medium sized shopping and service facilities and limited office uses adjacent to residential neighborhoods. The district is intended to meet the shopping and service needs of the immediate neighborhood and to have minimal negative impacts on surrounding residential uses. Areas determined appropriate for Neighborhood Commercial Districts are identified on the Community Development Plan Map.

4.0211 General Commercial District (GC)



The General Commercial District is intended to provide opportunities for retail, service and office development in commercial centers and existing commercial strips. Most business activities in this district are intended to be conducted within a completely enclosed building. A limited area may be used for outdoor business activities, product display or storage. Areas determined appropriate for General Commercial Districts are identified on the Community Development Plan Map

**4.0212 Central Rockwood and Corridor Districts**

Additional Commercial Development Districts unique to the Corridor Districts are listed in **Section 4.0400**. The Gresham Community Development Plan Map identifies the location of these additional development districts.

**Permitted Uses**

\*\*\*\*\*

**Table 4.0220: Permitted Uses In The Commercial Districts 1**

USES	NC	GC
A. Retail service establishments engaged in selling goods or merchandise to the general public for personal or household consumption (with no floor area limitation).	NP	P
B. Business service establishments engaged in rendering services to other businesses on a fee or contract basis such as: 1. Advertising, building maintenance, employment services, and consulting services. 2. Household moving and equipment rental, car washes, outdoor commercial amusements and mini-storage facilities.	NP NP	P NP
C. Eating and drinking establishments (maximum gross floor area of 3,500 square feet).	P	P2
D. Insurance agencies, real estate and other offices (maximum gross floor area of 5,000 square feet per use).	P	P2
E. Grocery stores (maximum gross floor area of 35,000 square feet).	P	P2
F. Personal service establishments includes: laundries, dry cleaners, barber shops and hair salons, and shoe repair shops, with a maximum gross floor area of 5,000 square feet per use.	P	P2
G. Retail businesses (hardware, gas stations, drug, clothing, photography, and similar retail uses with a maximum gross floor area of 10,000 square feet per use).	P	P2
H. Offices and Clinics	NP	P
I. Retail Trade Establishments engaged in selling goods or merchandise to the general public for personal or household consumption such as: 1. Retail Groceries, Department Stores Hardware Stores, and Sporting Goods Stores. 2. Automobile sales, mobile home or recreational vehicle sales, nurseries, and lumber sales.	NP NP	P NP
J. Community services	P	P
K. Temporary uses	P	P
L. Home occupations	P3-2	P3
M. Temporary Health Hardship Dwelling	L4-3	L4

N. Offices which are related and subordinate to any of the preceding permitted uses.	NP	NP
--	----	----

**Table 4.0220 Notes:**

City of Gresham Development Code (5/09)

1. See **Appendix 2.000** for expanded list of uses.
2. ~~Without floor area restrictions.~~
23. Home occupations shall be permitted only within pre-existing homes in the NC and GC development districts.
34. Permitted only in conjunction with pre-existing single-family homes in accordance with **Section 10.1300**.

\*\*\*\*\*

**4.0221 Other Permitted Uses**

Other uses can be allowed in the GC District which, in the determination of the Manager, are:

- A. ~~Business activities which are mostly conducted within a completely enclosed building; and,~~
- B. ~~Consistent with the applicable commercial land use policies and implementation strategies of the Community Development Code.~~

**4.0230 Commercial Land Use District Standards**

The site development requirements listed in **Table 4.0230** are applicable to all development within the Neighborhood Commercial and ~~General Commercial~~ districts. Development within these districts shall also be consistent with all other applicable requirements of the Community Development Code.

**Table 4.0230: Development Requirements For Commercial Districts**

	NC	GC
<b>A. Maximum Site Size</b>	4 acres <sup>1</sup>	Not Applicable
<b>B. Minimum Lot Size</b>	10,000 square feet	10,000 square feet
<b>C. Minimum Lot Dimensions</b>		
1. Width	1. 60 feet	1. 60 feet
2. Depth	2. 70 feet	2. 70 feet
<b>D. Minimum Yard Setbacks<sup>2</sup></b>		
1. Front	1. 20 feet	1. 20 feet
2. Side	2.	2.
a. interior lot	a. 0 feet	a. 0 feet
b. corner lot	b. 0 feet on the interior side and 15 feet on the side abutting the street <sup>3</sup>	b. 0 feet on the interior side and 15 feet on the side abutting the street
3. Rear	3. 0 feet	3. 0 feet
<b>E. Maximum Building Height</b>	35 feet <sup>4</sup>	See Section 4.0231(A) <sup>4</sup>
<b>F. Maximum Lot Coverage</b>	50%	Not Applicable
<b>G. On-Site Activities</b>	See Section 4.0231(B) <sup>5</sup>	See Section 4.0231(B) <sup>6</sup>

		Areas devoted to on-site outdoor business activities, product display or storage must be located so that they do not interfere with pedestrian circulation.
<b>H. Public Facilities, Site and Supplementary Requirements</b>	See Section 4.0231(C)	See Section 4.0231 (C)

**Table 4.0230 Notes:**

1. This requirement does not apply to the Springwater Plan District.
2. Buffering and screening may be required in addition to these setbacks. See **Section 9.0100** for the buffering and screening requirements. Also, refer to the height transition area requirement found in the Maximum Building Height **Section 4.0231(A)** since it can increase the minimum yard setbacks where a development abuts a residential district.
3. However, for up to fifty percent (50%) of the length of the side yard abutting a street (excluding that portion of the side yard which overlaps the required front yard), the street-side setback may be reduced to five (5) feet.
4. See **Section 9.0600 - Height Transition**
5. At least ninety-five percent (95%) of the business activities must be conducted within a completely enclosed structure. No more than five percent (5%) of the area devoted to business use may be used for outdoor business activities, product display, or storage.
6. ~~At least 85% of the business activities must be conducted within a completely enclosed structure. No more than 15% of the area devoted to business use may be used for outdoor business activities, product display, or storage.~~

**4.0231 Additional Commercial Land Use District Standards**

- A. Maximum Building Height: Three (3) stories or forty (40) feet unless equipped with a built-in fire protection system. When fire sprinklers, alarms, and when needed, enclosed, pressurized exit stairwell systems are provided, the building height can be increased to sixty-five (65) feet.
- B. On Site Activities:
  1. No outdoor business activities, product display or storage shall be located within yard setback or buffering and screening areas.
  2. Areas devoted to on-site outdoor business activities, product display or storage must be located so that they do not interfere with pedestrian circulation. ~~in the General Commercial District.~~
- C. Public Facilities, Site and Supplementary Requirements. All developments shall also be subject to the applicable requirements of **Section 4.0230 - Commercial Land Use District Standards; Article 9 - Common Requirements; and Appendix 5.000 - Public Facilities.**

**Section 4. Volume 3, Development Code, Section 4.0400 Corridor Districts is amended as follows:**

\*\*\*\*\*

**4.0413 Corridor Mixed-Use (CMU)**

This district designation is applied to certain clusters of properties along Transit Streets. In addition to moderate-density, multi-family residential uses, the CMU district permits small-scale commercial uses

and mixed-use developments. Commercial businesses operating in this district will serve primarily the day-to-day needs of residents in nearby housing developments and neighborhoods. Design standards in Section 7.0600 for new construction and remodels which meet the thresholds described in Article 7.0003 will help to ensure that new buildings become attractive additions to existing and developing neighborhoods.

**4.0414 Community Commercial (CC)**

This district designation is applied to larger nodes of primarily commercial development clustered around the intersections of arterial streets. This district services the surrounding community with a larger trade area than the Moderate Commercial but still has building size limitations for compatibility with the adjacent residential properties. The CC district will accommodate a wide range of community-scale commercial uses, including retail, services, and offices. This district also permits housing as a secondary use, with attached dwellings being developed in conjunction with commercial construction. New buildings will be pedestrian-oriented, with parking placed behind or beside buildings. Design standards in Section 7.0600 for new construction and remodels which meet the thresholds described in Section 7.0003 will help to ensure that new buildings become attractive additions to existing and developing neighborhoods.

**4.0415 Moderate Commercial (MC)**

The MC district is applied to smaller nodes of commercial activity than the Community Commercial and is clustered around key intersections. These districts are intended to function primarily as locally-oriented centers serving smaller trade areas than the Community Commercial district. Building size limitations ensure compatibility with the surrounding neighborhoods. Permitted development types include commercial retail, service, and office uses. This district also permits housing as a secondary use, with attached dwellings being developed in conjunction with commercial construction. Design standards in Section 7.0600 for new construction and remodels which meet the thresholds described in Section 7.0003 will ensure a strong pedestrian orientation for new development.

\*\*\*\*\*

**Table 4.0420: Uses Permitted in the Corridor Districts**

Use Categories:	Rock-wood Town Center	Station Center	Station Center (Ruby Jct. Overlay)	Corridor Multi-Family	Corridor Mixed-Use	Community Commercial	Moderate Commercial
<b>Commercial Uses:</b>							
A. Offices	P	P <sup>10</sup>	P	NP	P <sup>5</sup>	P <sup>16a,16b</sup>	P <sup>4a,4b</sup>
B. Clinics	P	P <sup>10</sup>	P	NP	P <sup>5</sup>	P <sup>16a,16b</sup>	P <sup>4a,4b</sup>
C. Retail Trade	P	P <sup>10</sup>	P	NP	P <sup>5</sup>	P <sup>16a,16b</sup>	P <sup>4a,4b</sup>
D. Retail Service	P	P <sup>10</sup>	P	NP	P <sup>5</sup>	P <sup>16a,16b</sup>	P <sup>4a,4b</sup>
E. Business Service	P	P <sup>10</sup>	P	NP	P <sup>5</sup>	P <sup>16a,16b</sup>	P <sup>4a,4b</sup>
F. Auto-Dependent Use	L <sup>1</sup>	NP	P	NP	P <sup>5</sup>	P <sup>16a,16b</sup>	P <sup>4a,4b</sup>
G. Outdoor Commercial	NP	NP	L <sup>3</sup>	NP	L <sup>3</sup>	P <sup>16a,16b</sup>	P <sup>4a,4b</sup>

H. Mini-Storage Facilities	NP	NP	NP	NP	NP	NP	NP
<b>Residential Uses:</b>							
I. Attached Dwellings on a Single Lot	P <sup>9</sup>	P <sup>13</sup>	P	P	P	L <sup>11, 16a</sup>	L <sup>4a, 11</sup>
J. Single-Family Attached Dwellings	P <sup>9</sup>	P	P	P	P	NP	NP
K. Duplex	NP	P	P	P	P	NP	NP
L. Single-Family Dwelling (Detached)	NP	NP	NP	L <sup>8</sup>	NP	NP	NP
M. Accessory Dwelling	P	P	P	P	P	NP	NP
N. Residential Homes	NP	NP	NP	L <sup>8</sup>	NP	NP	NP
O. Residential Facilities	P	P	P	P	P	L <sup>11, 16a</sup>	L <sup>4a, 11</sup>
P. Industrial Uses	NP	NP	L <sup>6, 7</sup>	NP	NP	NP	L <sup>4a, 14</sup>
Q. Mixed-Use Development	P <sup>9</sup>	P	P	NP	P	P <sup>15, 16a, 16h</sup>	P <sup>4a, 4b, 15</sup>
R. Temporary Uses	P	P	P	P	P	P	P
S. Accessory Structures	P	P	P	P	P	P	P
T. Temporary Health Hardship Dwelling	L <sup>12</sup>	L <sup>12</sup>	L <sup>12</sup>	L <sup>12</sup>	L <sup>12</sup>	L <sup>12</sup>	L <sup>12</sup>
U. Home Occupations	P	P	P	P	P	P	P
<b>Community Service Uses:</b>							
V. Type I	P	P	P	P	P	P <sup>16a, 16h</sup>	P <sup>4a, 4h</sup>
W. Type II	P	P	P	P	P	P <sup>16a, 16h</sup>	P <sup>4a, 4h</sup>
X. Type III	P <sup>2</sup>	P <sup>2</sup>	P	P	P	P <sup>16a, 16h</sup>	P <sup>4a, 4h</sup>

Table 4.0420 Notes:

\*\*\*\*

<sup>4</sup> Moderate Commercial.

a. The maximum building footprint size permitted for any building, regardless of the type of uses within it, occupied entirely by a commercial use or uses shall be 40,000 square feet with an exception for a maximum building footprint size of 60,000 square feet for a grocery store use.

b. The maximum total building size (floor area) for commercial uses shall be 80,000 square feet.

<sup>5</sup> The maximum building footprint size permitted for any building occupied entirely by a commercial use or uses shall be 10,000 square feet. For mixed-use buildings, the residential and commercial components shall comply with the standards of Section 7.0103(B)(1)(d)(2).

\*\*\*\*

<sup>16</sup> Community Commercial.

a. The maximum building footprint size permitted for any building, regardless of the type of uses within it, shall be 100,000 square feet; and

b. The maximum total building size (floor area) for commercial uses shall be 100,000 square feet.

**Corridor District Standards**

**4.0430 Development Standards**

Table 4.0430, below, summarizes development standards which apply in the Corridors and Districts. The standards contained in this table are supplemented by referenced subsections which provide additional clarification or guidance. Furthermore, the regulations of Section 7.0600 Corridor Design District Commercial Design Guidelines and Standards shall also apply.

**Table 4.0430 Development Requirements For Corridor Districts**

	Rock-wood Town Center	Station Centers	Station Center (Ruby Jct. Overlay)	Corridor Multi-Family	Corridor Mixed-Use	Community Commercial	Moderate Commercial
****							
<b>H. Maximum Building Setbacks (Section 4.0433)</b>	Commercial & Mixed-Use: 10 feet front and street-side <sup>2</sup> ; None for	20 feet front; None for rear and interior side; 20 feet for street-side <sup>3a</sup>	For residential, Commercial, and mixed-use: 20 feet front; none for rear	20 feet front; None for rear and interior side; 20 feet for street-side <sup>3a</sup>	20 feet front; None for rear and interior side; 20 feet for street-side <sup>3a</sup>	<del>10</del> 20 feet front; None for rear and interior side; 10 feet for street-side <sup>3b,c</sup>	<del>10</del> 20 feet front; None for rear and interior side; 10 feet for street-side <sup>3b,c</sup>

	interior side and rear. Residential: 20 feet front; None for interior side and rear; 20 feet street-side <sup>3a</sup>		and interior side; 20 feet for street-side. <sup>3a</sup> No maximum for Industrial uses.				
I. Minimum Building Height (Section 4.043 <del>3</del> )	2 stories	2 stories	2 stories (None for auto-dependent and industrial)	None	None	None	None
J. Maximum Building Height (Section 4.0434)	None <sup>9</sup>	80 feet <sup>9</sup>	80 feet <sup>9</sup>	45 feet <sup>9</sup>	45 feet <sup>9</sup>	80-45 feet <sup>9</sup>	45 feet <sup>9</sup>
K. Transit Design Criteria and Standards Apply (Section 4.0435)	Yes <sup>7</sup>	Yes <sup>7</sup>	Yes <sup>7</sup>	Yes <sup>7</sup>	Yes <sup>7</sup>	Yes <sup>7</sup>	Yes <sup>7</sup>
****							

Table 4.0430 Notes:

\*\*\*\*

2. A maximum front or streetside setback of up to twenty (20) feet may be permitted when enhanced pedestrian spaces and amenities are provided that comply with Section 7.0603(A)(5)(D).

3. The following setback standards apply:

- a. When abutting a Principal Arterial, a Major Arterial, a Minor Arterial or Boulevard street, the maximum front or streetside setback for a building containing dwelling units abutting a Principal Arterial, a Major Arterial, or a Minor Arterial street is thirty (30) feet. The maximum front or streetside setback may be exceeded when enhanced pedestrian spaces and amenities are provided that comply with Section 7.0603(A)(5)(D).
- b. When abutting a Principal Arterial, a Major Arterial, a Minor Arterial or Boulevard street, the maximum front or streetside setback for a building containing commercial uses is ten (10) feet. The maximum front or streetside setback may be exceeded when enhanced pedestrian spaces and amenities are provided per Section 7.0603(A)(5)(D).
- c. When abutting a Collector, Community or Local streets, the maximum front or streetside setback is five (5) feet. The maximum front or streetside setback may be exceeded when enhanced pedestrian spaces and amenities are provided that comply with Section 7.0603(A)(5)(D).

\*\*\*\*

7. Ground floor window standards for commercial buildings on transit streets (Section 7.0210) do not apply to residential developments.

\*\*\*\*

#### 4.0433 Setbacks

Required minimum and maximum setback standards are specified in Table 4.0430.

\*\*\*\*

#### B.

1. For Rockwood Town Center (RTC), Station Center (SC) and Station Center- Ruby Junction (SC-RJ);

Conformance with maximum setback distance is achieved for a commercial or mixed-use building when at least one (1) primary entrance located on the façade facing the street is placed no farther from the property line than the distance specified for Maximum Building Setback in Table 4.0430. For residential buildings, conformance is achieved when at least 50% of the façade facing the street is placed no farther from the property line than the distance specified for Maximum Building Setback in Table 4.0430. Maximum building setbacks may be exceeded when a development incorporates enhanced pedestrian spaces and amenities in the setback area. Enhanced pedestrian spaces and amenities consist of features such as plazas, arcades, courtyards, outdoor cafes, widened sidewalks, benches, shelters, street furniture, public art, or kiosks. In addition, on sites with more than one building, the maximum setback may be exceeded for commercial, mixed-use, and residential buildings containing three (3) or more dwelling units, provided conformance is achieved with the maximum setback distance for at least one (1) building. For single-family attached residential buildings, conformance also must be achieved with building orientation standards of Section 7.0201(E) for attached dwellings.

2. For commercial developments in Corridor Mixed-Use (CMU), Community Commercial (CC), and Moderate Commercial (MC), see Section 7.0600.

3. For mixed-use developments and attached residential dwellings on a single lot in all districts, see Section 7.0100.

\*\*\*\*

D. Setback standards for single-family attached dwellings are in Table 7.0201(ML)(3)(a).

#### 4.0434 Building Height

Minimum and maximum building heights are specified in Table 4.0430.



\*\*\*\*

B. In addition to conforming with the Ground Floor Windows requirements of **Section 7.0210**, for any new commercial or mixed-use building subject to a two (2) story height minimum, at least twenty percent (20%) of the upper facade area shall be made up of display areas or windows for all facades facing a street, except those developments subject to the Corridor Commercial Design Standards of Section 7.0600 and those subject to Plan District requirements such as the Downtown Plan District.

C. The maximum building height for any building containing dwelling units shall be reduced when located adjacent to an LDR-5, LDR-7, TLDR, or TR District, as provided in **Section 7.0201(KJ)**.

D. Heights of single-family attached dwelling buildings are identified in **Table 7.0201(ML)(3)(g)(f)**.

**4.0435 Transit Design Criteria and Standards in Central Rockwood and Corridor Districts**

The Central Rockwood Plan and Corridor districts are pedestrian districts. As such, new development must have a strong orientation to the pedestrian and be transit-supportive, as well as enhance the appearance and functioning of these districts. In order to achieve these purposes, the provisions of **Section 7.0103 and 7.0201** apply to new residential and mixed-use (residential) development, **Section 7.0202** applies to new commercial, industrial and mixed-use (commercial) development requiring design review approval ~~in all Corridor districts that is not in the Corridor Design District.~~ **Section 7.0600 Corridor District Commercial Design Guidelines and Standards** applies to all commercial development in the Corridor Design District, and **Section 7.0210(A)** applies in addition to other applicable standards and criteria, to all developments ~~except to those commercial developments in the Corridor Design District.~~ Additionally, the provisions of **Section 7.0210(B)** apply to new development requiring design review approval in the Station Center and Rockwood Town Center Districts.

\*\*\*\*

**Section 5. Volume 3, Development Code, Section 4.1200 Civic Neighborhood Plan District is amended as follows:**

\*\*\*\*\*

**4.1236 Building Lines, Orientation and Primary Entrance**

\*\*\*\*\*

**B. Standards:**

- 1. All new commercial, mixed-use, and community service buildings in all sub-districts of the Civic Neighborhood PD shall comply with the following standards for Building Orientation and Primary Entrance:
  - a. All buildings shall have at least one (1) of their primary entrances face an abutting street or, if available, on a primary pedestrian street, rather than the parking area. ~~Primary entrance is defined as a principal entry through which people enter the building. A building may have more than (1) one primary entry, as defined in the Building Code.~~

**Section 6. Volume 3, Development Code, Section 4.1100 Downtown Plan District Design Manual is amended as follows:**

\*\*\*\*\*

**Section 4.1101.B. How to Use the Code**

\*\*\*\*\*

6. Images.

Most images, including photographs, illustrations, and maps, included in the Downtown Plan District Design Manual (4.1100) are not part of the Development Code and do not act as Guidelines or Standards. These images are provided to assist readers in envisioning the intent and potential outcomes of the Guidelines and Standards.

Images that are not part of the Development Code are labeled as figures. Images that are part of the Development Code will be labeled with a Development Code section number.

**Section 7. Volume 3, Development Code, Section 7.0101 Dwelling Structures Containing Two or More Units, Elderly Housing and Mixed-Use Developments (Residential) General is amended as follows:**

\*\*\*\*\*

**Section 7.0101.D. How to Use the Code**

\*\*\*\*\*

4. Images.

Most images, including photographs and illustrations, are not part of the Development Code and do not act as Guidelines or Standards. These images are provided to assist readers in envisioning the intent and potential outcomes of the Guidelines and Standards. Images that are not part of the Development Code are labeled as figures. Images that are part of the Development Code will be labeled with Development Code section numbers.

**Section 8. Volume 3, Development Code, Section 7.0202 Community Service, Commercial, Industrial, and Mixed-Use Developments (Commercial Component) is amended as follows:**

**7.0202 Community Service, Commercial, (except those in the Corridor Design District), Industrial, and Mixed-Use Developments (Commercial Component)**

The following design review criteria and standards shall apply to Community Service (except elderly housing), Commercial,

(except those in the Corridor Design District reviewed under Section 7.0600), Industrial, and Mixed-Use Developments (with the exceptions that single-family attached units in a mixed-use development shall be reviewed per the standards of Section 7.0201 and that the residential portion of the mixed-use development shall comply with Sections 7.0101-7.0103).

In designing the site development plan and landscaping plan the following design criteria and standards shall apply:

- A. Areas to be landscaped as defined in Section 3.0010 - Definitions:
  - 1. Community Services
    - All areas not occupied by structures, pavement, or outdoor business activity, display or storage areas. In no case shall less than fifteen percent (15%) of the gross site area be landscaped.
  - 2. Commercial, Industrial and Mixed-Use Developments
    - a. A minimum of fifteen percent (15%) of the gross site area:
      - Office/Residential District;
      - Neighborhood, ~~General Moderate, and Community~~-Commercial Districts;
      - General and Heavy Industrial Districts
    - b. A minimum of 20% of the gross site area:
      - CMU District
    - e. Setback areas shall be landscaped or provided with enhanced pedestrian spaces such as benches and drinking fountains:

- Station Center and Rockwood Town Center Districts
- Neighborhood Commercial District, ~~General, Moderate, and Community Commercial Districts~~

- cd.** Any site area not developed for structures, paving, or enhanced pedestrian spaces shall be improved with landscaping.
- Station Center and Rockwood Town Center Districts

\*\*\*\*\*

**Section 9. Volume 3, Development Code, Section 7.0600 Corridor Design District Commercial Design Guidelines and Standards are added as follows:**

*Section 7.0600 is currently an empty place-holder section. Sections 7.0600, 7.0601, 7.0602 and 7.0603 will be incorporated into section 7.0600.*

See Attachment A - Section 7.0600 Corridor Design District Commercial Design Guidelines and Standards.

**Section 10. Other Development Code Sections are amended as follows:**

**4.1151(B) Design Guidelines and Standards: All Downtown Sub-Districts**

**(5) Transparency**

**(D) Standards**

\*\*\*\*\*

**Commercial, Employment, Live/work, Mixed-Use**

\*\*\*\*\*

4. To meet the clear, transparent glass requirement, storefront windows shall have a Visible Transmittance (VT) value of 90% sixty percent (60%) or greater.

\*\*\*\*\*

6. Films or other substances applied to the window to reduce the Visible Transmittance value below 90% sixty percent (60%) shall be prohibited.

\*\*\*\*\*

**7.0103(B) Two or More Units, Elderly Housing and Mixed-Use (Residential) Design Guidelines and Standards**

**(2) Façade Composition**

**(d) Standards**

\*\*\*\*\*

**Mixed-Use Style Developments.**

\*\*\*\*\*

14. To meet the transparent glass requirement, storefront windows shall have a Visible Transmittance (VT) value of ~~ninety percent (90%)~~ sixty percent (60%) or greater.

\*\*\*\*\*

#### 9.0110 Buffering and Screening Requirements

\*\*\*\*\*

C. For purposes of this section a vacant lot is a lot that is undeveloped or developed with a non-conforming use. Least to more intensive use is: LDR-5 and LDR-7 District dwellings, TR District dwellings, TLDR District dwellings, two (2) to four (4) attached dwellings or single-family attached dwellings, five (5) or more attached dwellings or single-family attached dwellings, residential community service, primarily residential mixed-use, office use, NC use, ~~GC~~ use, primarily commercial mixed-use, non-residential community service use, outdoor commercial use, ~~GI~~ use, and HI use.

\*\*\*\*\*

#### 9.0610 Height Transition Standards

\*\*\*\*\*

B. Commercial and Industrial Districts: The standard under **9.0610(A)** applies to all buildings to be built on lots in the NC, ~~GC~~, GI and HI Districts when those lots abut any district allowing residential development.

\*\*\*\*\*

#### Signs in Commercial, Mixed Use & Industrial Land Use Districts

##### A6.100 Commercial, Mixed Use and Industrial Districts

Signs in the NC, ~~GC~~, RTC, SC, CMU, CC, MC, GI and HI Districts and in the NC-PV, MUE-PV, TC-PV, EC-PV, VC-SW, RTI-SW, IND-SW and NC-SW sub-districts shall be subject to the following limitations, except for multi-business complexes (see **Section A6.101** for multi-business complexes):

\*\*\*\*\*

##### A6.020 Measurements

5. Free standing signs in the RTC, SC, CC, MC, NC, ~~GC~~, GI and HI Districts:

#### Section 11. Emergency Clause and Effective Date.

The City Council finds that the important proposed changes to the Gresham Community Development Plan contained in this Ordinance have been developed over a period of many months and are superior to the existing provisions of the Gresham Community Development Plan. The provisions of this Ordinance must be put into effect immediately to allow new development proposals to be subject to the provisions of this Ordinance and thereby meet the requirements of these superior new commercial design regulations. This Ordinance is deemed to be necessary for the preservation of the livability, health, safety and welfare of the people of the City of Gresham and, therefore, an emergency is hereby declared to exist and this Ordinance shall take effect immediately upon passage in one (1) reading by the City Council.

Enacted immediately upon one (1) reading at a single meeting of the City Council on November 16, 2010.

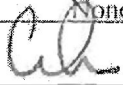
First reading and passed: November 16, 2010

Yes: Bemis, Widmark, Fuhrer, Craddick, Strathern, Warr-King, Nielsen-Hood

No: None

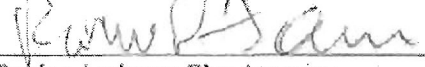
Absent: None

Abstain: None

  
City Manager

  
Mayor

Approved as to Form:

  
Senior Assistant City Attorney

Section 7.0600

Corridor  
Design District  
Commercial  
Design  
Guidelines and  
Standards

November 16, 2010

City of Gresham

# 7.0601 Corridor Design District

## Commercial Design Guidelines and Standards

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### A. **Applicability:**

This section shall apply to all commercial development within the Corridor Design District as defined in Article 3, unless superseded by the Plan District Standards. The Downtown Plan district is exempt from the Section 7.0601 regulations.

### B. **Purpose:**

Commercial design Guidelines and Standards help facilitate the development of attractive, innovative, high-quality and sustainable commercial developments by identifying the characteristics of good site and building design. Commercial design regulations promote livability, foster safe developments and encourage multi-modal transportation, thereby fostering quality environments and a sense of community throughout the City.

### C. **Design Review Process:**

New commercial developments, additions and remodels are subject to design review as defined in Article 3.0000 Definitions of Design District and Article for the determination of consistency with the Guidelines and/or Standards contained in this Code. Refer to Article 11 Procedures for review process details. Projects subject to design review are either reviewed by the Design Commission or reviewed by the Manager. Either the Design Commission or the Manager shall make findings and decisions concerning conformance with the Design Standards or Guidelines, based on which review process is selected.

Buildings over 30,000 square feet of floor area shall follow an enhanced design review process, and shall be required to have an initial consultation with the Design Commission to proactively address design issues early in the development process.

### D. **How to Use this Code:**

#### 1. Design Review:

The City has set up two tracks for Design Review processes. Applicants have the choice of complying with either option:

- a. **The Discretionary Process:** The Discretionary Process is intended for particularly creative proposals that might not comply with all or some of the Standards in Section 7.0603. The aim is to encourage applicants to propose exciting, innovative designs, while still ensuring the City's design concerns and objectives for commercial development are met. In this case, applicants shall meet the Design Guidelines. The Design Commission may waive a Guideline or Guidelines to achieve the flexibility necessary to support a particularly creative proposal. Approval requires that the applicant demonstrate to the Design Commission that the waiver from the Guideline(s) would result in a development that better meets the applicable commercial Design Principles and the intent statement preceding the Guidelines.
- b. **The Clear and Objective Process:** The Clear and Objective Process includes measurable Standards to meet the desired urban form. In the Clear and Objective Process the applicant must meet all development Standards. A decision on approval will come from the Manager or Design Commission, depending on the scale of the proposed development. Deviation from any of the Standards or referenced Standards in Article 7 will require the applicant to follow the Discretionary Process.

---

## 2. **Layout:**

The commercial design regulations are divided into three primary categories:

### a. **Site Design.**

Site Design Guidelines and Standards address the organization and arrangement of a development's components. They focus on the location and orientation of buildings, parking, service areas, landscaping and site features such as open space. Good site planning is of critical importance to the design of commercial development. Excellent site design can improve the aesthetics of a community, minimize a project's impacts on its neighbors, improve the quality of the streetscape, relate to or establish desirable development patterns, promote sustainability and improve neighborhood connectivity.

### b. **Building Design.**

Building Design Guidelines and Standards address the massing and exterior architectural elements of buildings, including components that define the scale, quality and character of a building, such as roofs, entries, windows, materials and details. Excellent building design enhances the quality of life for residents by improving the appearance of the City, by establishing a sense of community, and by improving the long-term economic value of the properties.

### c. **Sustainable Design.**

Sustainable Design Guidelines and Standards address regulations to minimize negative environmental impacts from development by utilizing sustainable building techniques which reduce stormwater runoff, heat island effects and pollution associated with energy usage and transportation.

## 3. **Images:**

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## 4. **Code Compliance:**

Developments shall comply with other Code sections including but not limited to Articles:

- 4: Land Use and Plan Districts;
- 5: Overlay Districts;
- 7: Design Review;
- 9: Common Requirements; and
- 11: Procedures.

## 5. **Exemptions:**

Commercial development which follows this Code is exempted from the following sections:

- a. 7.0202: Community Service, Commercial, Industrial and Mixed Use Developments (Commercial Component);
- b. 7.0210: Transit Design Criteria and Standards;
- c. 9.0823: Landscaping of Parking Lots; and
- d. 9.0824: Pedestrian Circulation/Walkways.



# 7.0602 Approval Criteria and Standards

## Commercial Design Principles

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### Site Design Principles

#### A. Accessibility:

Large commercial developments shall be sited and designed to accommodate multiple modes of transportation including transit, bicycles and pedestrians as well as the automobile while facilitating comfortable pedestrian movement to and throughout the site and reducing automobile dependence and dominance.

#### B. Activity:

Sites shall be developed in a manner that fosters activity throughout the day.

#### C. Building and Site Orientation:

Buildings shall be oriented toward and placed in close proximity to the street, defining the public space while creating a consistent and appropriate street edge.

#### D. Parking:

Surface parking shall be configured in a manner that minimizes its visual and environmental impacts on the site and on surrounding properties. Parking shall not dominate the site and the quantity of parking shall relate to actual usage and the needs of the patrons.

#### E. Public Spaces:

Site plans shall incorporate outdoor public spaces that encourage activity, gathering and enjoyment. These spaces shall foster desirability, place identity and enhance the overall aesthetic of the site.

#### F. Landscaping:

Landscaping shall be utilized to create attractive commercial environments by enhancing building and site appearance, defining pedestrian spaces, walkways and streets, breaking down the scale of parking areas, and screening service and loading areas.

#### G. Sustainability:

Commercial development shall utilize sustainable development practices. Development shall address issues such as stormwater, energy conservation and water usage to promote efficient use of land and resources, minimize environmental impacts and protect natural resources.

#### H. Safe Design:

Site and building design shall integrate Crime Prevention Through Environmental Design (CPTED) strategies as appropriate to enhance the safety, security and comfort of customers, employees and neighboring residents.

#### I. Impact Mitigation:

Sites shall be designed to mitigate or minimize impacts on surrounding properties and public spaces.

---

## **Building Design Principles**

### **J. Building Form and Articulation:**

Design strategies that break down the scale of large commercial buildings into smaller human-scale masses shall be utilized, creating visual interest and eliminating blank facades.

### **K. Building Activity and Glazing:**

Glazing shall be incorporated at pedestrian levels to allow views into active interior spaces and to provide a visual connection between the interior and exterior.

### **L. Prominence and Hierarchy:**

The building shall be designed in a manner that enriches and gives design prominence to critical locations.

### **M. High Quality Materials:**

Buildings shall utilize a combination of complimentary, high-quality materials that are attractive, durable and context appropriate.

### **N. Sustainable Architectural Design:**

Architectural design and practices shall be incorporated which conserve energy and resources as well as minimize life cycle costs. Large commercial buildings shall be designed to accommodate future tenants or uses in the event of vacancy.

## 1. Neighborhood Connectivity and Block Structure

A. **Intent:** To design sites in a manner that creates connections to surrounding properties and areas, reducing the distance required to access the site while encouraging walking and alternate modes of transportation. Block structures shall be used to break down the scale of the site, creating a pedestrian-scaled environment which allows for improved infill development and redevelopment potential.

B. **Applicable Commercial Design Principles from Section 7.0602:**

- A. Accessibility
- C. Building and Site Orientation
- G. Sustainability

C. **Design Guidelines:**

1. Future Street Plan Conformance and Preparation. The standards of **Section 7.0603(A)(1)(D)(1)** are required.
2. Mitigate traffic impacts. The standards of **Section 7.0603(A)(1)(D)(2)** are required.
3. Public Improvements. The standards of **Section 7.0603(A)(1)(D)(3)** are required.
4. Connections shall be provided to adjacent commercial properties to enhance pedestrian accessibility and limit unnecessary vehicular traffic on public streets.
5. Safe and Efficient Circulation: The standards of **Section 7.0603(A)(1)(D)(5)** are required.
6. Identification System: The standards of **Section 7.0603(A)(1)(D)(6)** are required.
7. Buildings shall foster a walkable development and not exceed dimensions that limit connections to surrounding areas. Spaces between buildings shall be of sufficient dimension to allow for connectivity and the creation of an attractive and walkable environment.
8. Development sites shall be integrated into the surrounding commercial areas and neighborhoods and provide appropriate vehicular and pedestrian connections to these areas.



Fig. A.1.C.9: Gresham Station utilizes public streets to break down the scale of the large parking area, adding pedestrian connections and creating a more walkable environment.



Fig. A.1.C.9(2): Edgewood Retail District, Atlanta, GA, has several connections to the surrounding neighborhood, improving pedestrian accessibility.

Fig. A.1.C.9(3): Cascade Station, Portland, OR, utilizes primary internal drives on a regular grid to divide the parking area and provide opportunities for landscaping.



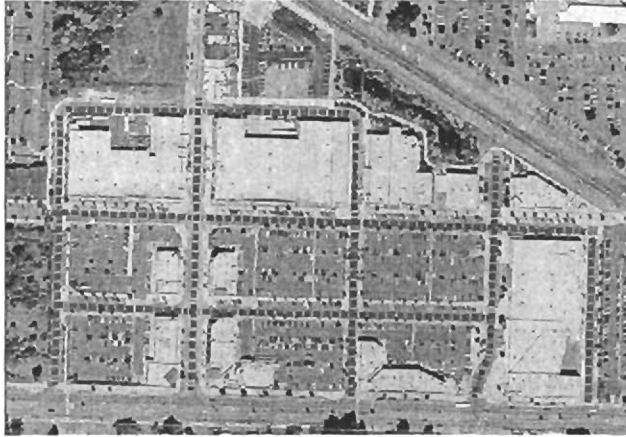


Fig. A.1.C.9: Gresham Station

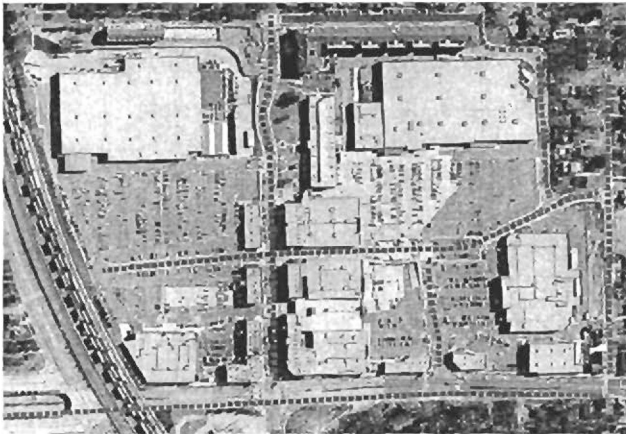


Fig. A.1.C.9(2): Edgewood Retail District, Atlanta, GA.

Fig. A.1.C.9(3): Cascade Station, Portland, OR.



## 1. Neighborhood Connectivity and Block Structure, continued

9. All developments which require large surface parking areas shall be designed with and divided by a grid of internal public streets or primary internal drives at the discretion of the Manager or Design Commission.
  - a. The grid shall:
    1. Break down the scale of the site;
    2. Provide definition to parking and development areas;
    3. Provide safe and convenient circulation routes for vehicles, pedestrians and bicycles;
    4. Allow for additional future development or redevelopment of the site; and
    5. Respond to the logical shape, orientation and topography of the site.
  - b. Where appropriate, public streets and pedestrian connections shall be made to adjacent streets and areas to improve connectivity, accessibility and walkability of the site.
  - c. Features shall be present at the site's entry to create a sense of arrival when entering a large commercial development.

### D. Design Standards

1. All new developments shall be designed in a manner that is consistent with and responds to the City's adopted Future Street Plans. Future street plans may be required at the discretion of the Manager or Design Commission, and shall be prepared and approved as required by **Section 9.0710**.
2. In designing accesses for the development, efforts shall be made to mitigate adverse traffic impacts on adjacent, low-density residential districts including LDR-5, LDR-7, TR and TLDR. In assessing such impacts, the typical daily street volumes shall be considered as specified in **Section A5.501**.
3. **Traffic Impacts and Transit Facilities.** Based on the anticipated vehicular and pedestrian traffic generation, and the policies of the Community Development Plan, adequate right-of-way and improvements to abutting streets shall be provided by the applicant and shall meet

## 1. Neighborhood Connectivity and Block Structure, continued

the street standards of the City. Required right-of-way improvements may include but not be limited to installation of lighting, signalization, turn lanes, paving, curbs, sidewalks, street signs, bikeways and other facilities needed because of anticipated vehicular and pedestrian traffic generation (refer to **Sections A5.400 and A5.500**).

4. Vehicular and pedestrian connections shall be designed to be provided to existing or future parking areas on adjacent properties as required by **Section 9.0822(A)(8)**. Cross access easements shall be required and shall take effect when adjacent properties are developed to this standard.
5. The circulation pattern shall be safe and efficient within the boundaries of the site. Consideration shall include the layout of the site with respect to the location, number, design and dimensions of vehicular and pedestrian access points, exits, drives, walkways, bikeways, emergency equipment ways and other related facilities.
6. An identification system, as approved by the Manager or Design Commission and in compliance with appropriate standards specified in **Appendix 6.000**, shall be designed and posted in such a manner as to allow the quick location of on-site buildings and entries.
7. In order to facilitate connections to surrounding areas, a contiguous building shall not exceed four hundred (400) feet in any dimension. A minimum separation of twenty (20) feet shall be required between buildings when the sum of the length of two (2) or more adjacent building equals or exceeds four hundred (400) feet. When the buildings are not separated by an internal public street, primary internal drive or a parking area, the separation area shall include landscaping. A pedestrian walkway shall be required at the discretion of the Manager or Design Commission.
8. Connections to surrounding properties and neighborhoods shall be established and include streets and/or pedestrian paths as determined by the Manager or Design Commission. Pedestrian paths shall connect to surrounding areas at spacing no greater than four hundred (400) feet where feasible as determined by the Manager or Design Commission.



Fig. A.1.D.4: Many vehicular and pedestrian connections are preset between adjacent commercial uses and sites.

# 1. Neighborhood Connectivity and Block Structure, continued

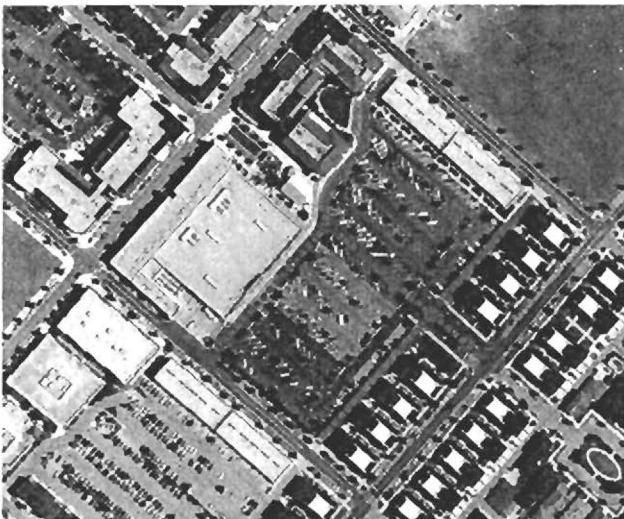
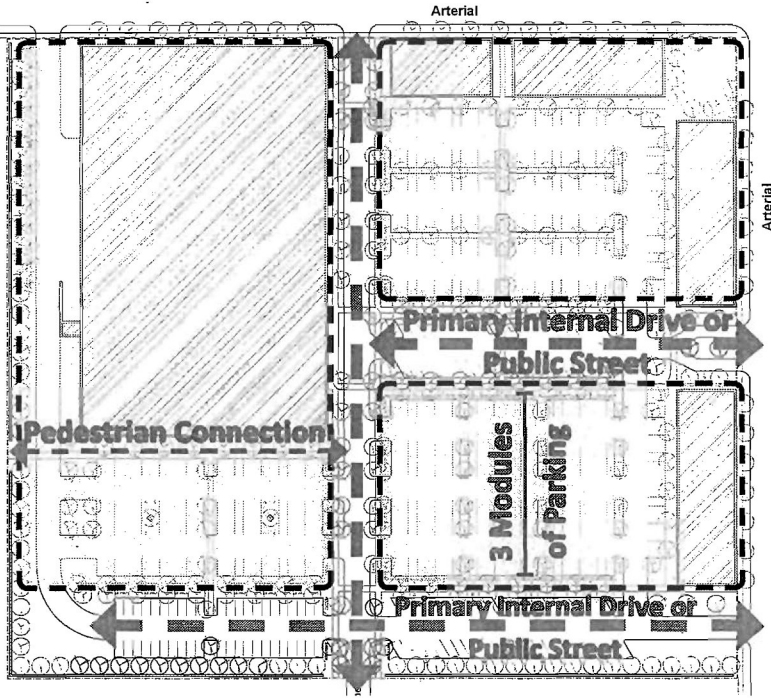


Fig. A.1.D.8: Many connections to surrounding properties are established.

9. Developments which require more than one hundred (100) off-street surface parking stalls shall utilize a system of internal public streets or primary internal drives that divides the site into rectangular or square blocks. Alternative block shapes may be approved at the discretion of the Manager or Design Commission to accommodate irregular site configurations.
  - a. A public street or primary internal drive shall occur at spacing no greater than every three (3) parking modules. A major landscape division may be allowed as a substitute feature for a public street or primary internal drive and may be used at spacing no less than every six (6) parking modules.
  - b. Access to sites from arterials or other existing public streets shall be either by an internal public street or a primary internal drive. Deviation from this standard to accommodate drives which primarily act as service routes or other cases may be approved by the Manager or Design Commission.
  - c. In addition to having primary access by primary internal drives or public streets, development sites shall have a minimum of one (1) primary vehicular and pedestrian entry which utilizes a minimum of one (1) gateway feature to establish prominence. Gateway features may include:
    1. A landscaped median;
    2. Decorative masonry piers;
    3. Public art pieces;
    4. Raised planters with seating walls;
    5. Pedestrian-oriented decorative way finding signage; and/or
    6. Other features approved by the Manager or Design Commission.

Fig. A.1.D.9: Potential site configuration utilizing public streets and primary internal drives.



## 2. Internal Circulation: Public Streets and Primary Internal Drives

- A. **Intent:** To create internal drives which accommodate pedestrian and vehicular access needs while providing amenities to improve the appearance of the development.
- B. **Applicable Commercial Design Principles from Section 7.0602:**
- A. Accessibility
  - B. Activity
  - D. Parking
  - F. Landscaping
  - G. Sustainability
  - H. Safe Design
- C. **Design Guidelines:**
1. **Public Streets.** The standards of **Section 7.0603(A)(2)(D)(1)** are required.
  2. **Primary Internal Drives** shall replicate the character of a public street, creating a pedestrian-oriented environment and accommodating pedestrians, vehicular traffic and parking. Primary internal drives shall incorporate amenities appropriate for commercial streets, including street trees, landscaping, pedestrian-level lighting and other features.
    - a. **Shade (Canopy) Trees.** The spacing and species standards of **Section 7.0606(A)(2)(D)(2)(d)** are required. Landscaped areas on primary internal drives shall include paving and soils which encourage the growth of large and healthy trees.
    - b. **Lighting.** Pedestrian-scale lighting shall be provided.
    - c. When present, parking on primary internal drives shall replicate configurations of on-street parking.
    - d. Amenity zone treatments shall respond to surrounding conditions.
    - e. Crosswalks on primary internal drives shall utilize bump outs and other features to ensure pedestrian safety by reducing crossing distances. Bump outs may also provide opportunities for landscaping and stormwater treatment and infiltration.
    - f. Primary internal drives may incorporate strategies to manage stormwater on site.



Fig. A.2.C.2: Landscaping, street trees and pedestrian-scaled lighting enhance the pedestrian areas on internal streets and primary internal drives.

## 2. Internal Circulation: Public Streets and Primary Internal Drives, continued



Fig. A.2.D.1: An internal public street with design elements which enhance the pedestrian experience.

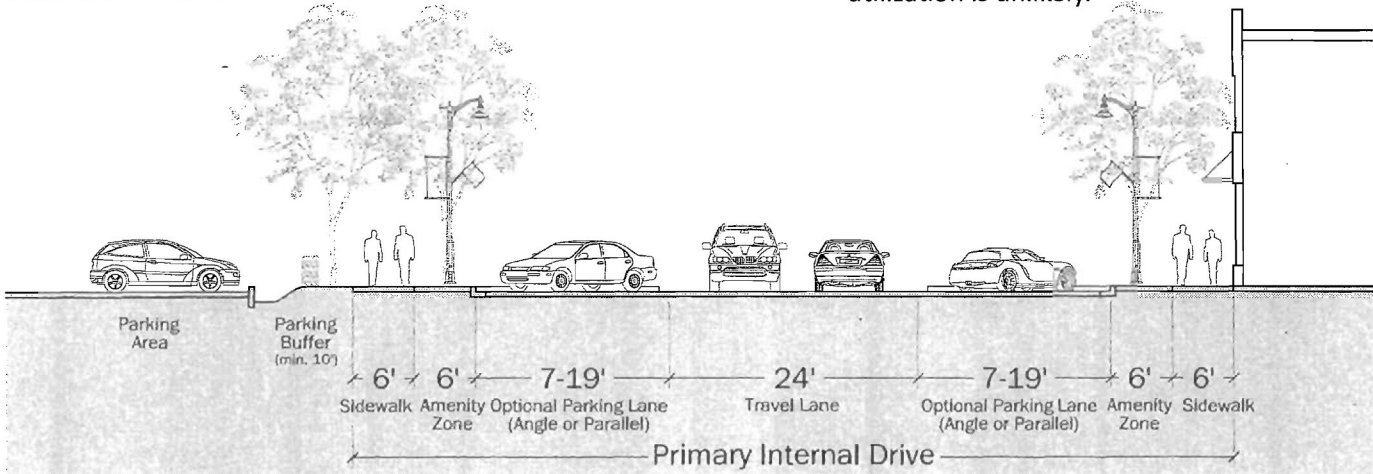
3. Major landscape divisions, when used as a substitute for public streets or primary internal drives, shall be of sufficient dimension and design to visually divide the parking area.

### D. Design Standards:

1. Public streets shall be designed to Public Works Standards.
2. When sites are required to utilize primary internal drives by **Section 7.0603(A)(1)(D)**, the following standards shall apply:  
 Primary internal drives shall consist of: a twenty-four (24) foot two-way drive lane; a sidewalk no less than six (6) feet in width and an amenity zone no less than six (6) feet in width shall be provided on each side of the drive.

- a. Drive lanes may be required to be a minimum width of twenty six (26) feet to accommodate emergency vehicles as determined by the Manager or Design Commission.
- b. Where primary internal drives are not adjacent to buildings, the amenity zone and sidewalk may each be reduced to five (5) feet in width if space limitations prohibit wider dimensions as determined by the Manager or Design Commission.
- c. A sidewalk on a primary internal drive which abuts a side or rear property line and does not abut a public right of way may be eliminated at the discretion of the Manager or Design Commission if pedestrian utilization is unlikely.

Fig. A.2.D.2: Section of Primary Internal Drive. Optional elements labeled as such.





## 2. Internal Circulation: Public Streets and Primary Internal Drives, continued

- d. Shade (canopy) trees on primary internal drives shall be planted in the amenity zone at an average tree spacing of thirty (30) feet. The amenity zone shall allow infiltration within six (6) feet of trees, through techniques such as permeable paving, tree grates or landscaped areas. Structural soil, Silva Cells or root channels shall be provided in the infiltration area of the amenity zone when paved. All trees planted on the primary internal drives shall be selected from the City of Gresham Approved Street Trees list.
- e. Lighting along primary internal drives shall be provided and not exceed twenty five (25) feet in height. Illumination levels are specified in **Section 7.0603(A)(9)(D)**.
- f. Primary internal drives may include a landscaped median, bicycle lanes and other features at the discretion of the Manager or Design Commission.
- g. Primary internal drives may include angled or parallel parking stalls. Angled parking stalls shall extend no more than nineteen (19) feet from the curb and parallel stalls shall extend no more than seven (7) feet from the curb. Parking on primary internal drives shall be at locations approved by the Manager or Design Commission. Reverse angled parking (back in angled parking) may be permitted in conjunction with a boulevard or median at the discretion of the Manager or Design Commission.

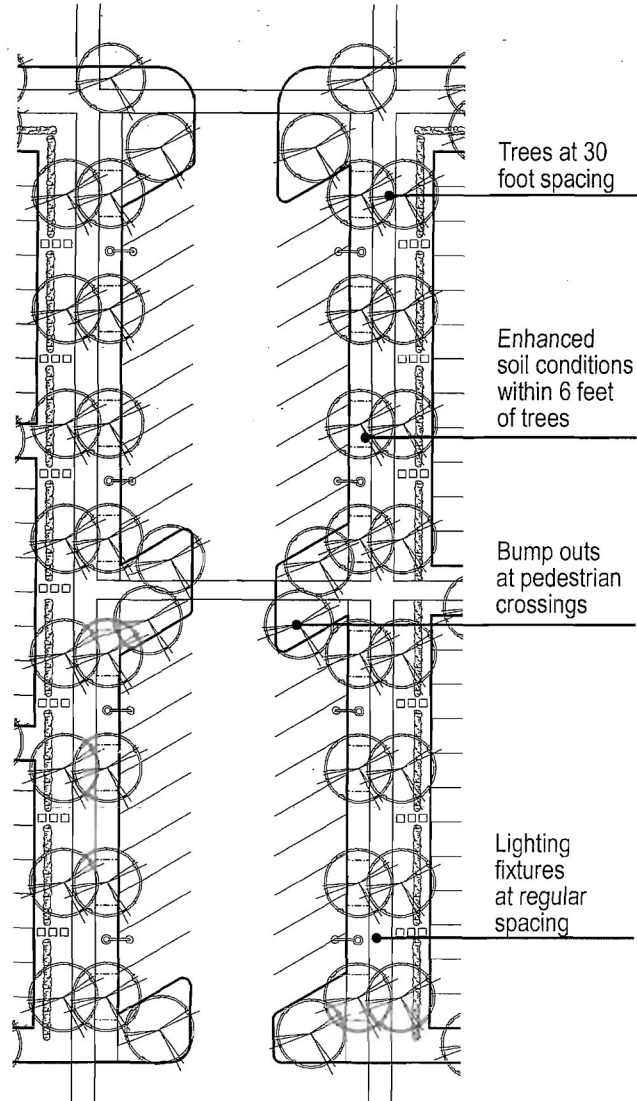
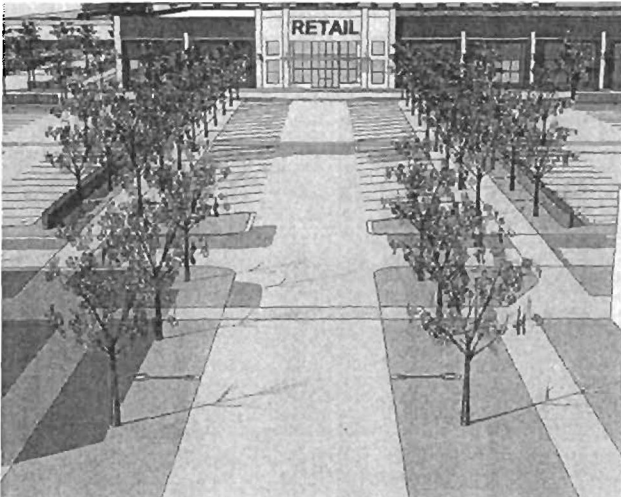


Fig. A.2.D.2(2): Plan of Primary Internal Drive showing optional angle parking.

Fig. A.2.D.2(3): Elevated view looking down a primary internal drive (optional angle parking shown).



## 2. Internal Circulation: Public Streets and Primary Internal Drives, continued



Fig. A.2.D.2.H.1: Amenity zone featuring a stormwater infiltration area and paved connections between parallel parking stalls and the sidewalk.

h. When primary internal drives include parking:

1. The amenity-zone shall allow frequent connections between parking stalls and the sidewalk. At least one (1) paved connection shall be provided between street trees. Paving shall match or accent the sidewalk, using concrete, paving which allows stormwater infiltration or pavers as approved by Manager or Design Commission.
2. Bump outs which extend the depth of the parking lane shall be provided to lessen crossing distances where pedestrian routes cross drive primary internal drives, such as at intersections and at mid-block crossings. Landscaped areas that do not interfere with clear vision requirements and stormwater infiltration areas may be included in areas of the bump outs not required for pedestrian use.

i. Where no parking abuts the amenity zone, the amenity zone may be landscaped instead of paved.

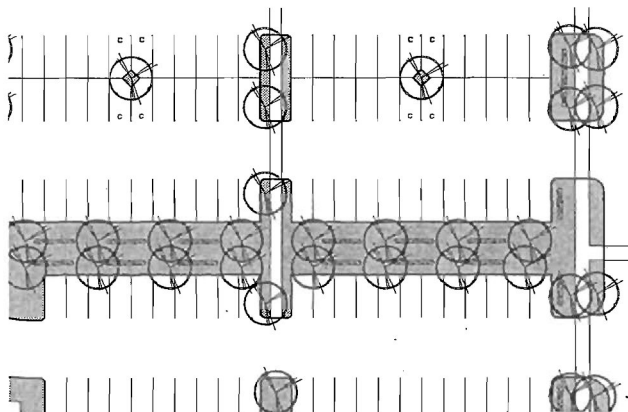
j. Crosswalks shall be provided on primary internal drives where pedestrian circulation routes cross vehicular ways. Crosswalks shall receive enhanced paving that creates a visual distinction between the crosswalk and the surrounding surfaces, such as scored concrete, colored and stamped concrete, brick, stone or concrete pavers or other high-quality material as approved by the Manager or Design Commission. Striping shall not be an acceptable method of creating visual distinction.

3. When used as a substitute for a primary drive aisle as provided for in Block Structure, major landscape divisions shall consist of a twenty four (24) foot landscaped area planted with trees, shrubs and ground cover.

a. This area may be designed to allow for stormwater infiltration.

b. A pedestrian walking path may be integrated into this area and, at the discretion of the Manager or Design Commission, may be required if it improves circulation within the site.

Fig. A.2.D.3: Major landscape division used as a substitute for a primary internal drive



### 3. Building Placement and Frontage Requirements

A. **Intent:** To ensure buildings are placed in appropriate locations, thereby defining the street, creating a comfortable pedestrian environment and minimizing the visual impact of parking from primary streets.

B. **Applicable Commercial Design Principles from Section 7.0602:**

- A. Accessibility
- B. Activity
- C. Building and Site Orientation
- D. Parking
- E. Public Spaces

C. **Design Guidelines:**

1. In order to create a consistent and cohesive building edge which defines the public space and creates an inviting and accessible pedestrian environment, buildings shall be placed close to the street and shall occupy sufficient street frontage to create a pedestrian friendly environment. Buildings not located along existing frontages shall be placed close to and concentrated along primary internal drives.
2. Buildings or public spaces shall be placed at intersections of public streets in order to create inviting pedestrian environments.
3. Parking shall be located to the side, interior or rear of the site and shall not dominate the public street frontages.



Fig. A.3.C.1.A: Buildings at the street edge enhance pedestrian spaces.



Fig. A.3.C.1.B: Frontage at the intersection occupied by a building.

Fig. A.3.C.1.C: Parking develop along the side of a building and not dominating the street.



### 3. Building Placement and Frontage Requirements, continued

Fig. A.3.D.1: Diagram illustrating building frontage standards

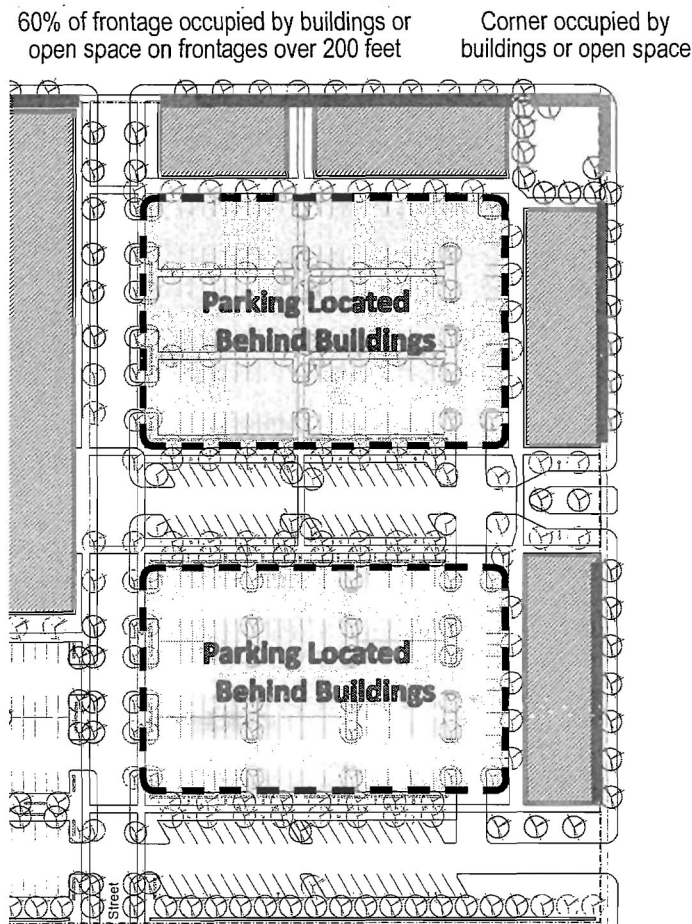


Fig. A.3.D.1.B: Open spaces located at the street may count toward frontage requirements.



#### D. Design Standards:

1. Frontage on existing public streets shall be occupied by building facades as follows:
  - a. Frontages two hundred (200) feet or less shall be occupied by building facades for a minimum of forty percent (40%) of the frontage length, as measured by the length of buildings present between the minimum and maximum setback (setback zone).
  - b. Frontages greater than two hundred (200) feet shall be occupied by building facades for a minimum of sixty percent (60%) of the frontage length, as measured by the length of buildings present within the setback zone.
2. Buildings and/or publicly accessible open spaces shall occupy one hundred percent (100%) of the street frontage at existing street intersections for a minimum of forty (40) feet as measured by the length present within the setback zone. If a publicly accessible open space is present at an intersection, its internal site edges shall be lined by buildings for no less than eighty percent (80%) of their length; however if a connection through the open space is required, a ten (10) foot walkway shall be allowed if otherwise prevented by the frontage requirement, at the discretion of the Manager or Design Commission.
3. Parking shall be located on the side, interior or rear of the site and shall not be present along existing public street frontages for more than thirty percent (30%) the street frontage.
4. At the discretion of the Manager or Design Commission, Auto-Dependent Uses may utilize alternative features in conjunction with the primary structure to count toward the building frontage requirement. Canopies located within the setback zone which are associated with these uses may count toward the frontage requirement if used with a decorative masonry wall thirty six (36) inches in height. Other alternative features may include pergolas or arcades and shall be reviewed by the Manager or Design Commission.

### 3. Building Placement and Frontage Requirements, continued

5. Internal public streets created as part of the development do not have a building frontage requirement for the initial development. Subsequent building development including redevelopment and intensification of the site shall comply with building frontage requirements on internal public streets.
6. Space attributable to newly created public streets and primary internal drives which are required as part of the development shall not count toward the total street frontage length.



Fig. A.3.D.1.D: Alternative features, such as a gas station canopy, may count toward the frontage requirement.

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## 4. Building Orientation and Entries

A. **Intent:** Buildings are oriented appropriately to enhance pedestrian accessibility and place the most visually interesting facade in public view.

B. **Applicable Commercial Design Principles from Section 7.0602:**

- A. Accessibility
- B. Activity
- C. Building and Site Orientation
- H. Safe Design

C. **Design Guidelines:**

1. All buildings shall be oriented toward and accessed from the street. If a building has frontage on more than one street, it shall be oriented and designed to provide reasonable pedestrian access along the most active street frontages.
2. Buildings, if located on a corner, shall orient an entry to the intersection of public streets.
3. Buildings with long street frontages or multiple street-facing tenant spaces shall provide additional entries to ensure reasonable pedestrian access and improve the appearance of the building and the public realm.
4. Buildings located at the rear of the site shall provide entries that correspond with pedestrian circulation routes, such as the intersection of public streets, primary internal drives and other pedestrian connections.
5. Mixed-use buildings shall have distinct entries for each use.
6. Entries shall be operable unless they create an undue hardship on the business, such as a lack of on-street parking serving the business, lack of storage or service space in the building or unresolvable security issues.

**Additional guidelines applicable to buildings greater than thirty thousand (30,000) square feet:**

7. When a building faces multiple streets, entries shall be provided on multiple frontages to enhance the accessibility and walkability of the development.



Fig. A.4.C.1: A commercial building with an entry facing the primary street on which it's located.

Fig. A.4.C.3: Frequent customer entries create attractive buildings and pedestrian spaces and increase accessibility.



## 4. Building Orientation and Entries, continued

### D. Design Standards:

1. Each building shall provide at least one entry facing the primary street on which the building is located. The primary street shall be the street of highest classification or as determined by the Manager or Design Commission.
2. If a building is located at the intersection of two (2) streets classified as a principal arterial, arterial, boulevard or Transit Street, an entry shall be located on the building corner facing the intersection.
3. Additional entries on a single building facade shall be required as follows:
  - a. When a building facade faces a public street or a primary internal drive and its length exceeds three hundred (300) feet, the building shall provide at least two (2) operable entries on that street-facing facade.
  - b. When a multi-tenant building facade exceeds one hundred twenty (120) feet in length with multiple tenant spaces located at the street edge, a minimum of two (2) entries shall be provided on that facade. At the discretion of the Manager or Design Commission, a covered breezeway through the building may count as one (1) entry for the purposes of this requirement.
4. Buildings located at the rear of the site along public streets or primary internal drives shall provide operational entries as noted in Standards (1), (2) and (3) above. Entries shall provide connections to pedestrian circulation routes.
5. When part of a mixed-use building, residential and other non-retail commercial uses shall have a distinct entry that is not shared with a retail use. A shared lobby space may serve multiple users of the same type. Required residential and non-retail entries shall be on and face a public street and may count toward the building entry requirement.
6. All entries to commercial spaces required by this Code shall be open to the public during all business hours.



Fig. A.4.D.2: A corner entry which provides access from and addresses both street frontages.



Fig. A.4.D.3.A: A large commercial building with multiple street facing entrances.

Fig. A.4.D.3.B: Multi-tenant building at the street edge with multiple street facing entries.





## 4. Building Orientation and Entries, continued

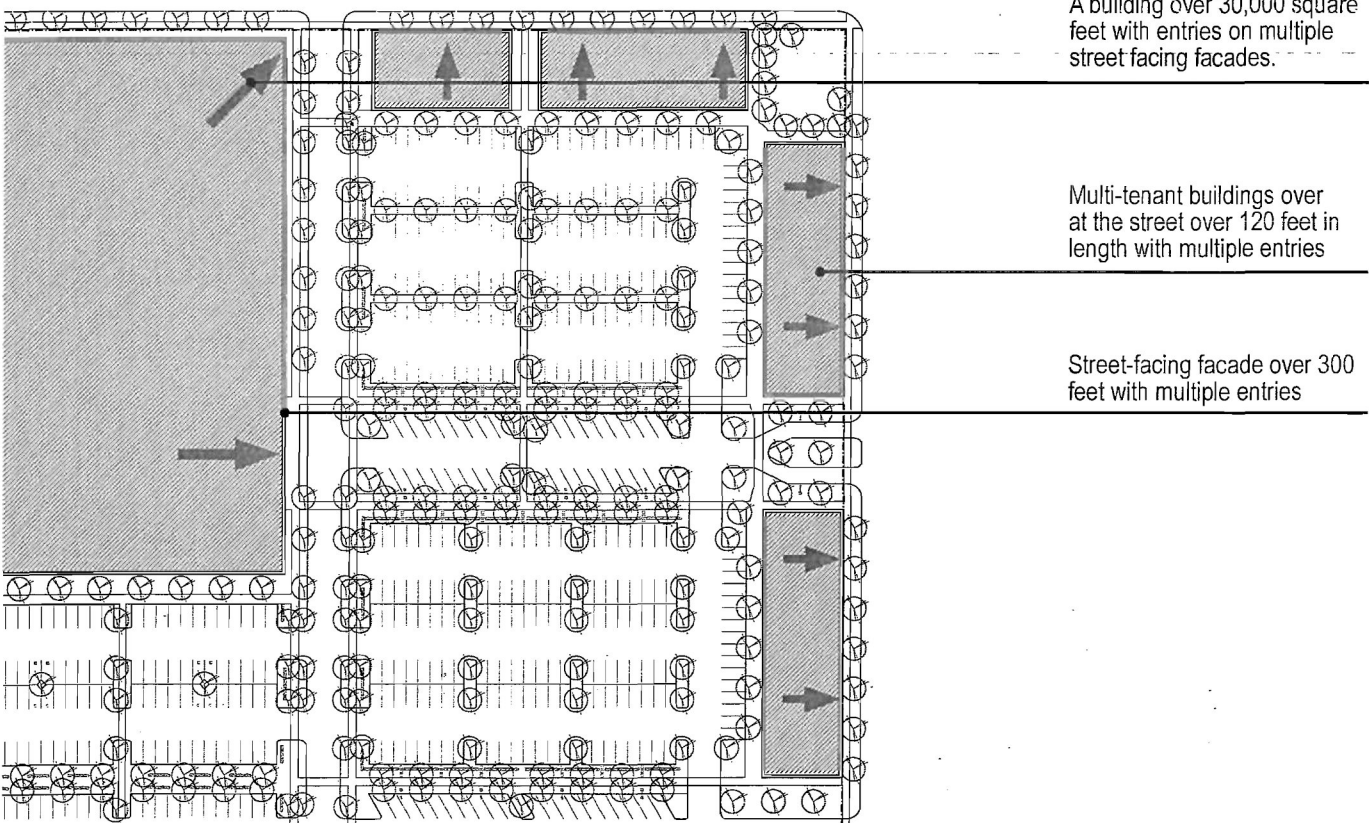
**Additional standards applicable to buildings greater than thirty thousand (30,000) square feet:**

7. Each building shall provide at least one (1) entry on each street frontage it faces except as follows:
  - a. If the building has three (3) street frontages, the building shall have a minimum of two (2) frontages with operational entries; and
  - b. If the building has four (4) or more street frontages, the building shall have a minimum of three (3) frontages with operational entries.
  - c. Corner entries facing two (2) street frontages shall count as an entry on each frontage.



Fig. A.4.D.7: A large commercial building utilizing a corner entry to provide access from both streets.

Fig A.4.D: Site diagram illustrating potential entry configuration of multiple buildings on a site.



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## 5. Publicly Accessible Open Space

A. **Intent:** Publicly accessible open spaces, which are developed as a focal point of large commercial development, encourage pedestrian activity in highly visible locations.

B. **Applicable Commercial Design Principles from Section 7.0602:**

- B. Activity
- E. Public Spaces
- F. Landscaping
- G. Sustainability

C. **Design Guidelines:**

1. Publicly accessible open spaces may count toward building frontage requirements and allow for greater building setbacks when they are developed for public use, located in highly visible areas and designed to encourage and concentrate activity. Publicly accessible open spaces shall be in close proximity to operable building entries and contain pedestrian connections through the space. Public spaces can be located between a building and the sidewalk, so long as active ground floor uses are accessible to and from the space and if the desired street wall and/or commercial continuity is not adversely impacted by the presence of this space.
2. Publicly accessible open space shall include a variety of public spaces, both hardscaped and landscaped, such as on-site plazas, interior courtyards, patios, terraces and gardens. Public spaces shall incorporate features that advance sustainable principles, such as use of gray water, solar collection for powering pumps or lighting, rain gardens, pervious paving, containers for recycling, and benches made from recycled materials. Public spaces shall include focal points such as art sculpture, water features, pavilions, seating areas, specimen plants, unique paving, or unusual lighting. When possible, these spaces shall take advantage of and preserve any natural features on the site and be designed to accentuate view corridors.
3. Publicly accessible open space size. The standard of **Section 7.0603(A)(5)(D)(3)** shall generally be met to create a functional, attractive pedestrian space. Unique design configurations may be acceptable if they meet the intent of the publicly accessible open space to encourage pedestrian activity.



Fig. A.5.C.1: An open space which provides connections between the building and the sidewalk.



Fig. A.5.C.2: An open space enhanced by landscaping and seating ledges.

Fig. A.5.C.2(2): Open spaces shall incorporate sustainable features such as rain gardens and LED lighting.



## 5. Publicly Accessible Open Space, continued

### D. Design Standards:

1. Publicly accessible open spaces may be utilized to create an increased setback and may count toward the required building frontage, up to twenty percent (20%) of the required building frontage length, as specified in 7.0603(A)(3)(D) when:
  - a. Placed next to or in front of a building setback no more than forty (40) feet from the right of way. When located between a building and a sidewalk, direct pedestrian access shall be provided to the abutting building.
  - b. The space contains vertical elements which screen parking and define the street edge.
  - c. A building entry faces and abuts the space or is located no greater than eighty (80) feet from the space, measured by walking distance, in order to promote activity in the publicly accessible open spaces.
2. Publicly accessible open spaces shall incorporate the following:
  - a. At least thirty percent (30%) of the area shall be planted with trees, shrubs, groundcover and perennial landscape plantings.
  - b. At least thirty percent (30%) of the area shall be hardscaped with decorative paving.
  - c. At least one bench or seating unit for each two hundred (200) square feet of area (seating may be grouped into benches or ledges).
  - d. Pedestrian-scaled lighting fixtures no taller than eighteen (18) feet.
  - e. One element with sustainability attributes (such as but not limited to rain gardens, solar powered lights or equipment, pervious paving or benches made from recycled materials).
  - f. Artistic design elements such as decorative paving patterns, ornamental art features, creative lighting elements, etc.
3. Publicly accessible open spaces shall have a minimum width of thirty (30) feet and a minimum depth of twenty (20) feet unless otherwise approved by the Manager or Design Commission due to site constraints.



Fig. A.5.D.1: Open spaces allow for increased building setbacks and may count toward the frontage requirements. The space features decorative paving and trees to enhance its appearance.



Fig. A.5.D.1.C: Entries facing onto open spaces encourage their utilization and activate the spaces.

Fig. A.5.D.2: A water feature utilized as a focal point of the space.



## 6. Parking

- A. **Intent:** Parking areas shall be designed to minimize and mitigate their negative visual and environmental impacts.
- B. **Applicable Commercial Design Principles from Section 7.0602:**
- C. Building and Site Orientation
  - D. Parking
  - G. Sustainability
  - I. Impact Mitigation
- C. **Design Guidelines:**
1. Parking shall be located and configured to minimize its visual impact from street frontages.
  2. Parking shall be set back from the street right-of-way and shall include a landscaped buffer to minimize its visual impact from the street and create a pedestrian-friendly street edge. Parking shall not be located at highly visible locations of a site, such as at a street corner.
  3. In order to minimize the size of off-street parking areas, provide convenient short-term parking in front of commercial developments and provide additional protection for pedestrians, the development of on-street parking on appropriate street types is encouraged. New on-street parking may count towards the minimum parking requirements as determined by the Manager or Design Commission.
  4. Parking areas shall provide appropriate separation, utilizing either landscaping or pedestrian walkways, from building walls.



Fig. A.6.C.1: Parking located on the side of a building and screened from the street to minimize its visual impact.



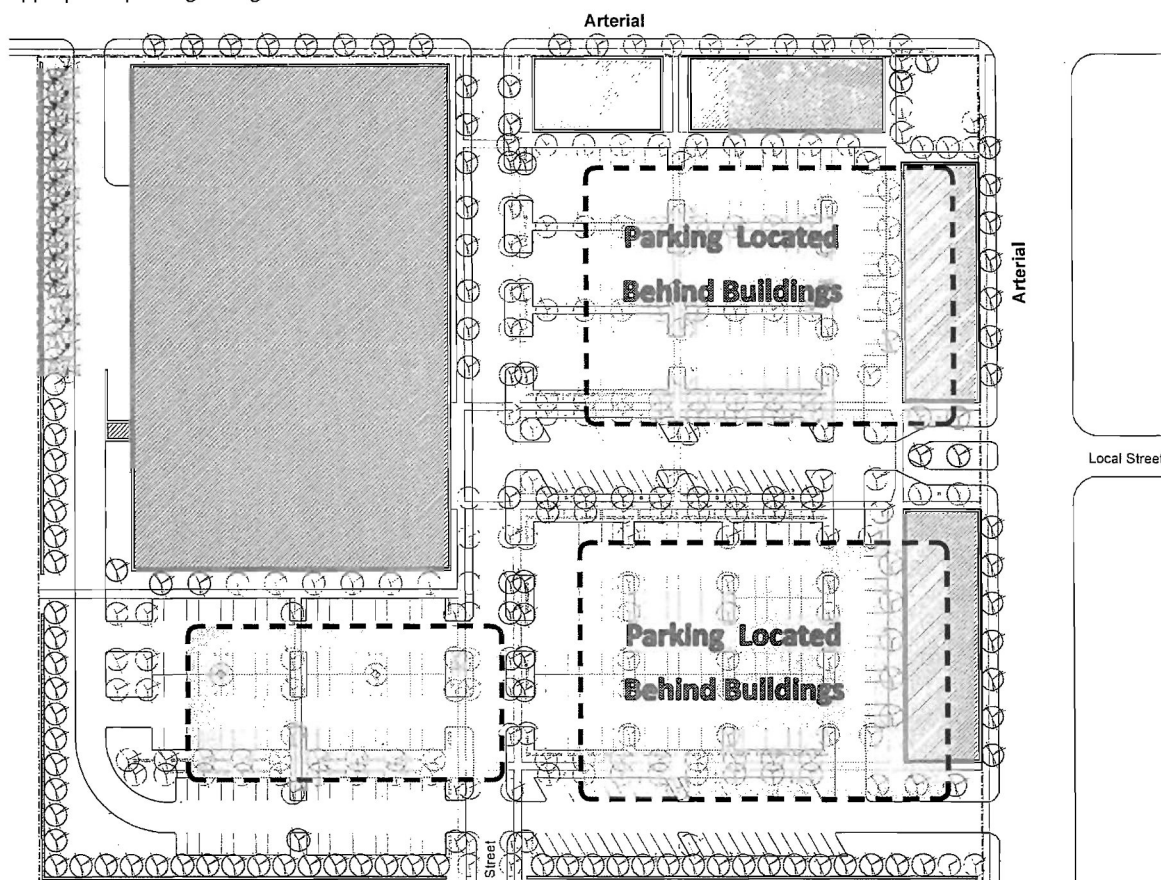
Fig. A.6.C.2: Parking set back from the street and buffered by landscaping.

## 6. Parking, continued

D. **Design Standards:** In addition to regulations found in **Section 9.0800** (except 9.0823 and 9.0824), the following standards apply:

1. Parking shall be located behind or on the side of buildings.
2. Parking shall not be located closer to the street right of way than ten (10) feet or the adjacent building facade setback, whichever is greater. The space between the parking and the street right-of-way shall be landscaped per **Section 7.0603(A)(7)(D)(3)**.
3. Parking along new public streets (on-street parking) created as part of the development shall be exempt from the provisions of (2) and above and may count toward the minimum parking requirement at the discretion of the Manager or Design Commission.
4. Parking areas shall be separated from exterior walls of a structure by a minimum five (5) foot landscaped planting bed or pedestrian walkway.

Fig A.6.D.1: Potential site design showing appropriate parking configurations.



## 7. Landscaping

- A. **Intent:** To utilize landscape features to improve the appearance of commercial sites, provide opportunities for stormwater management and minimize heat islands.
- B. **Applicable Commercial Design Principles from Section 7.0602:**
- D. Parking
  - F. Landscaping
  - G. Sustainability
  - H. Safe Design
  - I. Impact Mitigation
- C. **Design Guidelines:**
1. **Licensed Design Professional.** The landscape plan shall be created by a licensed design professional such as a Landscape Architect, Architect or Civil Engineer. The landscape plan shall exhibit the following characteristics:
    - a. The overall design of the site and the design of the proposed landscape materials shall achieve unique, attractive and significant landscaping on the site as a whole;
    - b. The proper type, spacing, height, placement and location of plant materials shall be provided to ensure that the intent of this ordinance is met;
    - c. The choice and selection of plant materials shall insure that root systems will not interfere with public utilities and so that fruit and other debris, except leaves, will not constitute a nuisance within public rights-of-way or to abutting property owners;
    - d. The choice and selection of plant materials shall ensure that the type of plantings selected will be of a type that will survive and thrive in the area in which they are to be located; and
    - e. The proper relationship between deciduous and evergreen plant materials shall exist so as to ensure that the desired buffering effect will be accomplished.
  2. **Site Landscaping.** The standard of **Section 7.0603(A)(7)(D)(2)** shall be met.



Fig. A.7.C.2: Landscaping throughout the site contributes to the aesthetic appeal of the development.



Fig. A.7.C.2(1): Entry landscaping improves the appearance of active pedestrian spaces.

Fig. A.7.C.2(3): Variety of plant material at different scales adds visual richness to landscaped areas.



## 7. Landscaping, continued



Fig. A.7.C.3: Landscaping in the parking area improves the appearance of these spaces and minimizes heat gain.



Fig. A.7.C.3.(2): Landscaping adjacent to parking stalls minimizes their visual impact.

Fig. A.7.C.3.D: Enhanced landscaping at the entry to the parking area.



### 3. Parking Area Landscaping:

- a. Parking areas shall include landscaping of sufficient quantity and size in order to minimize the visual impact of the parking area, provide opportunities for stormwater management, and reduce the heat island effect of the area.
- b. Canopy Tree Quantity and Size. The standard of **Section 7.0603(A)(7)(D)(3)(b)** shall be met.
- c. Plant materials shall be protected from damage by vehicles.
- d. Additional landscaping shall be provided at the parking area entries, enhancing the appearance of highly visible areas.

### 4. Parking Area Perimeter Screen Landscaping: Parking areas shall be buffered from streets and primary internal drives with landscaping that provides definition to pedestrian areas and screens parking.

- a. The dimension of the parking lot landscape buffer shall be adequate to screen the parking and mitigate its visual impact from the street.
- b. Perimeter screening shall be layered to provide visual interest, definition of pedestrian areas and screening at various heights.
- c. Clear Vision. To ensure natural surveillance of the parking area, clear visibility shall be established between the heights of three (3) and six (6) feet. The standards of **Section 7.0603(A)(7)(D)(4)(c)** shall be met.
- d. Small breaks in the screening shall be provided to allow pedestrian access between the parking area and the sidewalk.

### 5. Internal Parking Landscaping

- a. Clusters of Stalls: The standards of **Section 7.0603(A)(7)(D)(5)(a)** shall be met.
- b. Within parking areas, landscaped rows or islands shall be present in parking areas at sizes which foster healthy tree growth and create opportunities for stormwater infiltration. Spacing of landscaped features shall be sufficient to visually divide the space and shade pavement.



## 7. Landscaping, continued

- c. Landscaped islands that terminate a row of parking shall provide definition to the parking area when abutting a large building internal to the site.
  - d. Landscaped islands and rows shall contain appropriate ground-level planting materials.
  - e. Landscaped Island Dimensions. The standards of **Section 7.0603(A)(7)(D)(5)(e)** shall be met.
6. **Buffing and Screening:** Site boundaries abutting less intensive uses shall be buffered in a manner that minimizes impacts on adjacent properties.
  7. **Landscape Maintenance:** The standards of **Section 7.0603(A)(7)(D)(7)** shall be met.

### D. Design Standards:

1. A professional licensed Landscape Architect shall complete and stamp the landscape plan for the development. Landscape plans for stormwater facilities may be designed by either a licensed Landscape Architect or a licensed Civil Engineer.
2. **Site landscaping:**
  - a. A minimum of 15% of the gross site area shall be landscaped in the Community Commercial and Moderate Commercial districts. A minimum of 20% of the gross site area shall be landscaped in the Corridor Mixed Use district.
  - b. Setback areas shall be landscaped except for those setback areas occupied by Publicly Accessible Open Spaces designed in accordance with **Section 7.0603(A)(5)(D)**.
  - c. Landscaping, including parking area landscaping and buffering, shall be planted at sizes no less than the following:
    1. Deciduous canopy trees shall be a minimum of two and one-half (2.5) inch caliper size and shall be balled and burlapped;
    2. Deciduous ornamental trees shall be a minimum of two (2.0) inch caliper size and shall be balled and burlapped;
    3. Evergreen trees shall be a minimum of six (6) feet in height and shall be balled and burlapped;

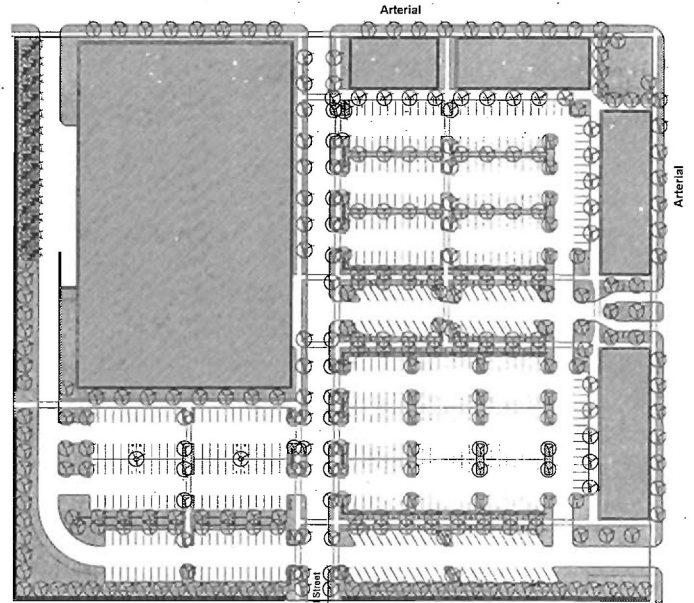


Fig. A.7.D.2: Diagram showing landscaped areas which count toward the site landscaping requirement.



Fig. A.7.D.2(1): Landscaping at the front of the buildings enhances the appearance of these areas.

Fig. A.7.D.2.C: Landscape material at sufficient sizes to make a significant visual impact.



## 7. Landscaping, continued

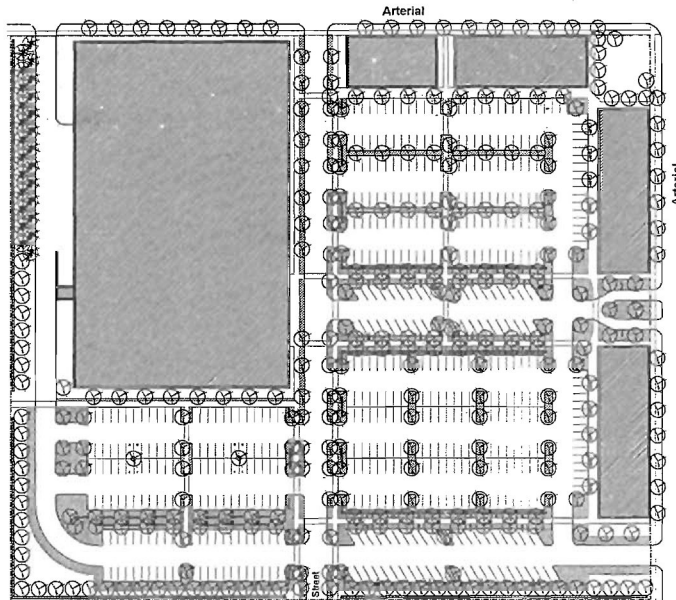


Fig. A.7.D.3: Diagram showing landscaped areas which count toward the parking area landscaping requirement.

Fig. A.7.D.3(1): Landscaping in a parking area which includes trees, shrubs and other ground-level plantings.



4. Evergreen and deciduous shrubs, with the exception of dwarf shrubs such as boxwood, must be a minimum of twenty-four (24) inches high from finished grade and a minimum of one (1) gallon size at planting;
  5. Perennials shall be a minimum of one (1) gallon size; and
  6. Ground covers shall be well rooted in either flats or a minimum of one (1) gallon pots.
3. Parking Area Landscaping: All Areas
    - a. The minimum percentage of parking area landscaping shall be fifteen percent (15%).
      1. Landscaped areas counting toward this requirement shall include parking area perimeter buffers, landscaped islands or rows, major landscape divisions, landscaping on internal public streets or primary internal drives and all other landscaped areas that are located within ten (10) feet of parking modules or stalls.
      2. A minimum of seventy percent (70%) of all landscaped area shall be covered with trees, shrubs and continuous ground cover (lawn, low evergreen shrubs or evergreen ground cover). Landscaped areas which include stormwater infiltration areas shall utilize appropriate plant materials.
    - b. A minimum of one (1) tree shall be planted for every six (6) parking stalls in the parking area. Required trees in the parking area shall be canopy trees capable of reaching thirty-five (35) feet in height and spread at maturity.
    - c. All parking area landscape shall be designed to ensure vehicles do not make contact with plant materials, utilizing overhang distances no less than two (2) feet when abutting shrubs or three (3) feet when abutting trees. Wheel stops may be used in place of overhang distances.
    - d. Parking area entries shall include a landscaped strip with trees at spacing no greater than thirty feet and shrubs.

## 7. Landscaping, continued

Amenity zone plantings on primary internal drives shall count toward this requirement, if present.

4. Parking Area Landscaping: Perimeter Screening
  - a. When located adjacent to a public street or primary internal drive, the parking area shall be buffered by a landscaped edge no less than ten (10) feet in width consisting of trees, shrubs, decorative fencing or walls and ground level plantings in a layered configuration (**Fig. 7.0603(A)(7)(D)(4)**). This buffer shall be located adjacent to the sidewalk of the public street or primary internal drive. Decorative fencing or walls may be included in place of required shrubs in the landscape buffer. When located adjacent to a primary internal drive or internal public street created as part of the development, the landscaped edge may be reduced to six (6) feet, if space limitations prohibit a wider buffer as determined by the Manager or Design Commission.
  - b. The landscaped edge shall consist of ground level planting beds adjacent to the sidewalk, trees planted at the thirty (30) foot spacing, and a continuous shrub or site-obscuring decorative fencing or garden wall. Walls and fences shall be built to a height of thirty-six (36) inches and shrubs shall be maintained at this height to allow surveillance of the parking area. An alternate parking lot perimeter screen landscaping may be approved by the Manager or Design Commission. This landscaping shall count towards the parking lot landscape requirements in **Section 7.0603(A)(7)(D)(3)**.
  - c. Landscaping shall allow clear vision into the parking area between the heights of three (3) and six (6) feet to allow for natural surveillance.
  - d. Breaks in the shrubs, fence or wall which allow for visual access shall occur every thirty (30) feet and shall not exceed three (3) feet in width. Where landscaped islands abut the parking perimeter screening, a break in the shrubs, fence or wall shall occur and provisions for pedestrian connections shall be provided, at the discretion of the Manager or Design Commission.

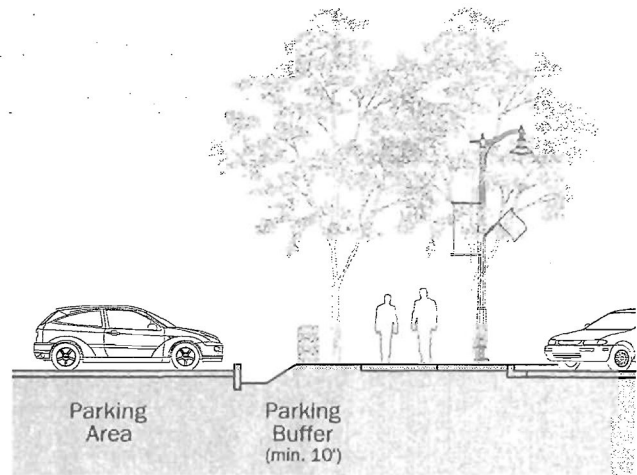


Fig. A.7.D.4: Potential design of the parking area perimeter screening landscaping, with ground level plantings, trees and shrubs. A bioswale (optional) is shown and may be included in the perimeter screening.

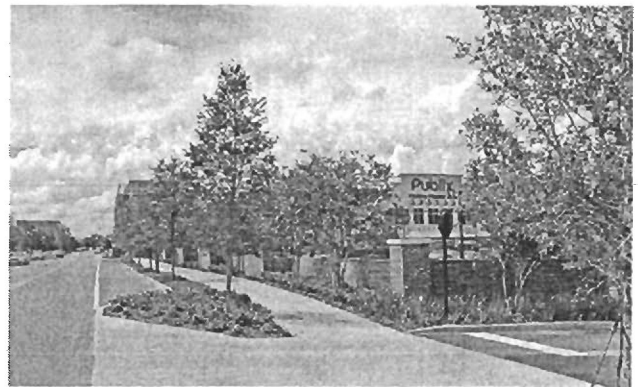


Fig. A.7.D.4(1): Perimeter screen using a masonry wall

Fig. A.7.D.4(2): Perimeter screen using shrubs and other plantings.



## 7. Landscaping, continued



Fig. A.7.D.5: Internal parking landscaping visually reduces the scale of the parking area and shades hardscapes to reduce heat islands.

Fig. A.7.D.5.B.1: Landscaped row visually divides the parking areas while potentially providing space for stormwater management.



### 5. Parking Area Landscaping: Internal

- a. Internal parking landscaping, including landscape rows and islands, shall divide the parking area into clusters of stalls not to exceed fifty (50) spaces.
- b. Developments shall utilize a series of landscaped islands and/or landscaped rows between parking modules.

1. Landscaped rows shall be present between parking modules and have a width not less than six (6) feet. Trees shall be planted on rows at spacing no greater than thirty (30) feet. Row-end islands shall be required with this option; or,

2. Internal landscaped islands shall be present within the parking area at the end of parking rows and locations along the length of the rows with an average spacing no greater than one (1) landscaped island every twelve (12) spaces. Additional islands may be required to meet the parking area landscape standards.

- a. Internal landscaped islands shall have a width of no less than nine (9) feet and have a minimum length equal to that of adjacent parking stalls less one (1) foot.

- b. Landscaped islands shall be planted with a minimum of one tree and may be designed to function as stormwater infiltration areas. Trees shall have a minimum clear trunk height of nine (9) feet at maturity.

- c. Minor landscaped islands may be placed between required landscaped islands to increase canopy coverage and landscaping within the parking area. Each minor landscaped islands shall include one planted tree and be no less than six feet (6) on each side and may be rotated forty-five (45) degrees to integrate with compact parking stalls.

## 7. Landscaping, continued

- c. When parking modules terminate into a drive aisle that does not contain parking and is adjacent to the front of a building over thirty thousand (30,000) square feet, the landscaped end islands at the terminus of the parking row shall define the parking area with the following standards:
1. The landscape end islands shall be a width of no less than eighteen (18) feet and have a depth equal to that of adjacent parking stalls.
  2. The landscaped end islands shall include at least two (2) canopy trees per landscape end island.
  3. Landscaped end islands may include a walkway within the required depth.
  4. The paved area in the drive aisles between the landscaped end islands shall be clearly marked with contrasting

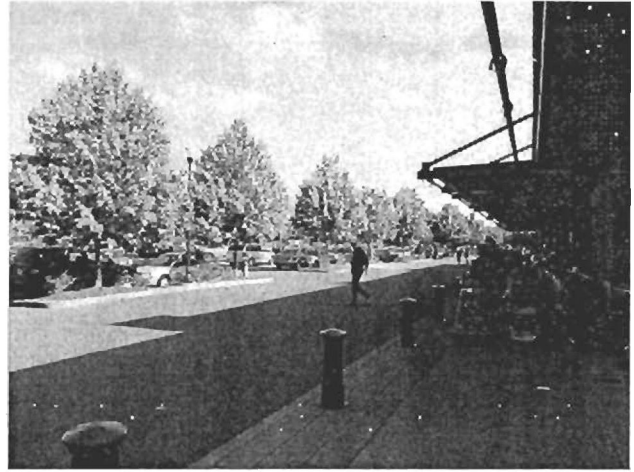
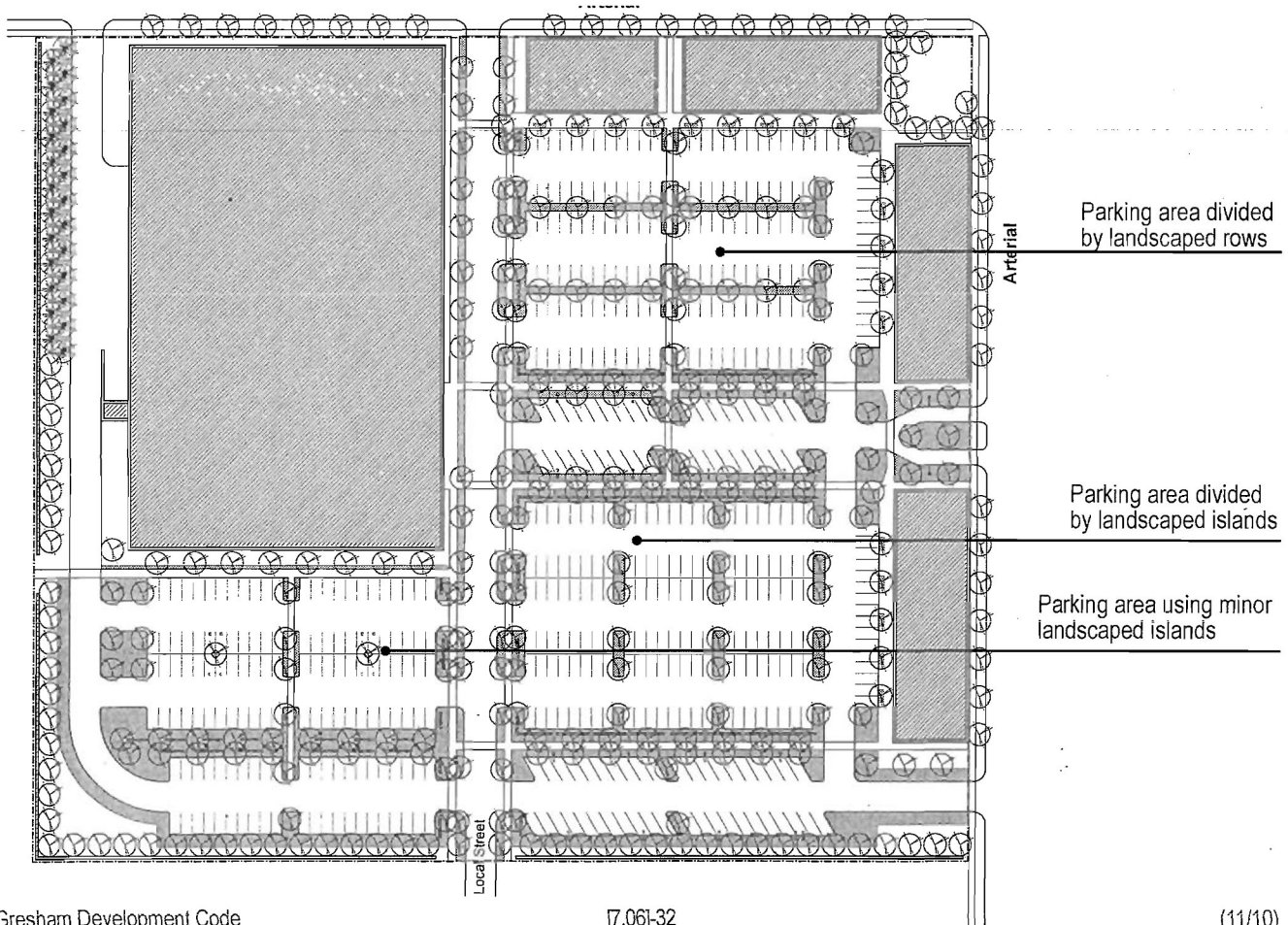


Fig. A.7.D.5.C: Well-landscaped row end islands define the parking area and drive aisles, creating an attractive condition.

Fig. A.7.D.5.B: Potential site design illustrating parking areas using landscaped rows and landscaped islands.



## 7. Landscaping, continued



Fig. A.7.D.7: Well maintained landscaped areas present a neat and orderly appearance.



Fig. A.7.D.7(1): Well maintained landscaped areas present a neat and orderly appearance.

slip resistant paving materials when a walkway extends through the landscaped end islands.

- d. Evergreen ground cover shall cover planting areas not covered by trees or shrubs or utilized for stormwater infiltration.
6. Buffering and Screening Landscaping: Standards for Buffering and Screening landscaping are found in **Section 9.0100** and **Section 7.0603(A)(10)(D)** of this code.
7. Landscape Maintenance. Compliance with the following criteria is required:
  - a. Inspections. A City representative will perform a final landscape inspection to ensure that the landscaping demonstrates equivalent compliance with the approved landscape plan upon completion of the project and before issuance of a Temporary or Final Certificate of Occupancy, following a request from the developer. The inspection time period is from March 1 to November 15. If an inspection is requested between November 16 and the last day of February and the landscaping is not complete, or if the applicant requests a Temporary Certificate of Occupancy to occupy one or more buildings on site prior to the landscaping being completed, a financial guarantee shall be provided. This will be based on one hundred ten percent (110%) of the estimated cost of plant materials and labor for the total landscape plan as indicated in a landscape cost estimate. Beginning March 1, the Applicant has one hundred eighty (180) days to complete the items or the City will cash in the amount being held and finish the landscape job.
  - b. Establishment Period. The establishment period for the plant material guarantee will begin at the Final Certificate of Occupancy inspection approval and extend to two (2) years from that date. All plantings shall be properly planted as to be in a healthy, growing condition at commencement of the establishment period. At the end of the establishment period, any plantings which are 20 percent (20%) dead or greater shall be replaced.

## 7. Landscaping, continued

c. Maintenance:

1. Maintenance of required plantings by the owner shall be carried out so as to present a healthy, neat and orderly appearance, free from refuse and debris.
2. To insure proper maintenance and as a condition of Final Site Plan approval, the property owner shall enter into and record with the City a Landscape Maintenance Agreement, or include such provisions as part of the developer's agreement or deed, each of which shall be approved by the City Attorney. Such instrument shall identify the minimum plan of maintenance, the person or entity responsible for maintenance, and shall provide the procedure, authority and finance for City cure of breaches by the responsible entity. Such instrument shall also include:
  - a. Provisions that all unhealthy and dead material shall be replaced within one (1) year, or the next appropriate planting period, whichever occurs first;
  - b. All landscaped areas shall be provided with an automatic and operating irrigation system;
  - c. Tree stakes, guy wires and tree wrap are to be removed after one (1) winter season; and
  - d. Plantings shall be guaranteed for two (2) years after the Final Certificate of Occupancy inspection approval.
3. Responsibility and Certificates of Occupancy. The owner of the property subject to the requirements of this Section shall be responsible for installing and maintaining landscaping per the approved final landscape plan as specified in this Section.



Fig. A.7.D.7(2): Well maintained landscaped areas present a neat and orderly appearance.

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## 8. Pedestrian Circulation

A. **Intent:** To provide safe, comfortable and convenient means of pedestrian movement in commercial developments by connecting building entrances, open spaces, streets, transit facilities and parking areas.

B. **Applicable Commercial Design Principles from Section 7.0602:**

- A. Accessibility
- B. Activity
- D. Parking
- F. Landscaping
- G. Sustainability
- H. Safe Design

C. **Design Guidelines:**

1. Sites shall have an integrated pedestrian circulation system which connects important areas of the site and provides dedicated space for pedestrian movements on site.
2. Accessible and Barrier-Free Design: The standards of **Section 7.0603(A)(8)(D)(2)** shall be met.
3. Pedestrian circulation routes shall provide convenient and direct connections between important locations on the site, including building entrances, open spaces, parking areas, existing sidewalks and transit facilities.
4. Pedestrian circulation routes shall be present to provide connections to adjacent sites.
5. Walkway Construction: The standards of **Section 7.0603(A)(8)(D)(5)** shall be met.
6. Walkways shall incorporate design elements which prevent vehicles from encroaching and which protect pedestrians.
7. Pedestrian circulation routes shall include sidewalks as well as dedicated routes through parking areas, which ensure safe, convenient and pleasant passage.
8. Additional walkways which are perpendicular to drive aisles shall be present when necessary to access buildings in those areas. These walkways shall provide additional buffering when between adjacent parking stalls. When crossing drive aisles, walkways shall utilize strategies which minimize crossing distances and slow traffic in order to provide safe passage for pedestrians.



Fig. A.8.C.1: A pedestrian circulation system, including sidewalks, connects building entrances and creates an attractive environment.

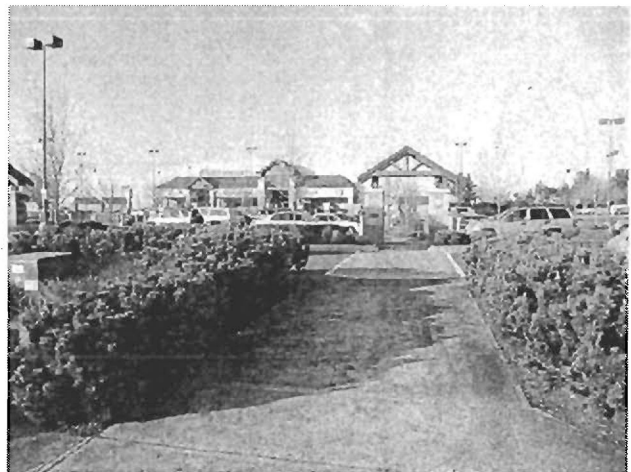


Fig. A.8.C.1(2): An internal pedestrian circulation route which connects to a public street.

## 8. Pedestrian Circulation, continued

### D. Design Standards:

1. All developments shall include an on-site pedestrian circulation system that provides connections between all adjacent streets, building entries (except service) including those of future buildings, transit stops and facilities, vehicular and bicycle parking areas, public spaces, other amenities on site and other important locations at the discretion of the Manager or Design Commission.
2. The internal pedestrian circulation system shall be barrier-free and meet accessibility standards of the Building Code. Where raised walkways are utilized, curb ramps shall be required on their ends.
3. The system shall provide reasonably direct connections between all destinations and out of direction travel shall be minimized. The pedestrian circulation system shall give priority to providing direct connections between all public streets, transit stops and the primary building(s) or use(s) on the site.
4. Connections to adjacent developments shall occur in accordance with standards in Neighborhood Connectivity and Block Structure **Section 7.0603(A)(1)(D)**.
5. All walkways comprising the pedestrian circulation system shall be hard surfaced and constructed of scored or saw-cut concrete or of decorative paving such as colored and stamped concrete, brick, stone or concrete pavers. Walkways shall be constructed at a consistent height except where crossing vehicular routes and be at least five (5) feet in width. When abutting parking spaces, walkways shall be at least seven (7) feet wide. When adjacent to or crossing vehicular traffic routes, surface materials shall contrast visually with adjoining surfaces.
6. Within the parking area, pedestrian circulation routes shall be separated from vehicular traffic, except where crossing drive lanes and parking stalls, by a raised curb, bollards, landscaping or other features approved by the Manager or Design Commission.



Fig. A.8.D.5: Visually distinct paving pattern when a pedestrian circulation route crosses a drive aisle.



Fig. A.8.D.6: A pedestrian circulation route which is separated from parking stalls with curbs, landscaping and illuminated bollards.

## 8. Pedestrian Circulation, continued

7. Sidewalks shall be present on all street frontages and primary internal drives as described in **Section 7.063(A)(2)(D) Internal Circulation: Public Streets and Primary Internal Drives** unless deviation is permitted by the Manager or Design Commission.
8. Parking Area Walkways:
  - a. Parking area walkways shall be created perpendicular to the drive aisles when the following conditions occur:
    1. Parking areas contain more than one hundred (100) spaces; and
    2. Commercial spaces or other uses on site exist in locations parallel to the parking drive aisle.
  - b. Parking area walkways shall have a direct connection to buildings located in spaces parallel to the drive aisle and may be required to cross internal public streets, primary internal drives and major landscape divisions.
  - c. Crosswalks shall be provided where parking area walkways intersect primary internal drives or internal public streets. If streets or primary internal drives include parking lanes, bump outs shall be present, extend the depth of parking lanes and follow other standards specified in **Section 7.063(A)(2)(D), Internal Circulation: Public Streets and Primary Internal Drives**
  - d. Parking area walkways shall connect to the on-site pedestrian circulation system, including sidewalks on streets and primary internal drives.
  - e. Parking area walkways shall extend no less than three (3) parking modules from the entries of commercial or other uses.
    1. If, after three (3) parking modules, the parking area walkway terminates into a primary internal drive or internal public street, bump outs and a crosswalk shall be provided across the primary internal drive.
    2. If no other sidewalks or walkways are present to access the commercial spaces or other uses, the parking area walkway shall extend to a sidewalk on a public street.



Fig. A.8.D.8.F: A walkway through the parking area featuring trees in grates.

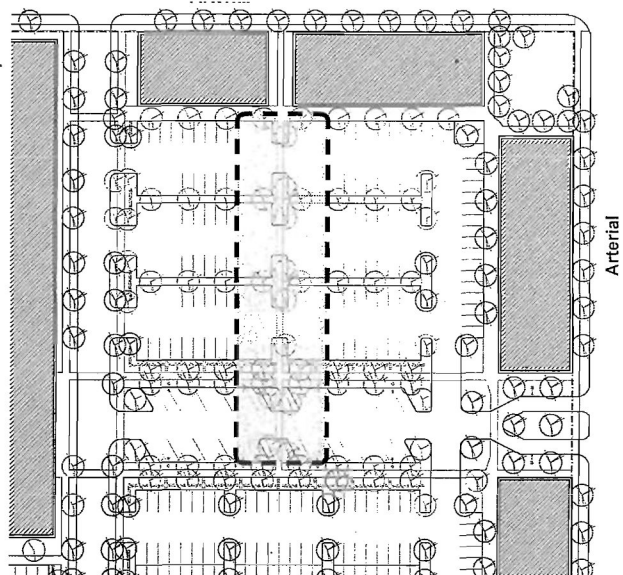


Fig. A.8.D.8: Site diagram illustrating a parking area walkway perpendicular to the parking drive aisles. The walkway extends 3 parking modules and crosses a primary internal drive.

## 8. Pedestrian Circulation, continued

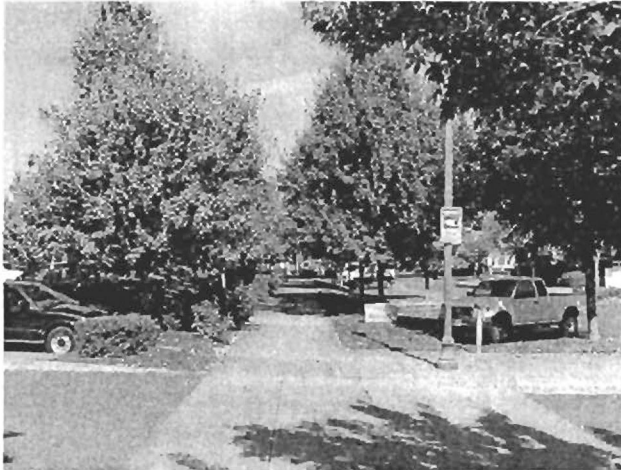


Fig. A.8.D.8.F(1): A parking area walkway with landscaping on both sides creates an attractive and pleasant passage while ensuring pedestrian safety



Fig. A.8.D.8.F(2): A parking area walkway with landscaping on both sides creates an attractive and pleasant passage while ensuring pedestrian safety

- f. Parking area walkways shall be landscaped along their entire length, exclusive of areas where the path crosses drive lanes, in the form of planted islands or planting strips. Landscaped islands which incorporate parking area walkways shall count toward the required parking area landscaped islands. Landscaping along parking area walkways shall total at least six (6) feet in width and individual landscaped areas shall be no less than four (4) feet in width. With required walkway widths, two (2) minimum width configurations are possible:
  1. A seven (7) foot walkway and a single six (6) foot landscaped area on one (1) side, or;
  2. A five (5) foot walkway with a four (4) foot landscaped area on each side.

## 9. Site Lighting

A. **Intent:** To utilize appropriate lighting fixtures and illumination levels to ensure safety during hours of darkness while providing an attractive visual element of the site design.

B. **Applicable Commercial Design Principles from Section 7.0602:**

- A. Accessibility
- B. Activity
- G. Sustainability
- H. Safe Design
- I. Impact Mitigation

C. **Design Guidelines:**

1. Lighting fixtures shall provide appropriate illumination levels for all areas of the site, creating inviting spaces and enhancing the safety of the site during evening hours.
  - a. Active pedestrian spaces shall be illuminated in hours of darkness.
  - b. Lighting fixtures shall not create negative impacts on surrounding properties or unnecessary glare within the site.
  - c. Lighting fixtures shall not create unnecessary upward directed illumination which contributes to sky-glow.
  - d. Lighting fixtures in pedestrian areas shall be appropriately scaled and placed to contribute to a cohesive and visually pleasing environment.

D. **Design Standards**

1. The site shall be designed to achieve uniform illumination levels with a minimum glare to adjacent properties in order to create a comfortable and safe environment.
  - a. The following areas shall be illuminated during the hours of darkness: primary internal drives; parking areas; service areas; pedestrian walkways; publicly accessible open spaces and building entries.
  - b. The following illumination levels plus those stated in the **Table 7.0603(A)(9)(D)(1)** shall act as minimum Standards for all exterior lighting. Maximum average lighting will be governed by the six to one (6:1) ratio of maximum average to minimum



Fig. A.C.1: Primary internal drives shall be illuminated to levels which ensure safety of pedestrians and motorists.

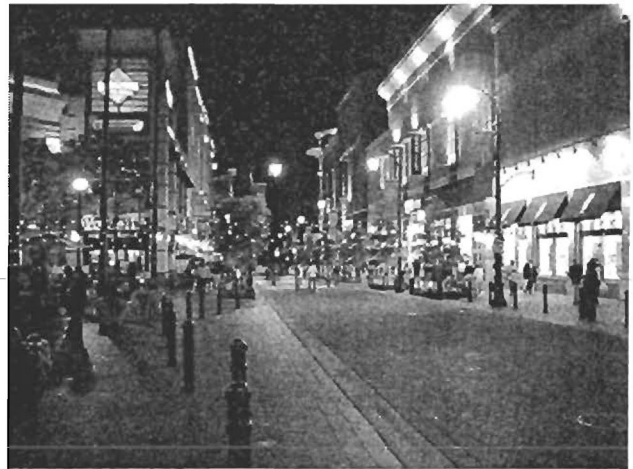


Fig. A.C.1(1): Active pedestrian spaces with appropriately scaled lighting fixtures and illumination levels.

## 9. Site Lighting, Continued



Fig. A.D.1: Pedestrian Walkways illuminated to safe and comfortable levels without creating excessively bright conditions.

illumination of the surface being lit as stated in the following table. Generally maximum illumination at the property line shall not exceed one-half (0.5) foot candle. However, where a site abuts a non-residential district, maximum illumination at the property line shall not exceed one (1) foot candle. Average foot candles shall be the average amount of light at three (3) feet in height above a surface as determined using a photometric plan with one (1) foot grid spot foot-candle readings. The Manager or Design Commission may modify these levels if such modifications are deemed necessary and appropriate for the use and surrounding area.

- c. Developments shall use full cut-off lighting fixtures to avoid off site lighting, night sky pollution and shining lights into residential units. The Manager or Design Commission may choose to waive or alter cut-off requirements of this Section when appropriate historic or decorative fixtures are proposed (e.g. use of decorative up-lighting to illuminate the underside of a canopy or columns on a facade, where a canopy or roof projection restricts the projection of the light into the night sky or the use of bollards). Weather- and vandalism-resistant covers shall protect lighting devices.
- d. The minimum light shall be measured in foot-candles at the point of least illumination when measured at three (3) feet above ground level.
- e. Light fixtures shall not exceed twenty-five (25) feet in height.
- f. Fixtures shall have a cut-off angle of ninety (90) degrees as measured perpendicular to the ground.
- g. No direct light source shall be visible at the property line (adjacent to residential) at ground level.
- h. With the exception of illuminated bollards, lighting fixtures shall not be placed within the mature canopies of trees or in other locations where illumination levels would be significantly impacted by landscaping.

Table 7.0603(A)(9)(D)(1): Site Illumination Values

Use	Illumination (Foot-candles)
Primary Internal Drives	1.0 minimum with an average of 3.5
Parking Areas	0.5 minimum
Loading and Unloading Areas	0.5 minimum
Open Spaces	0.5 minimum with an average of 1.5
Walkways	0.5 minimum with an average of 1.5
Building Entrances - Frequent Use	1.0 minimum with an average of 3.5
Building Entrances - Infrequent Use	1.0 minimum with an average of 2.0

## 10. Service and Loading Areas

- A. **Intent:** To minimize the negative impacts that required service functions, such as deliveries and trash removal, have on surrounding areas and adjacent properties.
- B. **Applicable Commercial Design Principles from Section 7.0602:**
- F. Landscaping
  - I. Impact Mitigation
- C. **Design Guidelines:**
1. Loading facilities and other service areas shall be located away from public view and public areas of the site to the greatest degree possible and shall minimize visual, acoustic and lighting impacts on surrounding areas.
    - a. Walls or liner spaces shall be present at dedicated loading facilities and shall be of sufficient scale to fully conceal and minimize noise from service vehicles.
    - b. When loading facilities face a public street, the use of liner spaces as a screening device is encouraged.
    - c. Screening walls shall be visually consistent with the building.
  2. Service area buffering shall incorporate strategies which provide year-round screening and buffering, such as walls and intensive landscaping. Buffering shall be present along the site perimeter and intensified at service areas to mitigate any potential visual or acoustic impacts on surrounding properties.
  3. Solid waste collection areas shall be designed and constructed as permanent elements of the site, utilizing high quality materials and a design that is consistent or complimentary to surrounding buildings.



Fig. A.10.C.1: Service areas located away from public areas of the site.

Fig. A.10.C.3: Dumpster enclosure effectively screened by an enclosure and landscape features.



## 10. Service and Loading Areas, continued

### D. Design Standards

#### 1. Dedicated Loading Facilities

- a. When dedicated loading facilities are required, loading areas shall be located at the rear of the building or in other locations as approved by the Manager or Design Commission. If loading areas cannot be located on the rear of the building, they may be placed along the side of the building and recessed from the front facade a distance which prevents service vehicles from extending onto adjacent walkways. Required loading area dimensions can be found in **Section 9.0840**.
- b. When frequent deliveries coincide with customer hours, the loading and delivery areas shall be separated from parking and pedestrian areas.
- c. Dedicated loading facilities, such as loading docks, shall be screened parallel to the building wall with liner spaces or walls integrated into the building and no less than fourteen (14) feet in height. The liner spaces or walls shall fully conceal service vehicles except at the entry in order to allow for safe vehicular movement while exiting.
- d. Liner spaces and screening walls shall be designed consistently with the remainder of the building and design regulations in **Section 7.0603(B)**.



Fig. A.10.D.1.D: Screening wall of a loading area using similar materials and articulation as the primary structure.

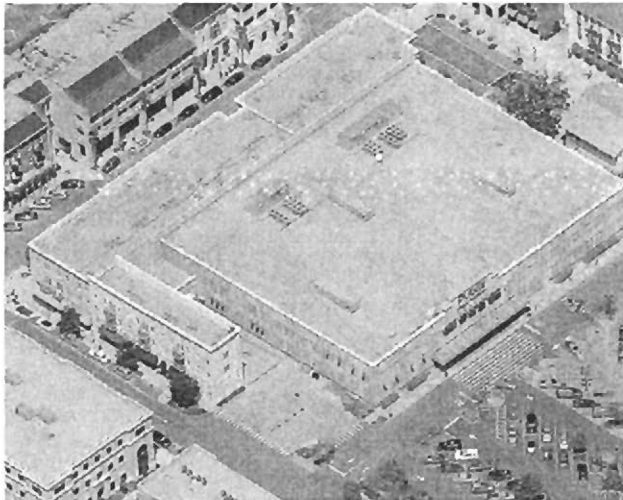


Fig. A.10.D.1.E: Aerial view of a building which use liner spaces to conceal service and loading areas.



## 10. Service and Loading Areas, continued

### 2. Buffering and Perimeter Site Screening:

- a. Alternative buffer plans, as provided for in **Section 9.0110(F)** shall be approved by the Manager or Design Commission for consistency with stated Design Principles and Guidelines in **Section 7.0603(A)(10)(B) and (C)**.
- b. In addition to buffering and screening requirements specified in **Section 9.0100**, when buffering is required, sites with buildings over thirty thousand (30,000) square feet shall utilize enhanced buffering standards as follows:
  1. A six to eight (6-8) foot masonry wall in place of required fencing, and
  2. A staggered double row of evergreen trees, totaling no less than ten (10) trees per one hundred (100) feet, shall be planted in place of required shade trees along the perimeter buffer within line of site of the opening of the dedicated loading facility. This requirement will be waived if liner spaces are used in place of a screening wall.

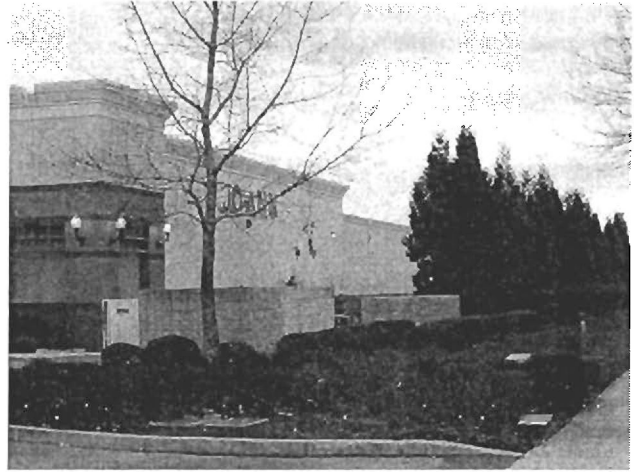


Fig. A.10.D.2.B.2: Conifer trees provide year-round screening to service and loading areas.

Fig. A.10.D.2: Site design diagram showing a potential service area configuration.

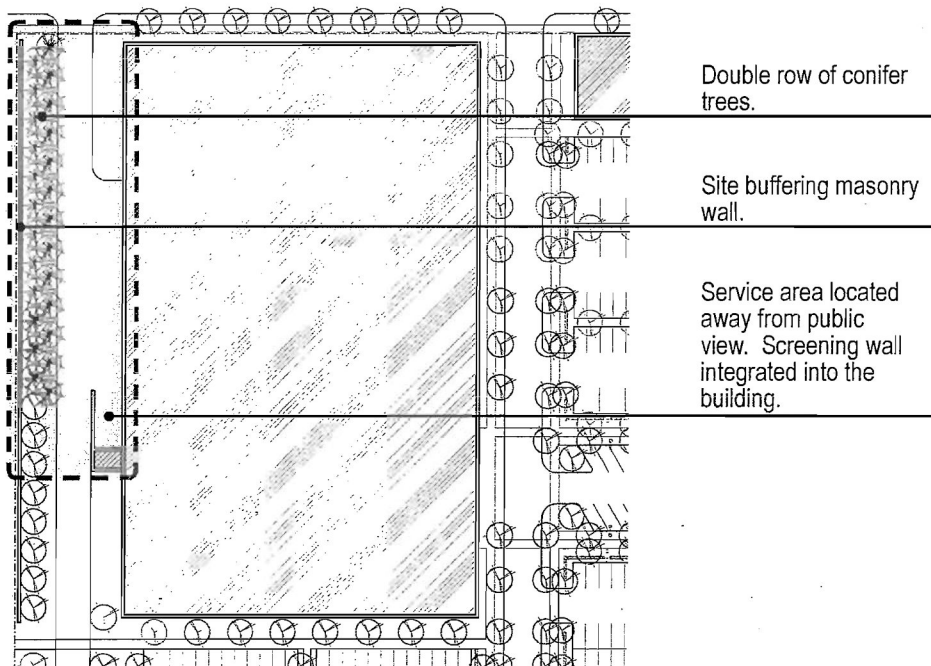




Fig. A.10.D.3: Masonry solid waste enclosure with landscaping.

3. In addition to requirements stated in **Section 7.0212** Solid Waste and Recycling Collection Area, the collection areas shall be entirely screened and enclosed by a masonry wall of at least six (6) feet in height, which is designed using identical materials and detailing as the primary building.

# 1. Building Rhythm and Facade Articulation

A. **Intent:** To enhance and enliven wall planes with design features which add depth, detail and interest to facades, reducing the visual scale of building facades to the human scale.

B. **Applicable Commercial Design Principles from Section 7.0602:**

- J. Building Form and Articulation
- K. Building Activity and Glazing

C. **Design Guidelines:**

1. Buildings shall not include long, monotonous, uninterrupted walls and shall utilize design strategies which create depth and add interest to the facade.
2. The building shall utilize design strategies which effectively add depth to the building and wall planes.
  - a. Articulating elements shall provide surface relief, depth and shadows to the facade by being recessed or projected.
  - b. Changes in building depth shall reinforce and create a consistent street wall.
3. Design elements shall occur at regular spacing which responds to the building module of the building, establishing a rhythm on the facade.
  - a. Building articulation shall establish a vertical-orientation in the facade with complimentary horizontal details, visually reducing the scale of the wall into several smaller lengths.
  - b. Building articulation shall create a human scale within the building and shall place emphasis on enhancing the pedestrian levels of facades adjacent to streets or parking areas.
  - c. Facades shall be designed in a manner that exhibits a storefront character.
  - d. Articulating features shall use a combination of changes of materials, plane, fenestration, detailing and the establishment of vertical and horizontal datums.



Fig. B.1.C.1: A well designed building incorporating design strategies to add depth and interest to the facade.



Fig. B.1.C.3.A: Design features create a vertical orientation in the facade.

Fig. B.1.C.3.C: Storefronts create an attractive building edge.



## 1. Building Rhythm and Facade Articulation, continued



Fig. B.1.D.2.A.1: A series of wall recessions used create depth in the wall plane.



Fig. B.1.D.2.A.2: Several changes in wall plane are used to establish depth in the facade.

Fig. B.1.D.2.A: A combination of projections/recessions and changes in wall plane are used to establish depth.



4. Projecting elements shall be included in the facade at the ground floor to enhance the character of the pedestrian level, provide additional depth in the facade, highlight prominent architectural features and create greater interest on the facade. Projecting elements shall follow a logical and repeating pattern corresponding to building articulation features, such as solar shades mounted over regularly spaced windows, or lighting fixtures or banners mounted to columns or pilasters.
5. Buildings shall feature an architecturally distinct base to address and enhance the meeting of the building and ground. Building bases shall be visually distinct and of a size which achieves visually pleasing and appropriate proportions.

### D. Design Standards:

1. Building walls shall be articulated with design features which add visual interest and prevent the appearance of blank walls.
2. Facade Depth: Facades visible from streets, parking areas or those with customer entries shall incorporate design strategies and features which create depth in wall planes.
  - a. Depth shall be established in facades by utilizing one (1) of the following strategies:
    1. A repeating pattern of wall recesses and/or projections that has a relief of at least sixteen (16) inches (such as recessed structural bays or recessed window openings between columns). Wall recessions and projections shall be at intervals not greater than thirty (30) feet on facades with customer entries and those facing the street, and at intervals not greater than ninety (90) feet on remaining facades.
    2. Changes in wall plane with a depth of at least twenty four (24) inches at intervals which respond to the building module and not less than thirty (30) feet and not more than ninety (90) feet.

## 1. Building Rhythm and Facade Articulation, continued

- b. Features used to establish depth in the facade shall be no less than seventy five percent (75%) of the height of the wall area attributed to the ground floor use (including the parapet on a single story building).
  - c. Buildings shall not have a total change in facade depth greater than twenty (20) feet at the ground level without the creation of publicly accessible open space, landscaped area or other feature approved by the Manager or Design Commission in this space. Vehicular loading areas which require a larger change in depth are exempt from this requirement.
3. Design Elements: All facades shall be articulated and a rhythm shall be established by repeating design elements at regular spacing which does not exceed thirty (30) feet along the length and/or height of the facade. These design elements shall be present for a minimum of eighty percent (80%) of the facade length.
- a. Buildings shall utilize a minimum of two (2) of the following design elements, each at the spacing specified above, to articulate the facade and establish rhythm:
    1. Columns, pilasters or reveals at least sixteen (16) inches in width which follow the building module.
    2. Major vertical mullions of at least six (6) inches in width on an all-glass facade which follow the building module.
    3. A repeating fenestration pattern including windows, window openings and doors.
    4. Belt courses or other horizontal banding.
    5. Vegetated facade panels or trellises maintained with healthy plant material.
    6. Integrated planters or landscape beds with a minimum width of ten (10) feet featuring trees and ground-level plantings at the required spacing.
    7. Pergolás, arcades or colonnades.
    8. Other features approved by the Manager or Design Commission.

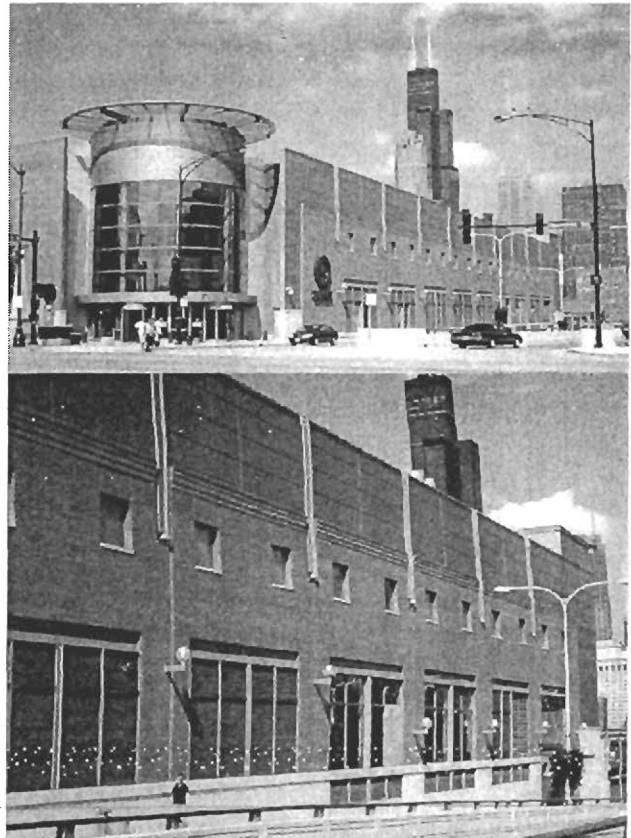


Fig. B.1.D.3: Rhythm and interest is established in the facade with a series of window opening, reveals, pilaster and light fixtures.



Fig. B.1.D.3(1): A facade which contains several design elements and strategies that are used to improve its appearance. Projecting facade elements, including solar shades and wall sconces are present along street facing facades.

## 1. Building Rhythm and Facade Articulation, continued



Fig. B.1.D.3.C: Slightly projected pilasters and recessed windows add surface relieve and shadow lines to the facade.

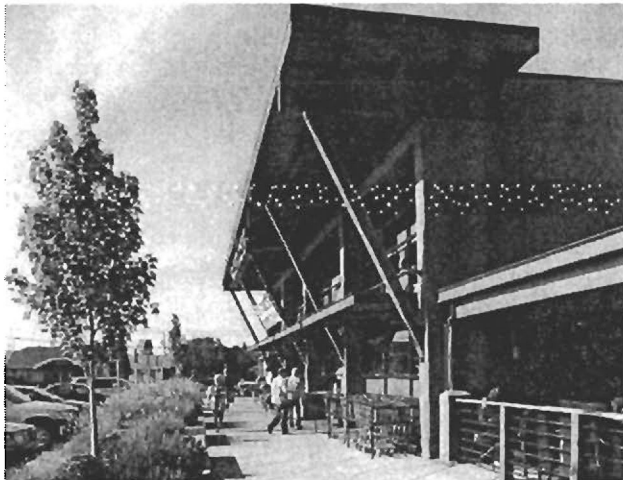


Fig. B.1.D.4: Canopies along the primary facade of the building add depth and shelter outdoor spaces from inclement weather.

- b. The use of columns, pilasters or reveals and belt courses or other horizontal banding shall only be counted as one articulating element on facades facing public streets and facades with customer entries. In this case, the use of an additional feature is required.
  - c. Design elements used to articulate the facade and establish rhythm shall include a small change in depth no less than four (4) inches from the adjacent wall plane. This dimension may be reduced to two (2) inches on belt courses and horizontal banding.
4. Projecting Facade Elements: Buildings shall provide projecting facade elements along facades fronting public streets, facades with building entries and other facades at the discretion of the Manager or Design Commission. These elements shall be at spacing no greater than thirty (30) feet and shall respond to other facade elements. Projecting elements include:
- a. Projecting lighting fixtures such as wall sconces;
  - b. Awnings, canopies or solar shades/reflectors placed over windows, doors or outdoor spaces with a minimum depth of four(4) feet;
  - c. Flags or projecting banners;
  - d. Decorative art pieces projecting from the wall;
  - e. Hanging planters; or
  - f. Other feature approved by the Manager or Design Commission.

## 1. Building Rhythm and Facade Articulation, continued

5. Building Base: Building facades shall include design elements which establish a building base.
  - a. Building bases shall consist of a visible change in the building facade, and include a change in material, color, texture, pattern or ornamentation and a change in depth no less than four (4) inches. The required change in depth for bases may be reduced to two (2) inches when they intersect other articulating features, such as pilasters, in order to provide visual distinction.
  - b. The base shall be a minimum height no less than five percent (5%) of the facade height and shall not exceed twenty percent (20%) of the facade height. At the discretion of the Manager or Design Commission, multi-story buildings of three levels or greater, shall have a building base equal to the wall area attributed to the ground floor. A landscaped area at the base of the building, with plant material at least five percent (5%) of the facade height may count toward the building base requirement.



Fig. B.1.D.5: A building base is established utilizing changes in pattern, material and depth in the facade. Additional building base landscaping is present to enhance this condition.

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## 2. Building Entry Feature

A. **Intent:** To establish a prominent building entrance that is a focal point of the building.

B. **Applicable Commercial Design Principles from Section 7.0602:**

- J. Building Form and Articulation
- K. Building Activity and Glazing
- L. Prominence and Hierarchy
- M. High Quality Materials

C. **Design Guidelines:**

1. Building entries shall be designed as a distinct, prominent element of the building, which compliments the remainder of the design.
2. Entries shall include a change in form of sufficient dimension to visually distinguish the entry from the remainder of the building facade.
3. Entries shall include design features and strategies which highlight these areas of the facade.
4. Materials on and surrounding the entry feature shall be attractive and of high quality.
5. Entries shall provide pedestrians protection from weather.
6. Entries shall be accessible to all users.

**Additional guidelines applicable to buildings greater than thirty thousand (30,000) square feet:**

7. Buildings shall have high levels of transparency concentrated around building entries.

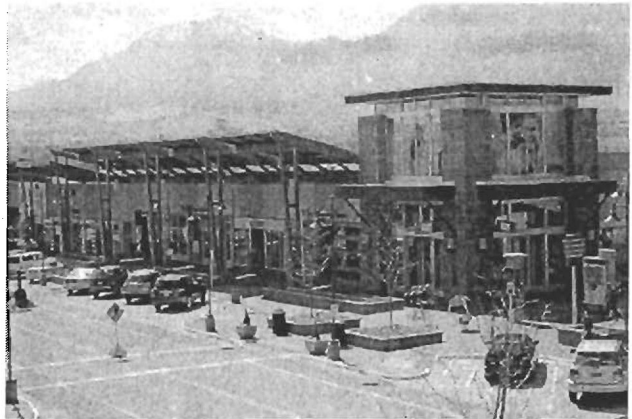


Fig. B.2.C.2: Distinct geometry add prominence to the entry feature.



Fig. B.2.C.7: Large amount of transparency at the entry improves its appearance and allow natural light into the interior.

## 2. Building Entry Feature, continued

### D. Design Standards:

1. Building entries shall feature a design that demonstrates visual prominence and architectural emphasis.
2. Building entries shall feature a visible change in building form from adjacent facade sections, which includes a change in depth of at least sixteen (16) inches.
3. Building entry features shall also include two (2) of the following features into the entry design to highlight the entry:
  - a. Oversized entry door(s);
  - b. Change in material, color, texture, pattern or articulation;
  - c. Change in roof form, such as but not limited to a projecting, curved or sloped roof;
  - d. Enhanced building ornamentation, in addition to that required in other sections of the Code, including but not limited to:
    1. Ornamental glazing flanking the doorway that is a minimum width of one (1) foot and is the full height of the doorway;
    2. Cornices, banding and belt courses;
    3. Medallions;
    4. Projecting features such as wall sconces, banners, railings and balustrades.
  - e. Distinct and decorative stone, masonry or tile paving pattern on the adjacent private sidewalk section. The size and design of the paving pattern shall correspond to geometry establish in the entry feature.
  - f. Entry courtyard with year round site furnishings like benches, tables and sitting areas;
  - g. Prominent landscape features such as integrated planters, arbors and/or base landscaping of trees, shrubs and groundcovers; or
  - h. Water feature.

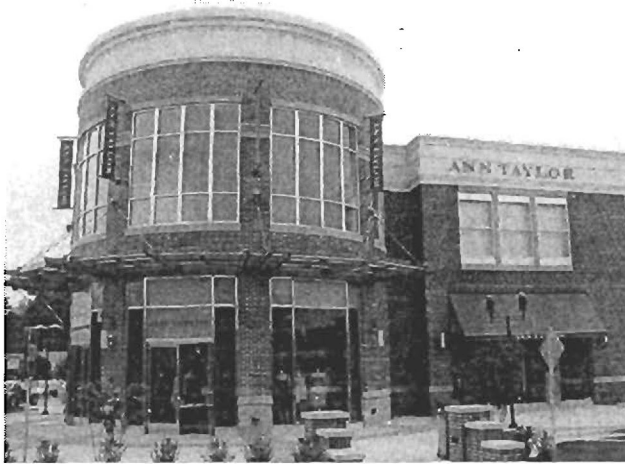


Fig. B.2.D.2: Change in form adds prominence to the facade at the building entry.



Fig. B.2.D.3: Details such as a change in paving pattern enhance the pedestrian space surrounding an entry.

## 2. Building Entry Feature, continued

### 4. Materials:

- a. Building entry doors shall be of high quality materials such as wood, glass, or other materials as approved by the Manager or Design Commission.
- b. Within thirty (30) feet along the length of the facade from each side of the entry doors, the building shall utilize only primary materials, as specified in **Section 7.0603(B)(8)(D)**, unless another material is approved by the Manager or Design Commission.

### 5. Buildings shall provide weather protection in the form of a canopy, awning or other feature of at least four (4) feet in depth.

- a. This feature shall utilize a distinct form and be larger in size than other overhangs on the facade; and,
- b. If the feature extends into the right of way, an encroachment permit is necessary.

### 6. Building entries shall comply with accessibility standards outlined in the Building Code.

### Additional standards applicable to buildings greater than thirty thousand (30,000) square feet:

7. Buildings shall have increased levels of glazing and transparency as required in **Section 7.0603(B)(4)**.



Fig. B.2.D.4: High quality materials contribute the appearance of building entries.



Fig. B.2.D.5: Weather protection is provided over the entry to protect customers from the elements.

Fig. B.2.D.7: Increased levels of transparency at the building entry creates an attractive condition.



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### 3. Prominent Facade Sections

- A. **Intent:** To establish prominence in sections of the facade which are highly visible from surrounding public spaces and streets.
- B. **Applicable Commercial Design Principles from Section 7.0602:**
  - J. Building Form and Articulation
  - L. Prominence and Hierarchy
  - M. High Quality Materials
- C. **Design Guidelines:**
  - 1. Highly-visible facade sections, including corners and facades fronting public spaces, shall be designed as a distinct, prominent element of the building, while complimenting the remainder of the design.
  - 2. Prominent facade sections should have forms which are distinct from adjacent wall sections, responding to highly visible areas of the site.
  - 3. Prominent facade sections shall included design elements and details which add visual interest to these areas.
  - 4. Materials on and surrounding prominent facade sections shall be attractive and of high quality.



Fig. B.3.C 3: Projected form at the intersection of two streets adds prominence to the corner.



Fig. B.3.C 4: Additional details on a facade section which terminates the view of a street or drive creates interest in highly-visible locations.

### 3. Prominent Facade Sections, continued

Fig. B.3.D.2: Prominent Facade Sections Profiles:

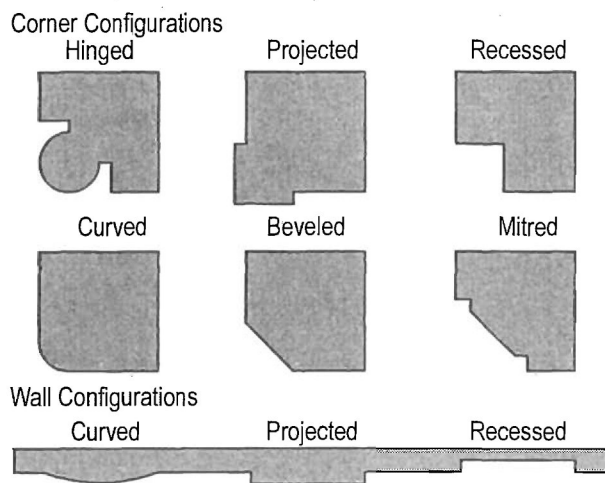


Fig. B.3.D 3: A building entry and change in form enhance the corner of a building.

Fig. B.3.D 4: Brick and other high-quality materials are used on highly visible facade sections.



#### D. Standards:

- Prominent facade sections shall include design elements that establish prominence in the building, responding to unique site configurations including street corners and the terminus of streets or primary internal drives. The orientation, massing and articulation of buildings shall place strong visual emphasis on these areas.
- Prominent facade sections shall include one of the following profiles (in plan view):
  - Curved or hinged corner or wall section;
  - A form which is projected or recessed from both abutting facades;
  - Beveled or mitred corner; or
  - Alternative configurations approved by the Manager or Design Commission.
- Prominent facade sections shall include distinctive architectural expressions in its facade, including at least two (2) of the following:
  - Operable customer entry;
  - Tower forms with an increase in height no less than ten percent (10%) of the adjacent wall height;
  - Increased glazing and transparency, with the full height of the wall area composed of no less than forty percent (40%) transparent glass. For the purposes of measurement, this area shall be no less than twenty (20) feet in length on a wall section at the terminus of a primary internal drive or public street or ten (10) feet in length along each facade when located at a corner of a building;
  - Expressive canopies with colors and materials distinctive from the rest of the building;
  - Higher bays: minimum eighteen (18) feet;
  - Cupola; or
  - Turret.
- For thirty (30) feet along the length of the facade from the building corner or center of the prominent facade section, the building shall utilize only primary materials, as specified in **Section 7.0603(B)(8)(D)**, unless another material is approved by the Manager or Design Commission.

## 4. Transparency

A. **Intent:** To add interest to exterior facades, allow for day lighting of interior space and create a visual connection between interior and exterior spaces.

B. **Applicable Commercial Design Principles from Section 7.0602:**

- H. Safe Design
- K. Building Activity and Glazing
- L. Prominence and Hierarchy

C. **Design Guidelines:**

**Guidelines applicable to buildings up to and including thirty thousand (30,000) square feet:**

1. Buildings shall have high levels of transparency at the pedestrian level on facades which face the street.
2. Non-street facing facades which have customer entries shall have sufficient levels of transparency to improve the appearance of the facade and allow for natural surveillance of the parking area.

**Guidelines applicable to buildings greater than thirty thousand (30,000) square feet:**

3. Buildings shall have high levels of transparency concentrated around active spaces, such as building entries.
4. Buildings shall include transparency at the pedestrian level on facades which face the street and those with customer entries.
5. Small tenant spaces on the perimeter of large commercial buildings shall have high levels of transparency.

**Guidelines applicable to all developments:**

6. The proportion of windows shall not contribute to a dominant horizontal geometry in the facade.
7. At the pedestrian level, transparent glazing shall allow visual interaction between active interior spaces and pedestrian areas on the buildings exterior. When this is not possible, windows which provide views of displays within the building may be acceptable.



Fig. B.4.C.1: High levels of transparency add interest by allowing views into active spaces within the building.



Fig. B.4.C.3: Windows facing the street improve the appearance of the building and the street.

Fig. B.4.C.4: Storefront windows surround the entry of a large commercial building.



## 4. Transparency, continued



Fig. B.4.C.4: Window proportions that do not contribute to a dominant horizontal geometry in the facade.



Fig. B.4.D.1: High levels of transparency facing the street improve the appearance of small commercial buildings.

Fig. B.4.D.4: High levels of transparency present at the entry of a large commercial building adds interest to the facade.



8. Window shall be maintained free of items which significantly limit the visual connection between interior and exterior spaces.
9. The use of reflective, tinted and spandrel glass shall not be permitted to meet transparency requirements.
10. Openings between interior and exterior spaces that do not contain glass may count toward the transparency requirement.
11. Mixed-use transparency (residential): The standards of **Section 7.0603(B)(4)(D)(11)** shall be met.

### D. Design Standards:

#### Standards applicable to buildings up to and including thirty thousand (30,000) square feet:

1. Buildings shall have a pedestrian level transparency zone with windows utilizing clear glass between the heights of two (2) and twelve (12) feet for no less than sixty percent (60%) of facades facing public streets.
2. Non-street facing facades with customer entries shall have pedestrian level transparency between the heights of two (2) and twelve (12) feet for forty percent (40%) of the wall area within thirty (30) feet of the entry. The length of this transparency zone may be reduced to the end of the tenant space which utilizes the entry if the transparency zone extends beyond that tenant's space.

#### Standards applicable to buildings greater than thirty thousand (30,000) square feet:

3. Buildings shall have an enhanced entry transparency zone, utilizing clear glass for sixty percent (60%) of the full height of the entry facade attributed to the ground floor space. This shall include the parapet in a single story building.



## 4. Transparency, continued

- a. The width of the enhanced entry transparency zone shall be no less than sixty (60) feet. At the discretion of the Manager or Design Commission, the width of the entry transparency zone may be reduced to twice the spacing of articulating features utilized to meet the Standards contained in **Section 7.0603(B)(1)(D)** if sixty (60) feet does not follow the rhythm of the building. This zone shall not be reduced below forty (40) feet.
  - b. To encourage day lighting within the building, clear glass shall be present at the finish ceiling level of the ground floor interior space.
4. Outside the enhanced entry transparency zone, buildings shall have a pedestrian level transparency zone with windows utilizing clear glass between the heights of two (2) and twelve (12) feet for no less than twenty percent (20%) of facades facing public streets and facades with customer entries.
  5. If liner spaces are present in a building over thirty thousand (30,000) square feet, the wall area attributed to these spaces must meet the transparency requirement for buildings thirty thousand (30,000) square feet and less. The window area attributed to these spaces may be applied towards the building transparency requirements for buildings over thirty thousand (30,000) square feet.

### Standards applicable to all developments:

6. Window Proportions: Windows shall utilize the following proportions. Walls comprised entirely of glass are exempt from this requirement.
  - a. Storefront windows shall exhibit a horizontal proportion no greater than 2:1 (length: height).
  - b. All other window openings shall exhibit a vertical or square proportion.
7. At the discretion of the Manager or Design Commission, display windows that do not provide views into the store may count towards the pedestrian level transparency requirement if the display extends a minimum of four (4) feet into the building and contains three dimensional



Fig. B.4.D.4.B: Windows which extend above the pedestrian level allow daylight into the space.



Fig. B.4.D.6: Vertical orientation of windows reduces the visual length of facades more effectively than horizontal windows.

Fig. B.4.D.8: Windows facing the street allow views into the space and are not significantly obstructed by shelving or other items.



## 4. Transparency, continued



Fig. B.4.D.11: Windows in residential units above the ground floor provide interest on upper levels of the facade.

- (3D) product displays or mannequins.
- 8. Required windows within the pedestrian level transparency zone shall be maintained free of shelving, signage (including painted window signage) or other items that reduces visibility by more than fifty percent (50%) between the interior and exterior spaces.
- 9. To meet the clear glass requirement, windows shall have a visible transmittance value (VT) no less than sixty percent (60%). Where clear glass is required, the use of reflective, tinted or spandrel glass shall not be permitted.
- 10. If structured parking is present at the ground level, window openings without glass but utilizing a mullion system or other decorative feature in those areas shall count toward this requirement at the discretion of the Manager or Design Commission.
- 11. If a building includes residential units:
  - a. The facade area associated with residential spaces shall comply with transparency requirements found in **Section 7.0103(B)(2)(d)**; and
  - b. Commercial space shall follow the stricter of the Transparency Standards found in **Sections 7.0603(B)(4)(D) and 7.0103(B)(2)(D)(11) and (12)**.

## 5. Roofs and Parapets

- A. **Intent:** To create a visually interesting condition at the top of the building that enhances the quality and character of the building.
- B. **Applicable Commercial Design Principles from Section 7.0602:**
- J. Building Form and Articulation
- C. **Design Guidelines:**
1. Parapets shall be of sufficient height to conceal necessary roof-top equipment.
  2. Parapets shall not be excessively tall and dominate the facade or create an obviously false appearance. Parapet extensions may be used to highlight focal points of the building.
  3. Parapets shall not appear as flat and obviously false extensions of building wall sections, but rather appear as distinct building masses and extend into the depth of the building.
  4. Features shall be present on visible roof surfaces to reduce the visual scale of these surfaces and provide interest along their length.

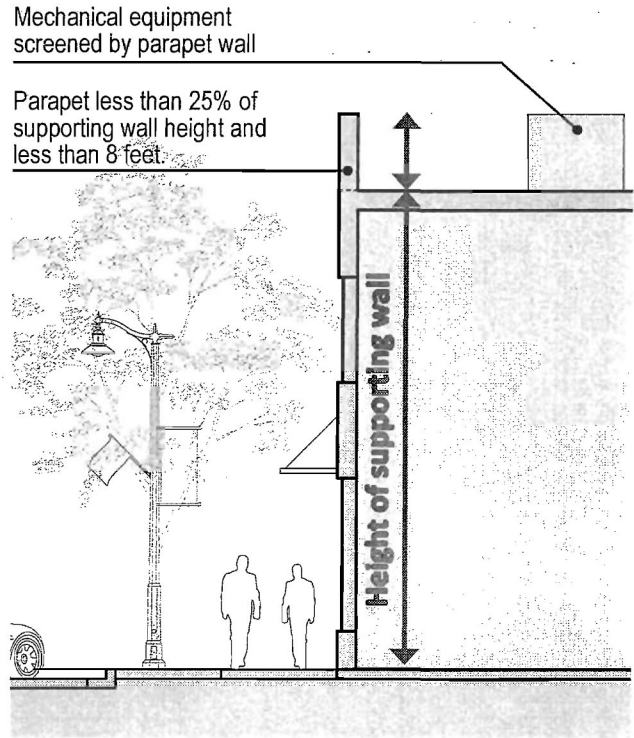


Fig. B.5.D.1: Diagram illustrating standard 70603(B)(5)(D)(1) and (2).

## 5. Roofs and Parapets, continued

### D. Design Standards:

1. Mechanical and communication equipment and components shall be screened so that the equipment is not visible at ground level from streets and other public spaces, including alleys. It shall be screened in a manner that is compatible with the architectural character of the building. Appropriate screening for rooftop equipment includes parapet walls or fabricated enclosures such as panels. The Manager or Design Commission reserves the right to review screening of rooftop equipment by requesting sight line studies. Solar energy panels are exempt from this requirement.
2. Parapets shall not exceed twenty five percent (25%) of the height of the supporting wall, as measured from grade to the exterior roof surface, and shall not exceed eight (8) feet in height.
3. In order to establish depth at the roof line, when parapets are used to increase the height of specific building wall sections, the parapet shall extend into the depth of the building no less than twice the distance of the increase in height, as measured from the point of intersection with the lower parapet or roof if no parapet is present.
4. When pitched roof surfaces are visible (slopes greater than 2:12), the roof surface shall include a change in form, such as a change in height, pitch, orientation, or other changes in form at spacing no less than sixty (60) feet. Sloping roofs shall also include at least two (2) of the following design elements:
  - a. Slope of 4:12;
  - b. Two (2) or more sloping planes;
  - c. Overhanging eaves extending at least one (1) foot beyond the supporting wall;
  - d. An acceptable roof style such as a gable, hipped, shed, butterfly roof form or other as determined by the Manager or Design Commission.

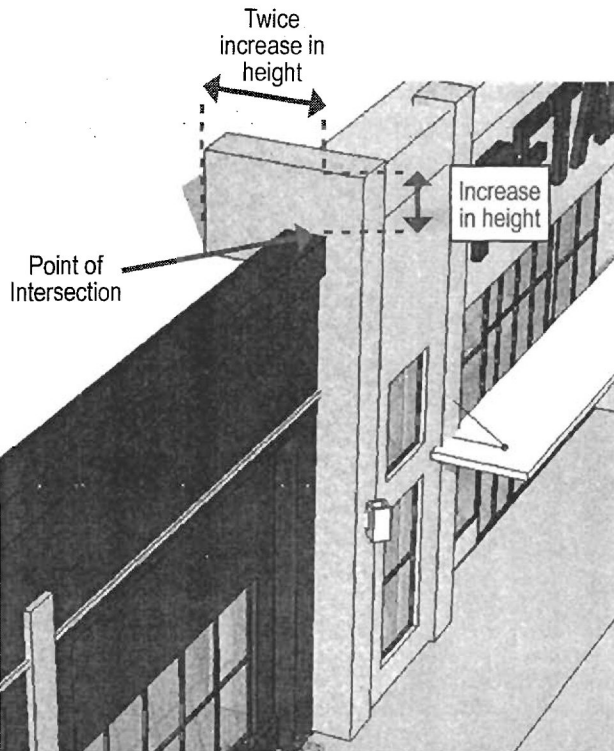


Fig. B.5.D.3: Diagram illustrating Standard 7.0603(B)(5)(D)(3).



Fig. B.5.D.4: A pitched roof with several changes in form which add interest to visible roof surfaces

## 6. Arcades

A. **Intent:** Properly designed arcades can enhance facade designs by establishing depth and providing protection from the elements.

B. **Applicable Commercial Design Principles from Section 7.0602:**

J. Building Form and Articulation

C. **Design Guidelines:**

1. Arcades shall be integrated into the overall design of the building, featuring similar or complimentary changes in articulating features and roof forms which enhance the pedestrian level. The design of arcades shall prevent the development of dominant horizontal elements in the facades of single-story buildings and incorporate features which interrupt its length, including changes of depth and height.
2. Dimensions and spacing of arcade columns shall respond to columns, pilasters or reveals on the primary structure.
3. Walkways beneath arcades shall be of sufficient width to accommodate anticipated pedestrian traffic.
4. Arcades shall be designed to limit or prevent dark spaces and facades beneath it.



Fig. B.6.C.1: An arcade featuring similar design elements and strategies to integrate it into the overall building design.



Fig. B.6.C.4: An arcade which allows sufficient light on to the building facades beneath it.

## 6. Arcades, continued

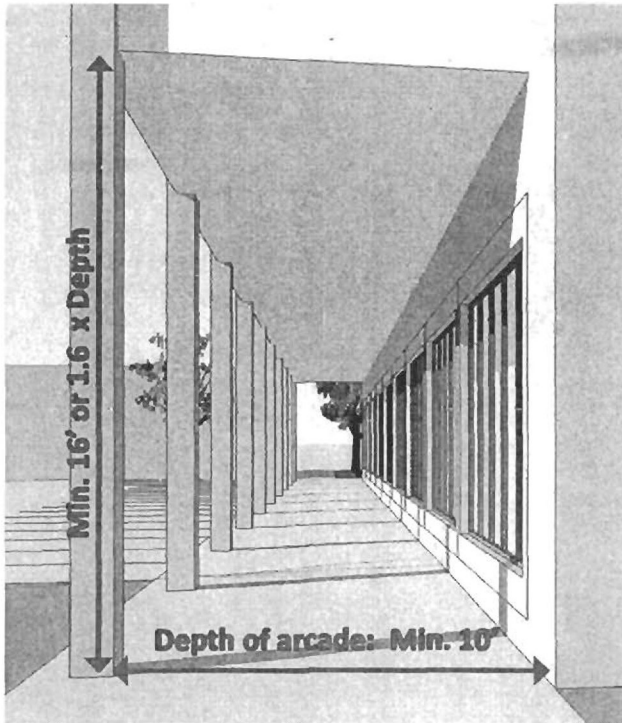


Fig. B.6.D.4: Minimum dimensions of an arcade. Dimensions ensure sufficient space is present for pedestrian movement and allow natural illumination of the walk and the facade.

### D. Design Standards:

1. When arcades are located in front of or integrated into a building, they shall conform to standards found in the following sections:
  - a. **Section 7.0603(B)(1)** Building Rhythm and Facade Articulation; and
  - b. **Section 7.0603(B)(5)** Roofs and Parapets (if applicable);
2. Arcades shall feature columns which correspond to articulating features and shall have a minimum width of sixteen (16) inches.
3. Beneath the arcade, an unobstructed walking path of at least ten (10) feet in width shall be provided.
4. The space beneath arcades shall be designed to prevent dark facades. The open height of the arcade, as measured from the highest open point under the outside edge of the arcade, shall be a minimum of sixteen (16) feet. If the walking path below the arcade exceeds ten (10) feet, the height of the arcade shall be no less than one and six tenths (1.6) times the depth of the arcade attributed to the walking path.

## 7. Outdoor Sales and Storage Areas

- A. **Intent:** To ensure outdoor sales areas are designed in a manner which creates attractive enclosures that are consistent with the building design and outdoor storage areas are screened from public view.
- B. **Applicable Commercial Design Principles from Section 7.0602:**
- J. Building Form and Articulation
  - M. High Quality Materials
- C. **Design Guidelines:**
1. When present, outdoor sales areas shall be designed as a permanent and integral component of the primary structure.
  2. The structure shall be of a sufficient height to appear as an element of the adjacent building.
  3. The structure shall be constructed of durable, high-quality and attractive materials, and the design shall complement the building design, following the established rhythm and design details from articulating features of the adjacent building.
  4. Outdoor storage areas shall be enclosed and screened by landscaping.

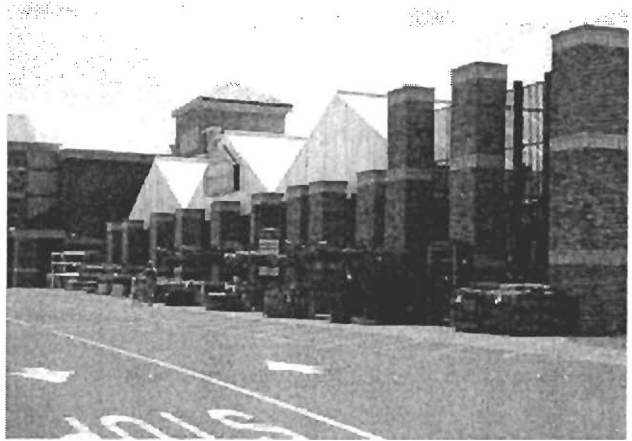


Fig B.7.C.1: Outdoor sales area which compliments the adjacent building, utilizing similar materials and articulation.

## 7. Outdoor Sales and Storage Areas, continued

### D. Design Standards:

1. Outdoor sales areas shall be located adjacent to and shall share at least one (1) common wall with the building.
2. The outdoor sales areas shall be enclosed by a decorative fence or wall or a greenhouse-type glazed structure no less than twelve (12) feet in height.
3. If a fence or wall encloses the space, the following standards shall apply:
  - a. Piers shall be a minimum dimension of sixteen (16) inches in width and at spacing no greater than thirty (30) feet.
  - b. When a decorative fence is utilized, a base shall be built to a height equal to the height of the building base treatment up to a maximum of twenty four (24) inches.
  - c. The base and piers of the fence or wall shall utilize brick, stone, decorative concrete block, finished concrete or other material that is approved by the Manager or Design Commission. All materials shall be consistent with the colors and finishes of the primary building.
4. Outdoor storage areas shall be entirely screened by the employment of vegetative materials or alternative as deemed appropriate by the Manager or Design Commission. Exceptions to this requirement include: New or used cars, cycles, and truck sales (but not including car parts or damaged vehicles); new or used boat sales; recreational vehicle sales; mobile homes sales; new or used large equipment sales or rentals; florists and plant nurseries.

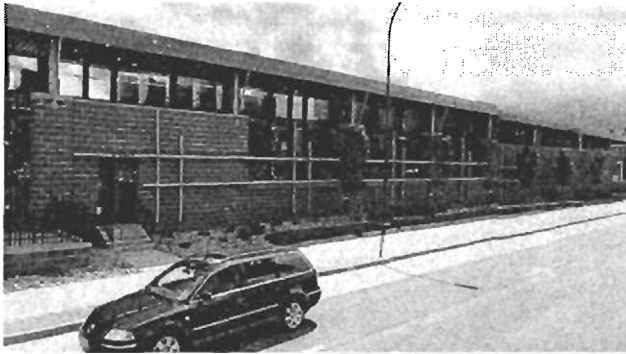


Fig B.7.D.2: A permanent outdoor sales area that maintains the street edge.



Fig B.7.D.2: An outdoor sales area which uses masonry piers and decorative fencing. The masonry base is also lined with landscape to improve its appearance.



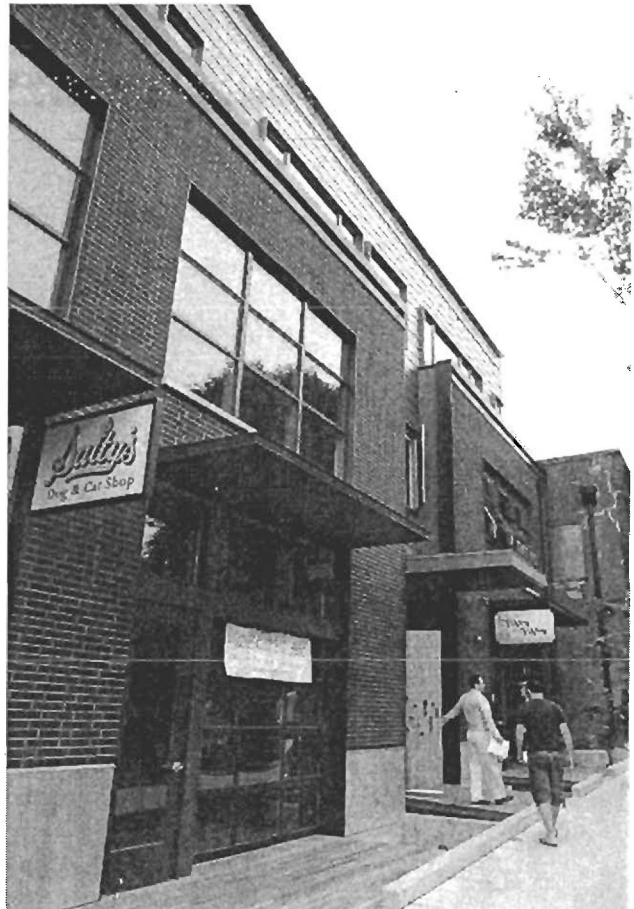
## 8. Materials

- A. **Intent:** To ensure buildings utilize high-quality, durable and attractive materials which contribute to the aesthetic quality of the development.
- B. **Applicable Commercial Design Principles from Section 7.0602:**
- M. High Quality Materials
- C. **Design Guidelines:**
1. The predominant building materials shall be high quality, durable and attractive.
  2. The predominant building material may be complimented with other materials which may not be appropriate on large areas of the facade.
  3. Accent materials, which would generally not be acceptable on large areas of the facade, may be used in limited areas of the facade to highlight architectural features.
  4. Prohibited materials: The standards of **Section 7.0603(B)(8)(D)(4)** shall be met.
  5. Fencing shall be durable, maintainable and attractive.



Fig. B.8.C.1: Stone and wood combined to create an attractive appearance.

Fig. B.8.C.2: The variety of materials, primarily brick which contrasted with concrete and metals, creates interest and texture on the facade.



## 8. Materials, continued



Fig. B.8.D.1: The street-facing facade utilizes a complimentary stone and glass combination.

Fig. B.8.D.1(1): Multiple finishes of brick and stone are used together to establish texture in the facade.



### D. Design Standards:

1. Buildings shall utilize primary materials for no less than sixty-five percent (65%) of the building facades. Primary materials shall include:
  - a. Brick;
  - b. Stone/masonry;
  - c. Stucco;
  - d. Glass (transparent, spandrel, block); and
  - e. Other materials as approved by the Manager or Design Commission.
2. The following secondary materials are prohibited as primary cladding on building facades and shall not be allowed on more than thirty-five (35%) of building facades:
  - a. Finished wood, wood veneers and wood siding;
  - b. Finished metal panels such as anodized aluminum, stainless steel or copper, featuring a polished, brushed or patina finish;
  - c. Concrete blocks with integral color (ground, polished or glazed finishes)
  - d. Concrete (poured in place or precast);
  - e. Fiber reinforced cement siding and panels;
  - f. Ceramic tile; and
  - g. Other materials as approved by the Manager or Design Commission.
3. The following materials are permitted as accent materials on no greater than 5% of the facade as trims or accents (e.g., flashing, projecting features, ornamentation, etc.):
  - a. Concrete blocks with integral color (split-face finish)
  - b. Standing seam and corrugated metal;
  - c. Glass block;
  - d. Vegetated wall panels or trellises; and
4. Prohibited materials: The following materials shall not be used:
  - a. Vinyl siding;
  - b. T-111 Plywood; and
  - c. Exterior Insulation Finishing System (EIFS).

## 8. Materials, continued

5. Fencing materials shall be durable, maintainable and attractive. The following fencing materials are prohibited:
  - a. Plastic or vinyl fencing; and
  - b. Chain-link fencing.
6. Materials not listed in this Section shall be reviewed by the Manager or Design Commission to determine appropriateness. These materials shall be used only if, in the opinion of the Manager or Design Commission, they contribute to exceptional design.



Fig. B.8.D.3: Vegetated wall panels may be used to accent primary materials.

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## 9. Building Harmony

- A. **Intent:** To ensure a complimentary and harmonious relationship between multiple buildings on a single site, while preventing the monotony of identical buildings.
- B. **Applicable Commercial Design Principles from Section 7.0602:**
- J. Building Form and Articulation
  - M. High Quality Materials
- C. **Design Guidelines:**
1. When multiple buildings exist on a single site, they shall exhibit design strategies which create visual cohesion while maintaining variation and individuality between the buildings.
  2. Buildings shall use a palette of consistent or complimentary materials or colors to create harmony and unity between structures.
  3. Buildings shall incorporate consistent or complimentary design strategies and features.



Fig. B.9.D.1: Orenco Station utilizes consistent materials and colors to create a cohesive environment which features distinct buildings.

## 9. Building Harmony, continued

### D. Design Standards:

1. When multiple buildings exist on a site, individual buildings shall utilize a consistent and complimentary design that creates a cohesive visual environment while allowing individual buildings to be unique.
2. When part of a larger site, all buildings shall share a single common building material or common color.
3. All buildings shall also utilize a minimum of one (1) of the following design strategies:
  - a. Use of consistent forms, such as but not limited to arches;
  - b. Use of consistent wall articulation features such as those noted in **Section 7.0603(B)(1)(D)**;
  - c. Consistent building proportions for window openings;
  - d. Consistent use of building details such as but not limited to awnings, canopies, lighting fixtures, medallions, etc.; or
  - e. Other design strategy approved the Manager or Design Commission.



Fig. B.9.D.1: Buildings at Edgewood Retail District all use brick and stone, and feature similar design elements.

# 1. Sustainable Site and Building Design

- A. **Intent:** To minimize negative environmental impacts from development by utilizing sustainable building techniques which reduce water usage, stormwater runoff, heat island effects, life-cycle cost and energy usage.
- B. **Applicable Commercial Design Principles from Section 7.0602:**
  - A. Accessibility
  - D. Parking
  - F. Landscaping
  - G. Sustainability
  - I. Impact Mitigation
  - N. Sustainable Architectural Design

## C. Design Guidelines:

- 1. Landscape and Stormwater
  - a. High efficiency irrigation systems and strategies shall be utilized to minimize potable water usage on landscaping.
  - b. Gresham's Green Development Practices: The standards of **Section 7.0603(C)(1)(D)(1)(b)** shall be met.
  - c. Stormwater shall be managed on-site with infiltration and treatment areas, which may be included in landscaped areas. Stormwater shall be controlled so no adverse effects are created on surrounding areas and/or public infrastructure.
- 2. Heat Island Reduction: Roof surfaces shall utilize light-colored materials to minimize localized heat gains.

### Guidelines applicable to buildings greater than thirty thousand (30,000) square feet:

- 3. Buildings shall incorporate other sustainable design features that reduce heat islands, energy and water usage, manage stormwater and preserve existing environmental assets.



Fig. C.1.C.1.B: The boulevard at Cascade Station incorporates stormwater facilities for the development.

Fig. C.1.C.3: REI in Portland achieved LEED Gold for incorporating sustainable development practices.



## 1. Sustainable Site and Building Design, continued

### D. Design Standards:

#### Standards Applicable to All Developments

1. Landscape and Stormwater
  - a. Landscaping shall reduce potable water usage for irrigation by the use of a drip irrigation system with rain sensors or other means as approved by the Manager or Design Commission.
  - b. Developments shall follow the City of Gresham's Green Development Practices for Stormwater Management. Sites shall use the infiltration and treatment strategies listed below to effectively treat and infiltrate stormwater on-site. These strategies shall be implemented in the following order of priority:
    1. Reduce impervious surfaces (such as green roof, pervious pavement);
    2. Surface infiltration facilities (such as rain gardens, planter boxes);
    3. Sub-surface infiltration methods (such as drywells);
    4. Other options as approved by the Manager or Design Commission.
  - c. Grading and contouring of the site, site surface drainage and on-site storage of surface water facilities shall be constructed, when necessary, so that there is no adverse effect on neighboring properties, public right-of-ways or the public storm drainage system.
2. Heat Island Reduction - Roof Surface: All low-sloped (pitches  $\leq 2:12$ ) roof surfaces, exclusive of space dedicated to mechanical systems, vegetated roof surfaces or solar panels, shall utilize a "white roof" with an Solar Reflectance Index (SRI) of seventy-eight (78) or greater.

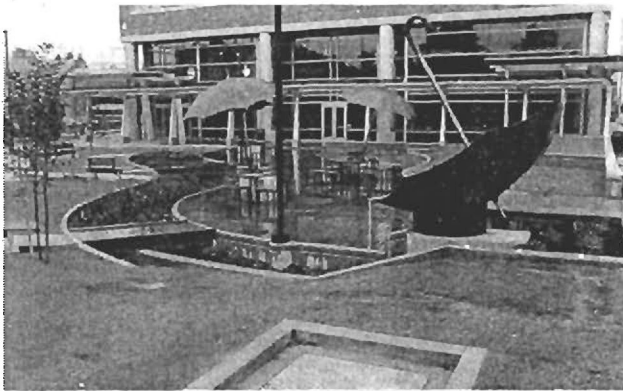


Fig. C.1.D.1.B: An open space which incorporates stormwater infiltration facilities.

Fig. C.1.D.2: A roof using solar energy panels on a "white roof" to minimize heat gain and reduce cooling costs.





# 1. Sustainable Site and Building Design, continued

## Additional Standards for buildings greater than thirty thousand (30,000) square feet:

3. Buildings shall comply with two (2) of the following requirements:
  - a. A vegetated roof surface comprising a minimum of thirty percent (30%) of the roof area;
  - b. Solar energy panels comprising an area equivalent to twenty percent (20%) of the roof area;
  - c. A system that collects rainwater from a minimum of fifty percent (50%) of the total roof area for reuse (e.g., site irrigation or gray water reuse);
  - d. Skylights or other day lighting system which illuminates seventy five percent (75%) of the building floor area, with skylights occupying a minimum of three percent (3%) of the roof area with spacing between skylights not greater than one and four tenths (1.4) times the ceiling height;
  - e. Provide an on-site alternative fuel refueling station (such as an electric, bio-diesel, or natural gas refueling station, etc). An electric fueling station must be within sight of a functional building entry;
  - f. Source sustainable and local building materials from within five hundred (500) miles of the development site for no less than twenty (20%) of the total construction materials;
  - g. Preserve no less than fifty percent (50%) of existing regulated trees on site, minimum four (4). Regulated trees must be healthy as determined by a consulting arborist, a qualified arborist or a registered consulting arborist;
  - h. Preserve all Habitat Conservation Area (HCA) on-site, minimum one quarter (.25) acre.

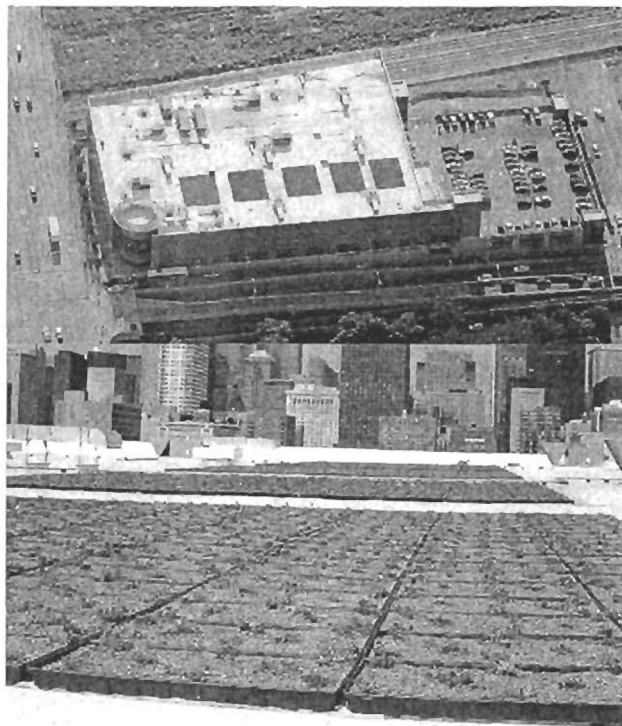
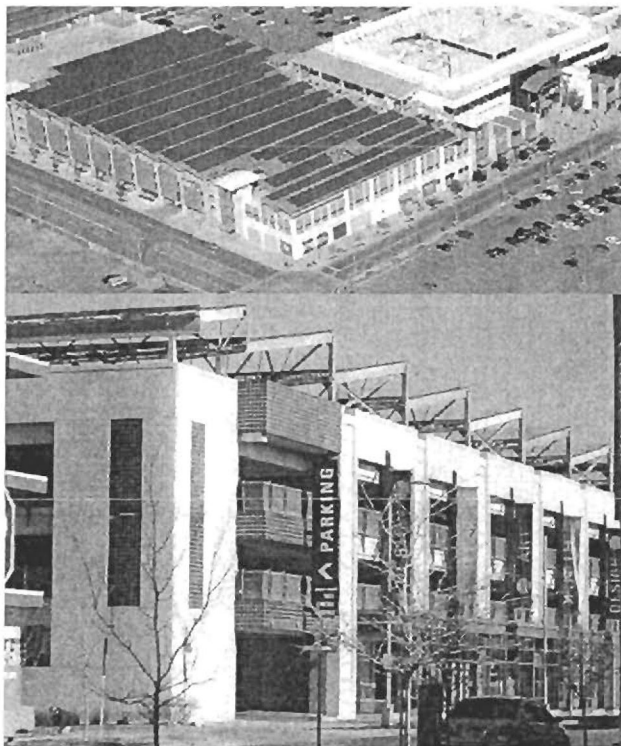


Fig. C.1.D.3.A: Green roof panels installed on a large commercial building.

Fig. C.3.B: Solar arrays on a parking structure at Belmar, Lakeview, CO.



CITY OF GRESHAM, OREGON  
RETAIL DESIGN AND  
DEVELOPMENT STANDARDS

**CURRENT  
LAND USE DISTRICT**  
Map One of Eight:  
NORTHWEST GRESHAM

**LEGEND**

- CC - Community Commercial
- PLSS Quarter Sections
- Current Gresham City Limits
- Tax Lots

ZONING	ACRES
CC - Community Commercial	8.4

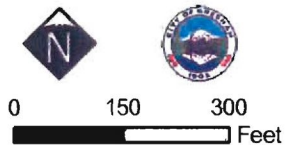


EXHIBIT B  
CPA 10-026  
CB 13-10



CITY OF GRESHAM, OREGON  
RETAIL DESIGN AND  
DEVELOPMENT STANDARDS

**PROPOSED  
LAND USE DISTRICT**  
Map One of Eight:  
NORTHWEST GRESHAM

**LEGEND**

- MC - Moderate Commercial
- PLSS Quarter Sections
- Current Gresham City Limits
- Tax Lots

ZONING	ACRES
MC - Moderate Commercial	8.4

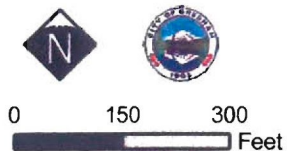
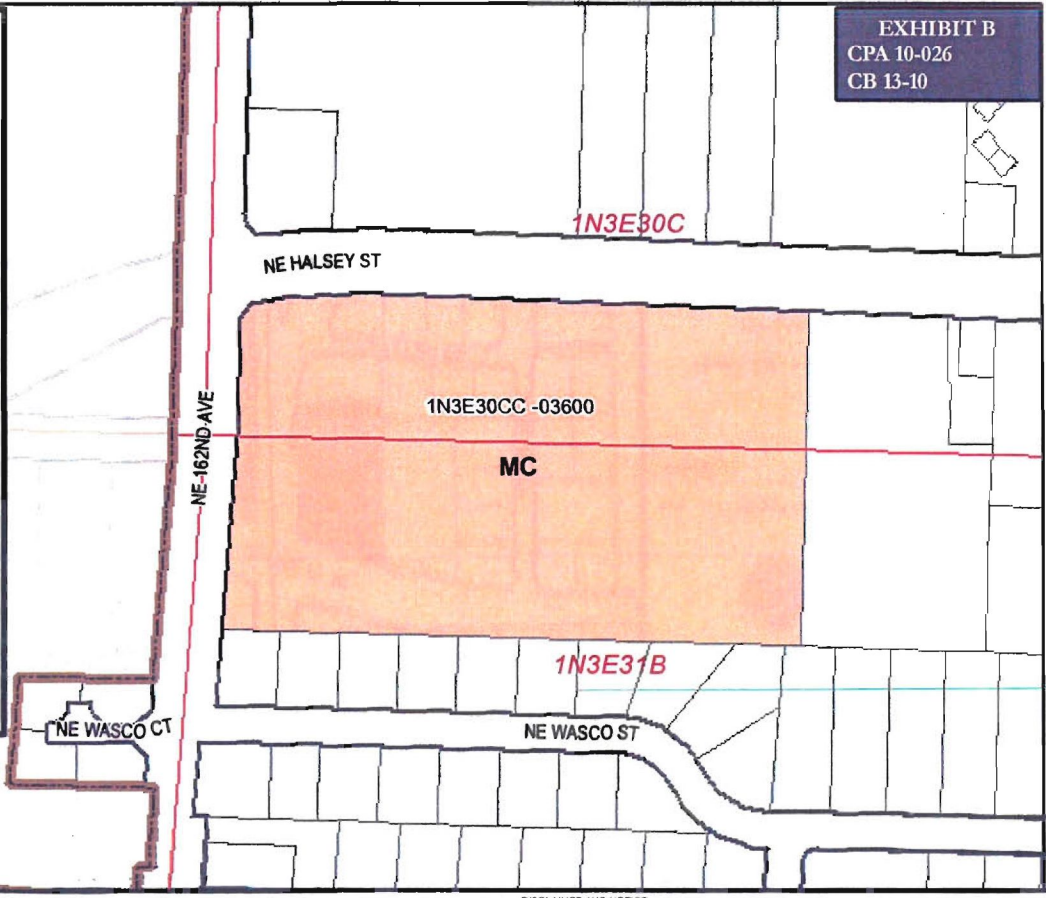


EXHIBIT B  
CPA 10-026  
CB 13-10



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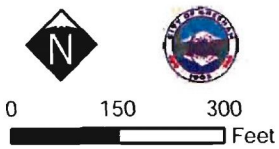
CITY OF GRESHAM, OREGON  
RETAIL DESIGN AND  
DEVELOPMENT STANDARDS

**CURRENT  
LAND USE DISTRICT**  
Map Two of Eight:  
NORTHEAST GRESHAM

LEGEND

- GC - General Commercial
- PLSS Quarter Sections
- Current Gresham City Limits
- Tax Lots

ZONING	ACRES
GC - General Commercial	1.6



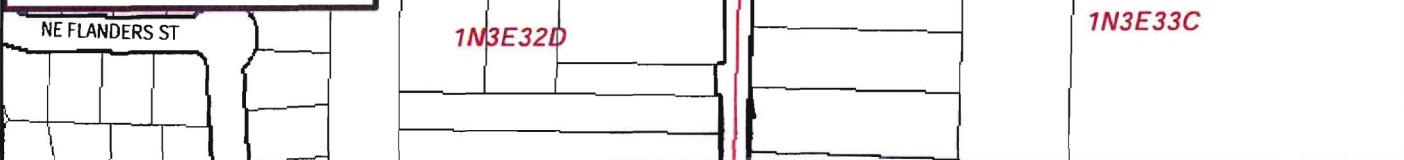
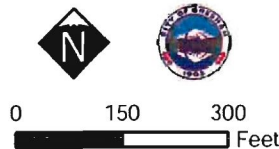
CITY OF GRESHAM, OREGON  
RETAIL DESIGN AND  
DEVELOPMENT STANDARDS

**PROPOSED  
LAND USE DISTRICT**  
Map Two of Eight:  
NORTHEAST GRESHAM

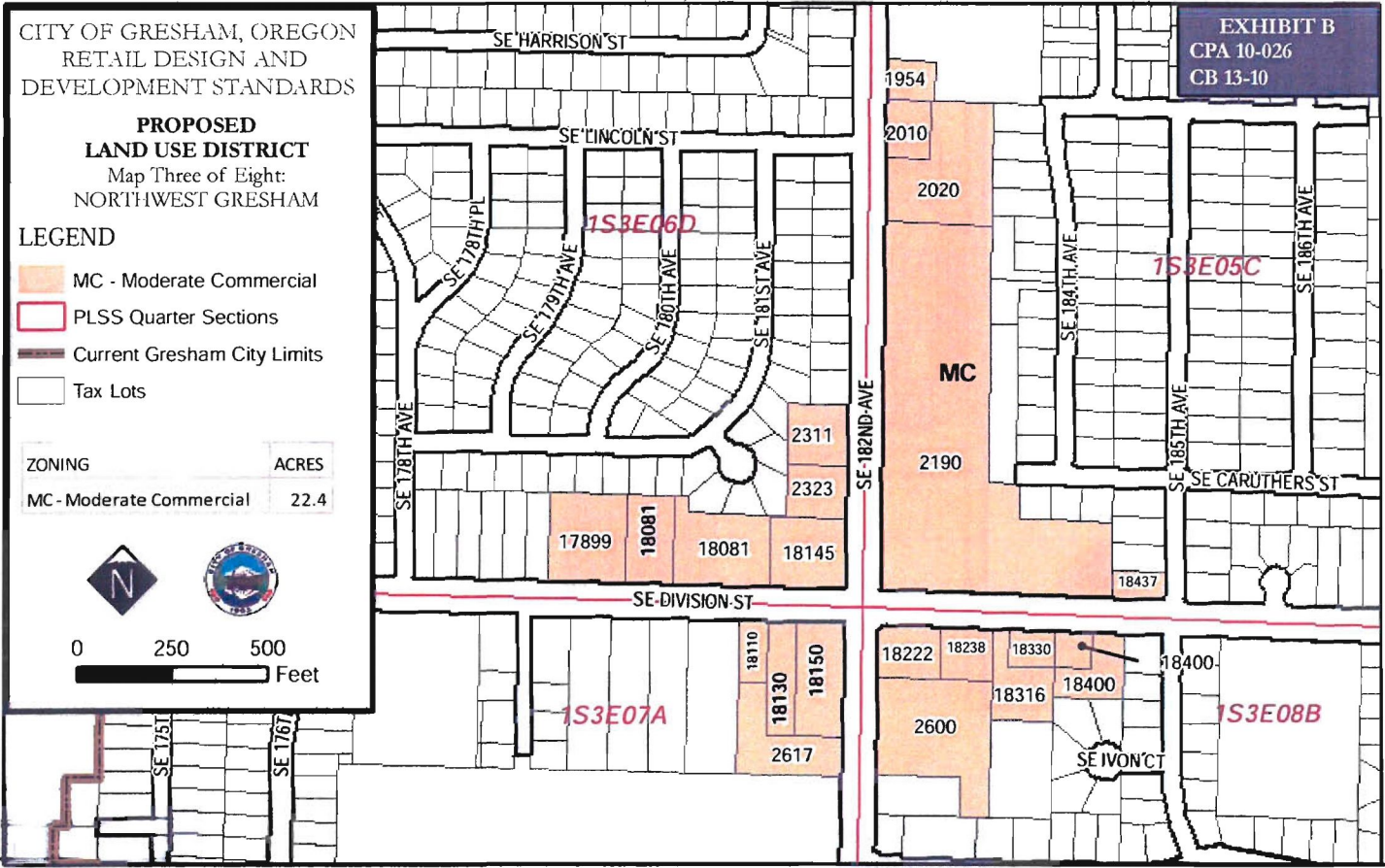
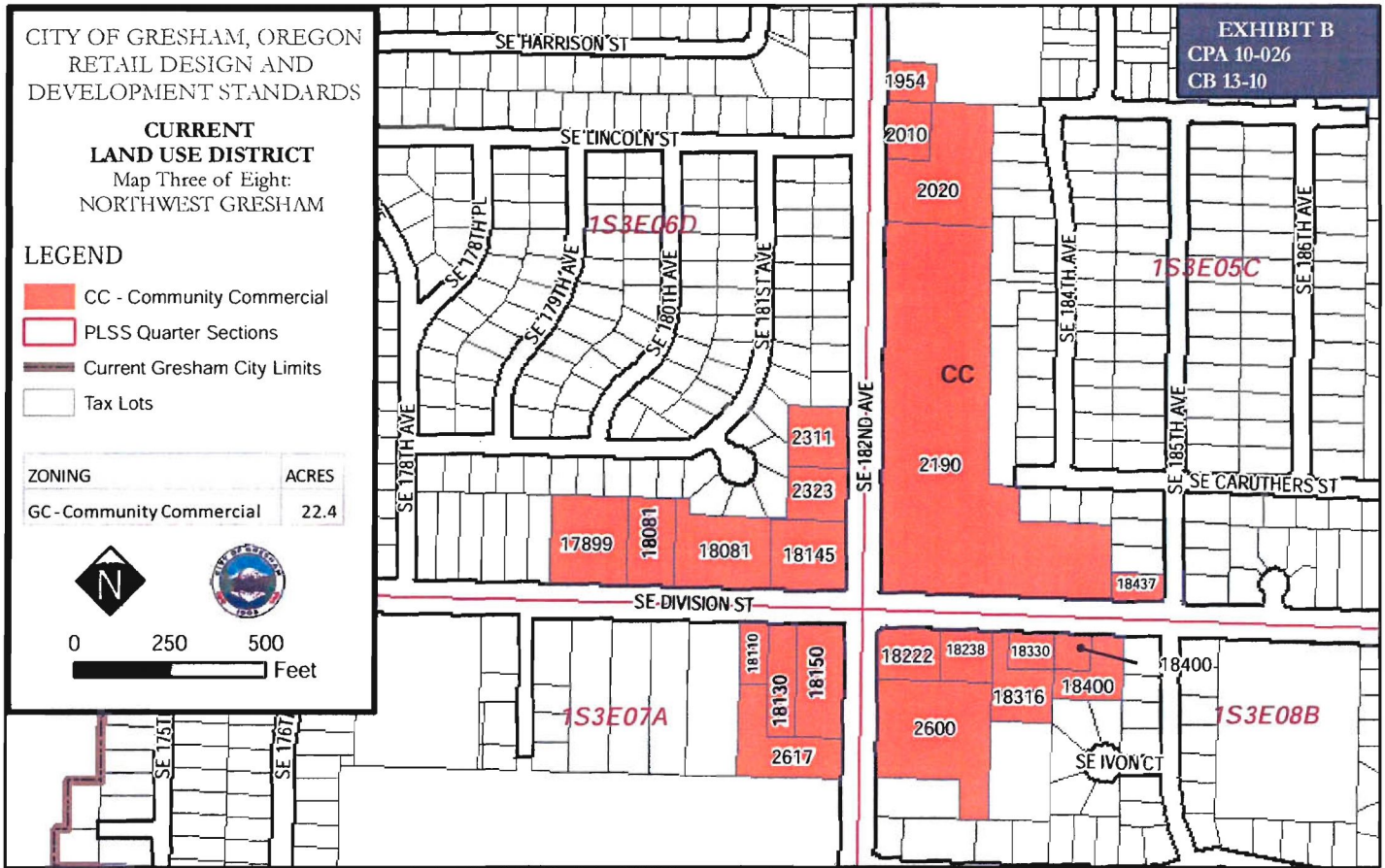
LEGEND

- NC - Neighborhood Commercial
- PLSS Quarter Sections
- Current Gresham City Limits
- Tax Lots

ZONING	ACRES
NC - Neighborhood Commercial	1.6



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Y:\inter-Departmental\MapsAndData\Projects\2010\1406-1498\1127MapDocs\Map3.mxd Oct. 1, 2010

CITY OF GRESHAM, OREGON  
RETAIL DESIGN AND  
DEVELOPMENT STANDARDS

**CURRENT  
LAND USE DISTRICT**  
Map Four of Eight:  
NORTHEAST GRESHAM

**LEGEND**

- CC - Community Commercial
- PLSS Quarter Sections
- Current Gresham City Limits
- Tax Lots

ZONING	ACRES
CC - Community Commercial	13.1

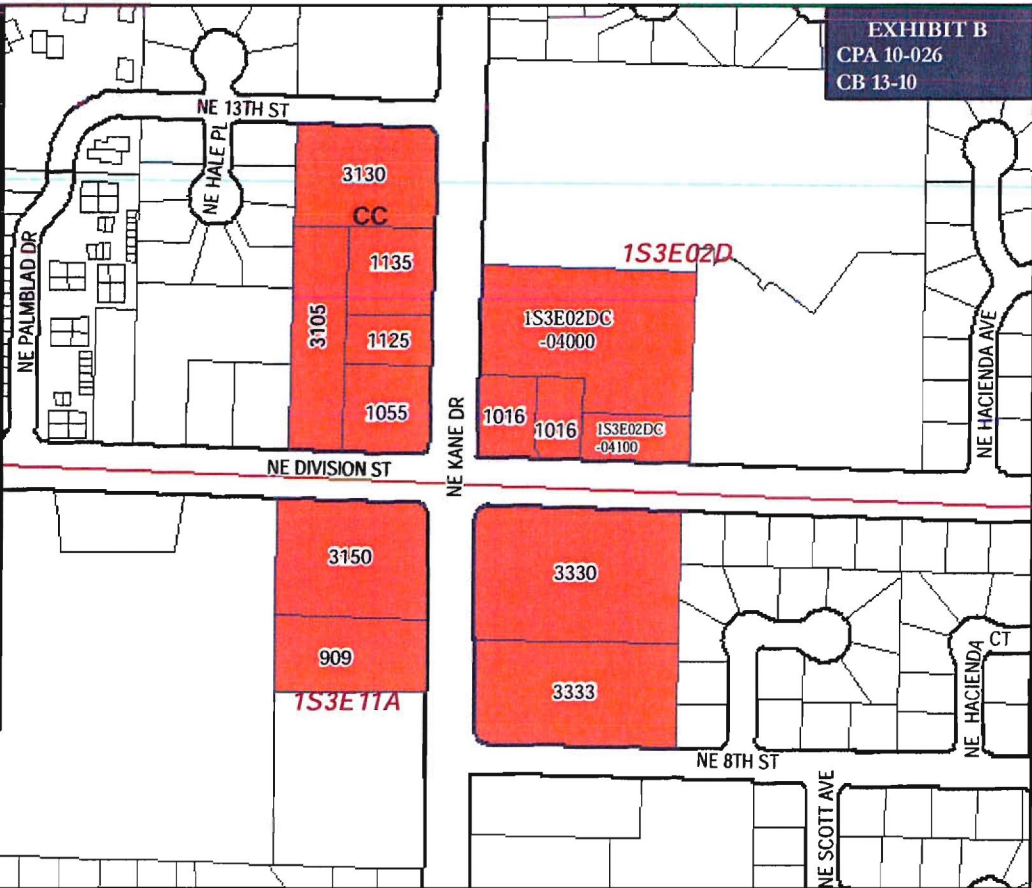


EXHIBIT B  
CPA 10-026  
CB 13-10

CITY OF GRESHAM, OREGON  
RETAIL DESIGN AND  
DEVELOPMENT STANDARDS

**PROPOSED  
LAND USE DISTRICT**  
Map Four of Eight:  
NORTHEAST GRESHAM

**LEGEND**

- MC - Moderate Commercial
- PLSS Quarter Sections
- Current Gresham City Limits
- Tax Lots

ZONING	ACRES
MC - Moderate Commercial	13.1

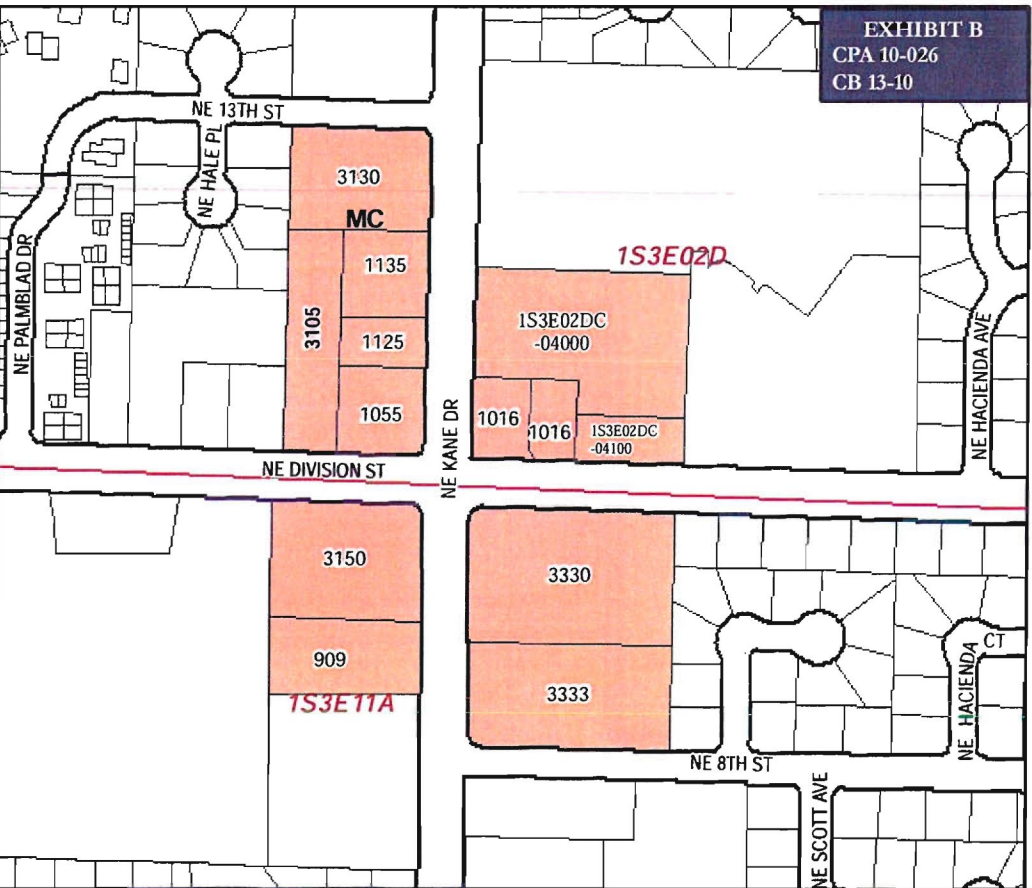
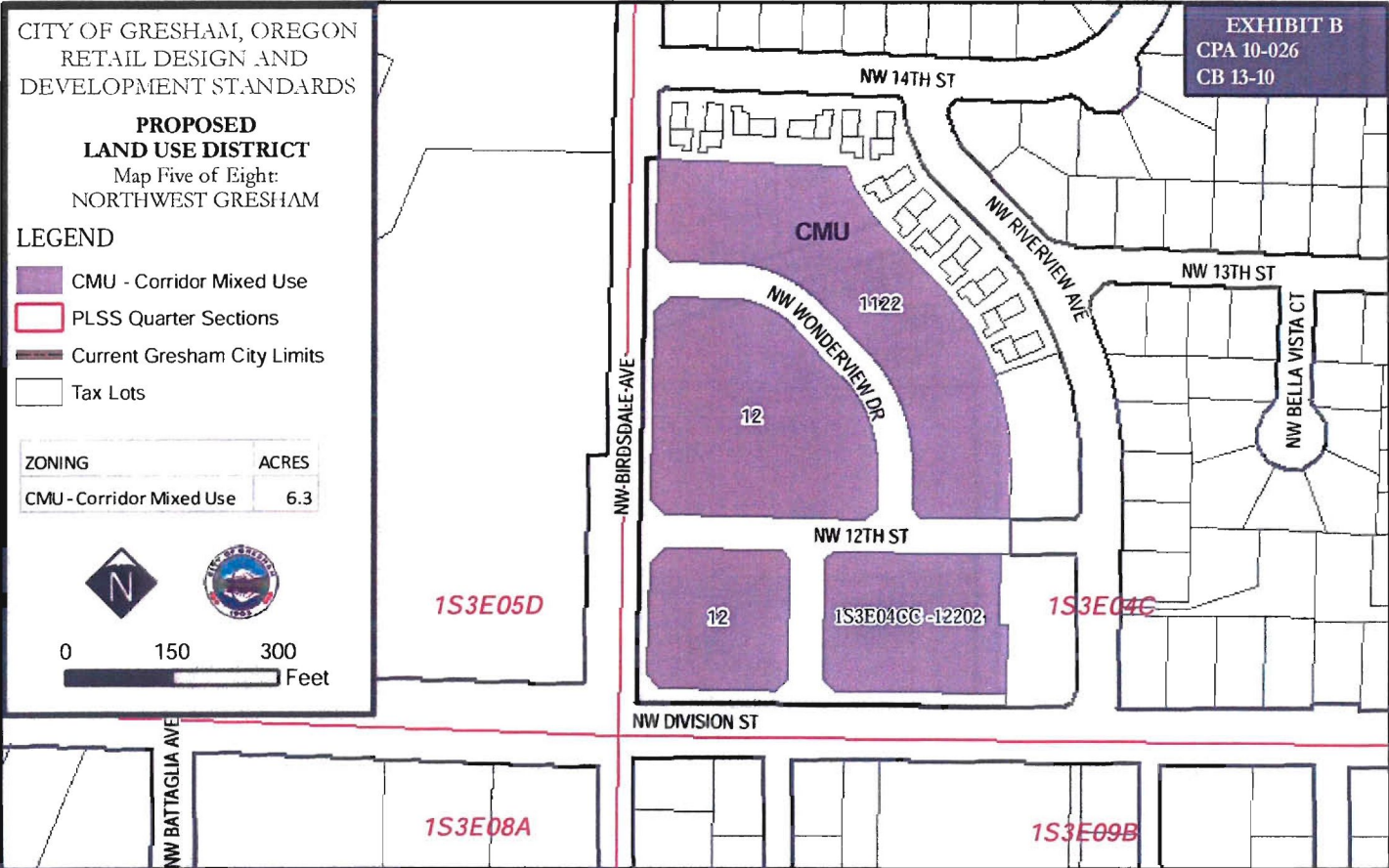
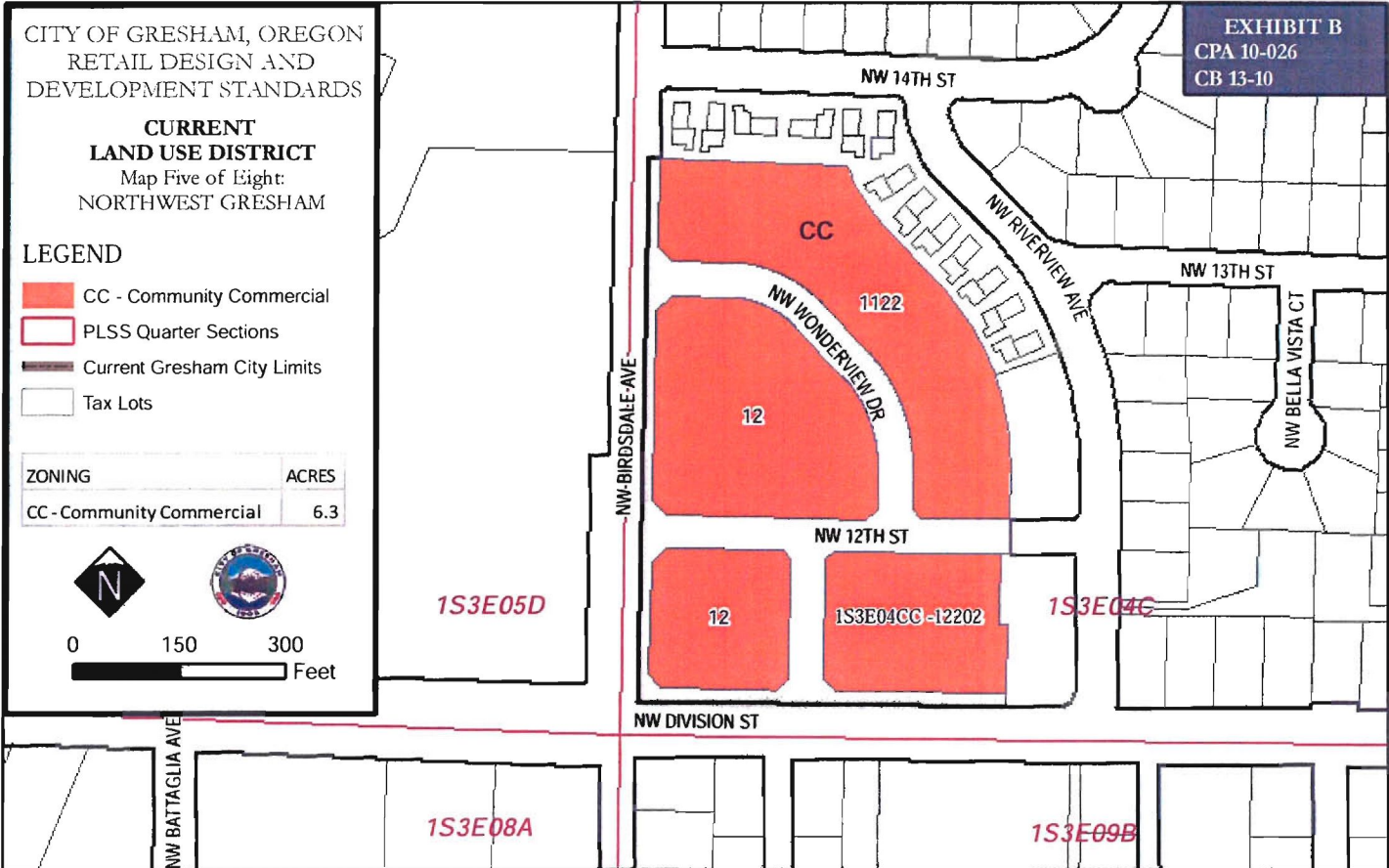


EXHIBIT B  
CPA 10-026  
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CITY OF GRESHAM, OREGON  
RETAIL DESIGN AND  
DEVELOPMENT STANDARDS

**CURRENT  
LAND USE DISTRICT**  
Map Six of Eight:  
SOUTHWEST GRESHAM

**LEGEND**

- CC - Community Commercial
- PLSS Quarter Sections
- Current Gresham City Limits
- Tax Lots

ZONING	ACRES
CC - Community Commercial	30.6

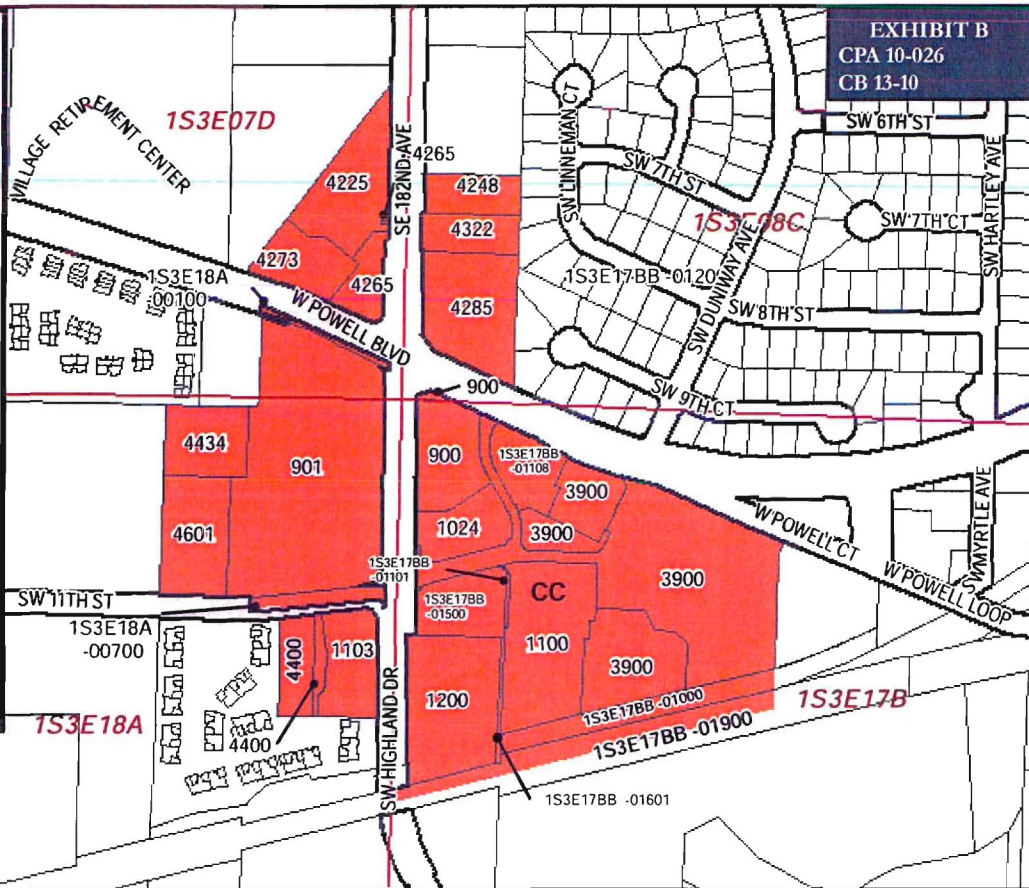


EXHIBIT B  
CPA 10-026  
CB 13-10

CITY OF GRESHAM, OREGON  
RETAIL DESIGN AND  
DEVELOPMENT STANDARDS

**PROPOSED  
LAND USE DISTRICT**  
Map Six of Eight:  
SOUTHWEST GRESHAM

**LEGEND**

- MC - Moderate Commercial
- PLSS Quarter Sections
- Current Gresham City Limits
- Tax Lots

ZONING	ACRES
MC - Moderate Commercial	30.6

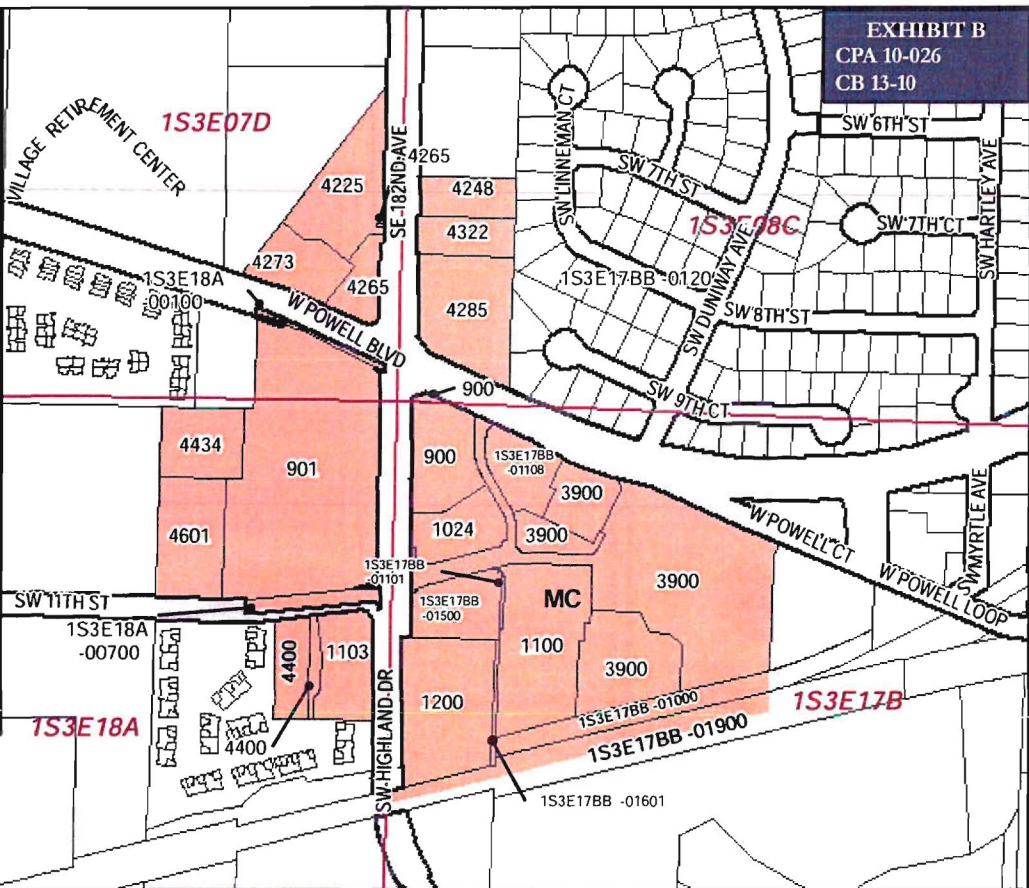


EXHIBIT B  
CPA 10-026  
CB 13-10

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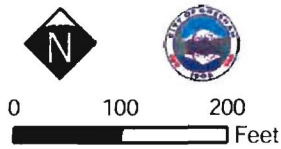
CITY OF GRESHAM, OREGON  
RETAIL DESIGN AND  
DEVELOPMENT STANDARDS

**CURRENT  
LAND USE DISTRICT**  
Map Seven of Eight:  
SOUTHEAST GRESHAM

LEGEND

- GC - General Commercial
- PLSS Quarter Sections
- Current Gresham City Limits
- Tax Lots

ZONING	ACRES
GC - General Commercial	0.8



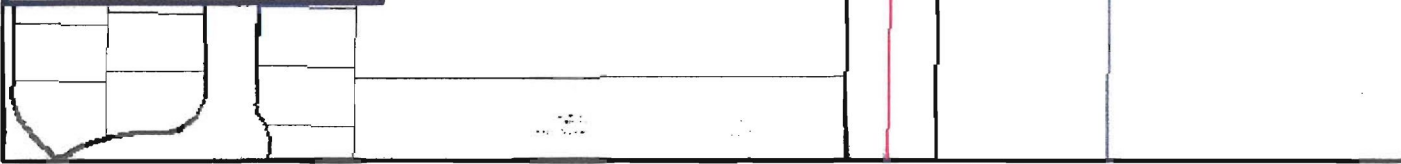
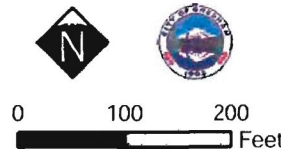
CITY OF GRESHAM, OREGON  
RETAIL DESIGN AND  
DEVELOPMENT STANDARDS

**PROPOSED  
LAND USE DISTRICT**  
Map Seven of Eight:  
SOUTHEAST GRESHAM

LEGEND

- NC - Neighborhood Commercial
- PLSS Quarter Sections
- Current Gresham City Limits
- Tax Lots

ZONING	ACRES
NC - Neighborhood Commercial	0.8



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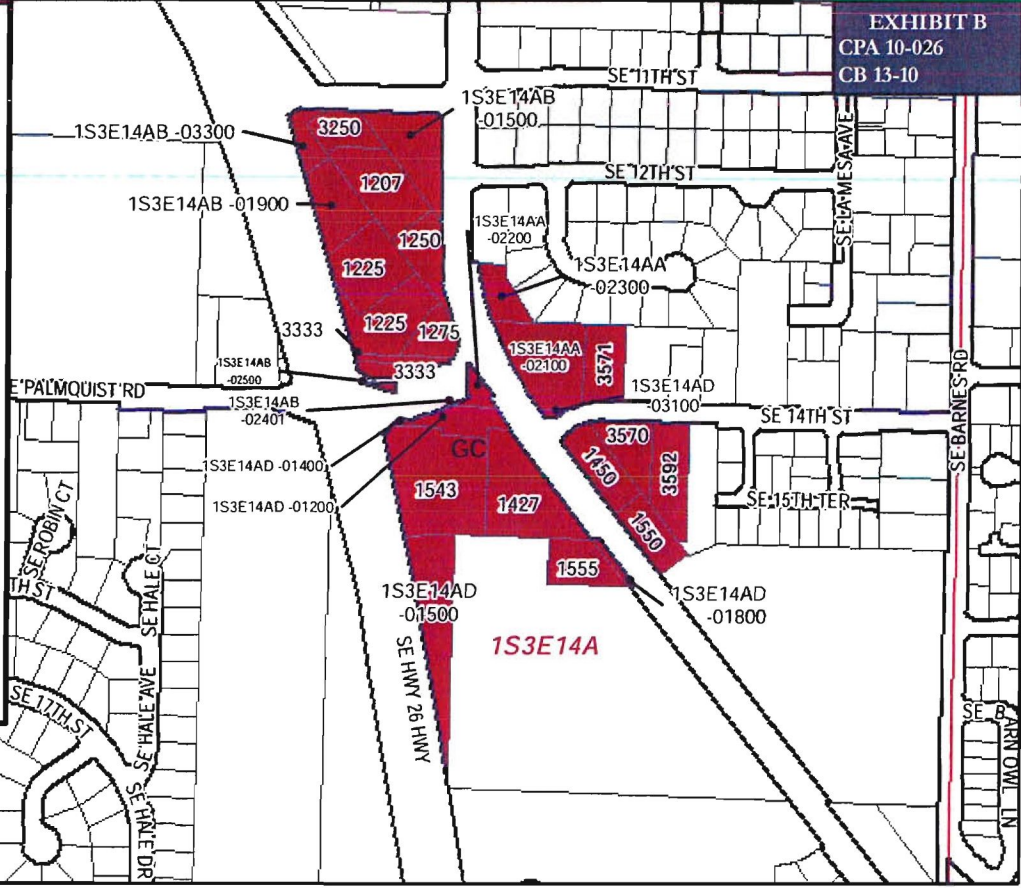
CITY OF GRESHAM, OREGON  
RETAIL DESIGN AND  
DEVELOPMENT STANDARDS

**CURRENT  
LAND USE DISTRICT**  
Map Eight of Eight:  
SOUTHEAST GRESHAM

**LEGEND**

- GC - General Commercial
- PLSS Quarter Sections
- Current Gresham City Limits
- Tax Lots

ZONING	ACRES
GC - General Commercial	13.1



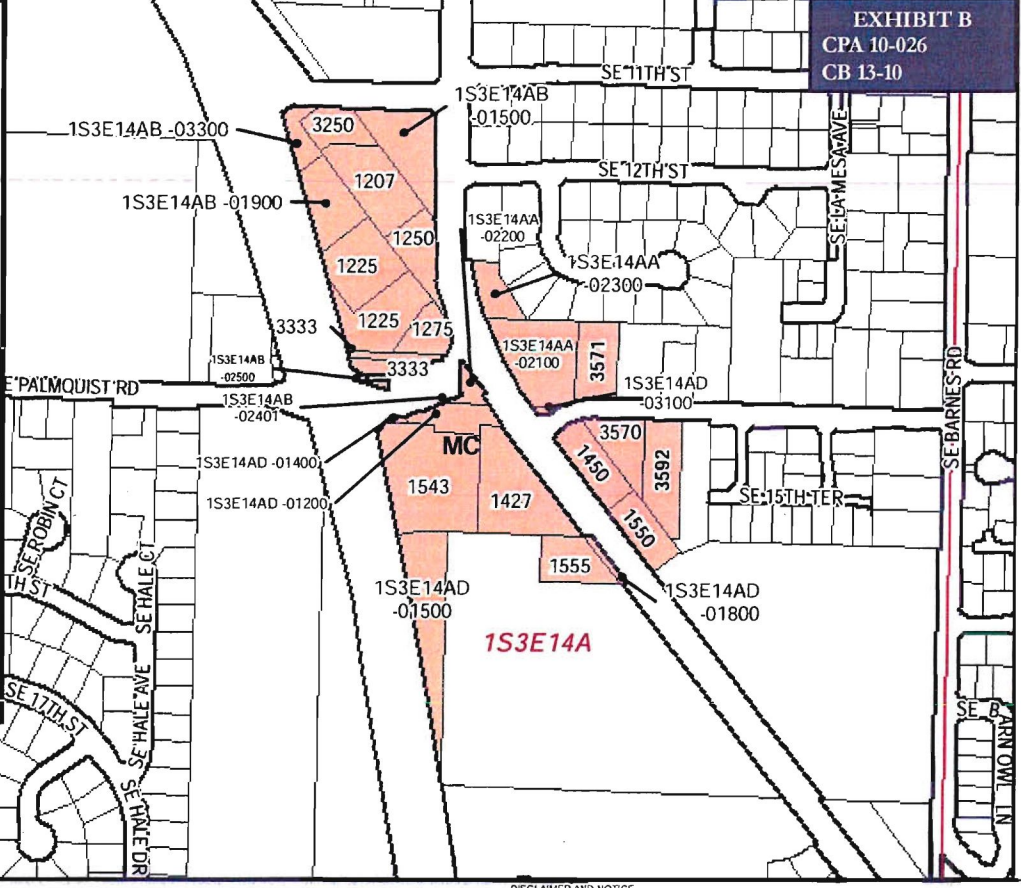
CITY OF GRESHAM, OREGON  
RETAIL DESIGN AND  
DEVELOPMENT STANDARDS

**PROPOSED  
LAND USE DISTRICT**  
Map Eight of Eight  
SOUTHEAST GRESHAM

**LEGEND**

- MC - Moderate Commercial
- PLSS Quarter Sections
- Current Gresham City Limits
- Tax Lots

ZONING	ACRES
MC - Moderate Commercial	13.1



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